

TRANSMITTAL TO CITY COUNCIL

Case No.(s)	Planning Staff Name(s) and Contact No.	C.D. No.
CPC-2010-760-GPA-VZC-HD-SPR	LYNDA SMITH 213-978-1196	8

Items Appealable to Council:	Last Day to Appeal:	Appealed:
VZC-HD-SPR	OCT 23 2012	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>

Location of Project (Include project titles, if any.)

2500 S. WESTERN AVENUE
 2490-2552 S. WESTERN AVENUE
 2501 S. HOBART BOULEVARD
 1985-1999 W. ADAMS BOULEVARD

Name(s), Applicant / Representative, Address, and Phone Number.

COMMUNITY IMPACT DEVELOPMENT II, LLC
 JAMES HOWARD JR.
 801 S. GRAND SUITE 625
 LOS ANGELES, CA 90017
 323-766-0700

Name(s), Appellant / Representative, Address, and Phone Number.

Final Project Description (Description is for consideration by Committee/Council, and for use on agendas and official public notices. If a General Plan Amendment and/or Zone Change case, include the prior land use designation and zone, as well as the proposed land use designation and zone change (i.e. "from Very Low Density Residential land use designation to Low Density land use designation and concurrent zone change from RA-1-K to (T)(Q)R1-1-K). In addition, for all cases appealed in the Council, please include in the description only those items which are appealable to Council.)

The Proposed Project is a two-story office building with approximately 56,960 square feet of floor area located over five levels of parking (two subterranean levels, one partially subterranean level, and two above ground levels) with 314 parking stalls, all on an approximately 70,763 square-foot site. The site is currently zoned RD1.5-1 and C2-1.

Fiscal Impact Statement <small>*Determination states administrative costs are recovered through fees.</small>	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>	Environmental No.	Commission Vote:
		ENV-2010-328-MND	6-0

 JAMES WILLIAMS, Commission Executive Assistant II	Date: <u>OCT 24 2012</u>
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LOS ANGELES CITY PLANNING COMMISSION
200 N. Spring Street, Room 272, Los Angeles, California, 90012, (213) 978-1300
www.lacity.org/PLN/index.htm

Determination Mailing Date: OCT 03 2012

CASE NO.: CPC-2010-760-GPA-VZC-HD-SPR
CEQA: ENV-2010-328-MND

Location: 2500 S. Western Avenue; 2490-2552 S. Western Avenue; 2501 S. Hobart Boulevard and 1985-99 W. Adams Boulevard
Council District: 8 – Parks
Plan Area: South Los Angeles
Request(s): General Plan Amendment; Vesting Zone Change; Height District Change and Site Plan Review

Applicant: Community Impact Development II, LLC

At its meeting on August 9, 2012, the following action was taken by the City Planning Commission:

1. **Approved a General Plan Amendment** to the South Los Angeles Community Plan from Low Medium II Residential to **Community Commercial**.
2. **Approved a General Plan Amendment** to the South Los Angeles Community Plan removing the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan which limits all properties designated Community Commercial to Height District 1.
3. **Approved a Vesting Zone Change and Height District Change** from RD1.5-1 to [T][Q]C2-2D.
4. **Approved a Site Plan Review** for projects which result in greater than 50,000 gross square feet of nonresidential floor area.
5. **Adopted** the attached **Conditions of Approval**.
6. **Adopted** the attached **Findings**.
7. **Adopted** Mitigated Negative Declaration No. **ENV-2010-328-MND**.
8. **Advised** the applicant that time limits for effectuation of a zone in the [T] Tentative or [Q] Qualified classification are specified in Section 12.32.G of the L.A.M.C. Conditions must be satisfied prior to the issuance of building permits and, that the [T] Tentative classification be removed in the manner indicated in the attached pages.

RECOMMENDATIONS TO CITY COUNCIL:

1. **Recommend** that the City Council **approve a General Plan Amendment** to the South Los Angeles Community Plan from Low Medium II Residential to **Community Commercial**.
2. **Recommend** that the City Council **approve a General Plan Amendment** to the South Los Angeles Community Plan removing the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan which limits all properties designated Community Commercial to Height District 1.
3. **Recommend** that the City Council **approve a Vesting Zone Change and Height District Change** from RD1.5-1 to [T][Q]C2-2D.
4. **Recommend** that the City Council **approve a Site Plan Review** for projects which result in greater than 50,000 gross square feet of nonresidential floor area.
5. **Recommend** that the City Council **adopt** the attached **Conditions of Approval**.
6. **Recommend** that the City Council **adopt** the attached **Findings**.
7. **Recommend** that the City Council **adopt** Mitigated Negative Declaration No. **ENV-2010-328-MND**.
8. **Recommend** that the City Council **consider** a reduction in non-required parking by 20 spaces, with the conversion of 2 of these parking spaces to additional bicycle parking.

Fiscal Impact Statement: There is no General Fund impact as administrative costs are recovered through fees.

This action was taken by the following vote:

Moved: Freer
Seconded: Lessin
Ayes: Hovaguimian, Kim, Perlman, Roschen
Recusal: Romero
Absent: Burton, Cardoso

Vote: 6-0



James K. Williams, Commission Executive Assistant II
City Planning Commission

Effective Date: The decision of the City Planning Commission is effective upon the mailing date of the determination letter and becomes final if no appeals are filed within the specified time limits.

Appeals: If the Commission has disapproved the Zone Change request, in whole or in part, the applicant may appeal that disapproval to the Council **within 20 days** after the mailing date of this determination. Any aggrieved party may appeal the Site Plan Review. Any appeal not filed within the **20-day period** shall not be considered by the Council. All appeals shall be filed on forms provided at the Planning Department's Public Counters at 201 N. Figueroa Street, Fourth Floor, Los Angeles, or at 6262 Van Nuys Boulevard, Suite 251, Van Nuys.

Final Appeal Date: OCT 23 2012

If you seek judicial review of any decision of the City pursuant to California Code of Civil Procedure Section 1094.5, the petition for writ of mandate pursuant to that section must be filed no later than the **90th day** following the date on which the City's decision became final pursuant to California Code of Civil Procedure Section 1094.6. There may be other time limits which also affect your ability to seek judicial review.

Attachments: Ordinance, Maps, Findings, Resolution
City Planner: Lynda Smith

CONDITIONS FOR EFFECTUATING [T] TENTATIVE CLASSIFICATION REMOVAL

Pursuant to Los Angeles Municipal Code Section 12.32 G, the [T] Tentative Classification shall be removed by the recordation of a final tract map or by posting of guarantees through the B-permit process of the City Engineer to secure the following without expense to the City of Los Angeles, with copies of any approvals or guarantees provided to the Planning Department for attachment to the subject City Plan case file.

Dedication(s) and Improvements. Prior to the issuance of any building permits, public improvements and dedications for streets and other rights-of-way adjoining the subject property shall be guaranteed to the satisfaction of the Bureau of Engineering, Department of Transportation, Fire Department (and other responsible City, regional, and Federal government agencies, as may be necessary).

Responsibilities/Guarantees.

- 1.) As part of early consultation, plan review, and/or project permit review, the applicant/developer shall contact the responsible agencies to ensure that any necessary dedications and improvements are specifically acknowledged by the applicant/developer.
- 2.) Prior to issuance of sign-offs for final site plan approval and/or project permits by the Department of City Planning, the applicant/developer shall provide written verification to the Department of City Planning from the responsible agency acknowledging the agency's consultation with the applicant/developer. The required dedications and improvements may necessitate redesign of the project. Any changes to the project design required by a public agency shall be documented in writing and submitted for review by the Department of City Planning.

A. Dedications Required:

Western Avenue (Major Highway – Class II). A 12-foot wide and variable width strip of land along the property frontage except at the location of the existing building to remain near the intersection with Adams boulevard to complete a 52-foot half right-of-way in accordance with Major Highway – Class II standards, including a 15-foot by 15-foot cut corner at the intersection with Adams Boulevard, a certified survey map showing the existing buildings and areas to be dedicated shall be submitted along with the dedication application. In addition, submit a legal description describing the area to be dedicated with the dedication application.

Hobard Boulevard (Local Street) – None.

Adams Boulevard (Scenic Major Highway – Class II). None at this time. The existing building to remain precludes the required dedication.

B. Improvements Required:

Western Avenue – Construct additional surfacing to provide a 40-foot half roadway in accordance with Major Highway-Class II standards, including asphalt pavement, integral concrete curb, 2-foot gutter and a 12-foot wide full width concrete sidewalk. At the location of the existing building to remain, repair any broken, off-grade or bad order concrete curb, gutter, sidewalks and close any unused driveways. These improvements should suitably transition and join the existing improvements.

Hobart Boulevard – Repair any broken, off-grade or bad order concrete curb, gutters and sidewalks and

close any unused driveways adjoining the property.

Adams Boulevard - Repair any broken, off-grade or bad order concrete curb, gutters and sidewalks and close any unused driveways adjoining the property.

Sewer lines exist in Western Avenue, Hobart Boulevard and Adams Boulevard. Extension of the 6-inch house connection laterals to the new property line may be required. All Sewerage Facilities Charges and Bonded Sewer Fees are to be paid prior to obtaining a building permit.

An investigation by the Bureau of Engineering Central District Office Sewer Counter may be necessary to determine the capacity of the existing public sewers to accommodate the proposed development. Submit a request to the Central District Office of the Bureau of Engineering (213) 482-7050.

Roof drainage and surface run-off from the property shall be collected and treated at the site and drained to the street through drain pipes constructed under the sidewalk and through curb drains or connections to the catch basins.

Street Lighting: Installation of street lights to the satisfaction of the Bureau of Street Lighting. If new street light(s) are required (satisfactory to the Bureau of Street Lighting (213) 847-1551), the property within the boundary of the development shall be formed or annexed into a Street Lighting Maintenance Assessment District prior to final recordation or issuance of the Certificate of Occupancy.

Install tree wells with root barriers and plant street trees satisfactory to the City Engineer and the Urban Forestry Division of the Bureau of Street Services. Some tree removal in conjunction with the street improvement project may require Board of Public Works Approval. The applicant should contact the Urban Forestry Division for further information (213) 847-3077.

Trees: Board of Public Works approval shall be obtained prior to the issuance of the Certificate of Occupancy of the development project, for the removal of any tree in the existing or proposed public right-of-way area associated with the improvement requirement outlined herein. The Bureau of Street Services Urban Forestry Division is the lead agency for obtaining Board of Public Works approval for the removal of such trees.

Submit a parking area and driveway plan to the Central District Office of the Bureau of Engineering and the Department of Transportation for review and approval.

Submit shoring and lateral support plans to the Bureau of Engineering Excavation Counter for review and approval prior to excavating adjacent to the right-of-way.

Notes:

The Department of Transportation may have additional requirements for dedications and improvements.

Relocate traffic signs, equipment and parking meters to the satisfaction of the Department of Transportation (213) 482-7024.

Refer to the Department of Water and power regarding power pole (213) 367-2715.

Refer to the Fire Department regarding fire hydrants (213) 482-6543.

Notice. If conditions dictate, connections to the public sewer system may be postponed until

adequate capacity is available.

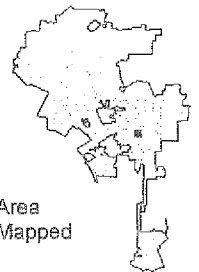
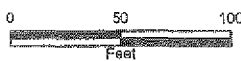
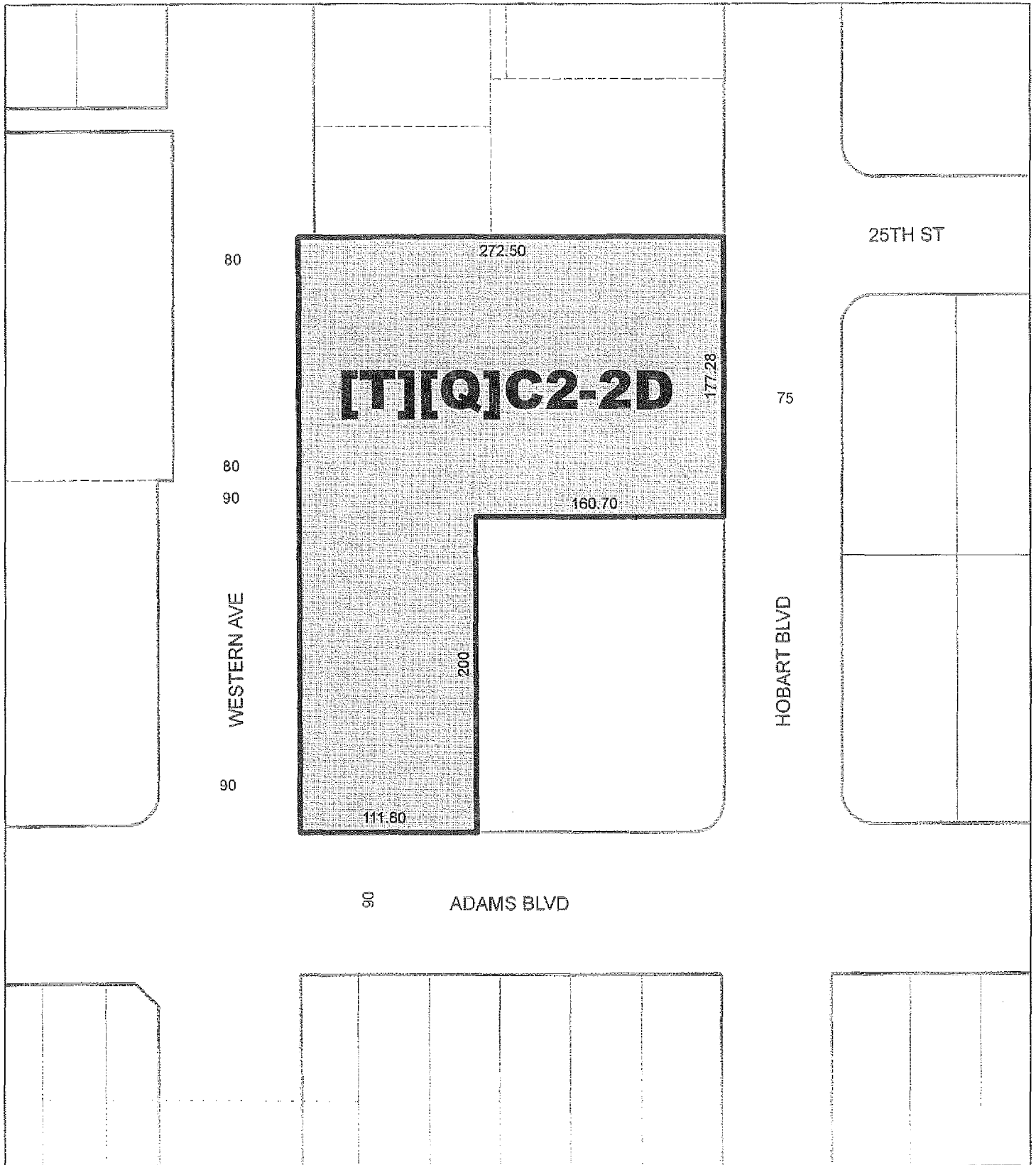
Notice. Certificates of Occupancies for the subject property will not be issued by the City until the construction of all the public improvements (streets, sewers, storm drains, etc.), as required herein, are completed to the satisfaction of the City Engineer.

ORDINANCE NO. _____

An ordinance amending Section 12.04 of the Los Angeles Municipal Code by amending the zoning map.

THE PEOPLE OF THE CITY OF LOS ANGELES DO ORDAIN AS FOLLOWS:

Section __. Section 12.04 of the Los Angeles Municipal Code is hereby amended by changing the zone classifications of property shown upon a portion of the Zoning Map incorporated therein and made a part of Article 2, Chapter 1 of the LAMC, so that such portion of the Zoning Map shall conform to the zoning on the map attached hereto and incorporated herein by this reference.



C.M. 123 B 193 | CPC 2010-0760-GPA-VZC-HD-SPR

AAV *[Signature]*

082712

[Q] QUALIFIED CONDITIONS OF APPROVAL

Pursuant to Section 12.32 G of the Municipal Code, the following limitations are hereby imposed upon the use of the subject property, subject to the "Q" Qualified classification.

A. Entitlement Conditions.

1. **Use/Density-D Limitation.** The development of the subject property shall comply with all area, use and height provisions of the C2-2D Zone, except as may be conditioned herein. The maximum permitted Floor Area Ratio (FAR) shall not exceed 2:1.

2. **Site Plan.** Prior to the issuance of any building permit, detailed development plans, including a complete landscape and irrigation plan and a parking area and driveway plan, shall be submitted to the Planning Department for review and sign-off clearance. These plans shall be in substantial conformance with the plot plan, elevations and landscape plans signed dated **August 9, 2012**, attached to the administrative file. The plans shall comply with applicable provisions of the Municipal Code, the subject conditions herein and the intent of the subject permit authorization. The following shall also apply:

- a. Prior to the issuance of a building permit, the proposed courtyard shall be reviewed and approved by the Office of Historic Resources and the Urban Design Studio. The type, and placement of the proposed Paul Williams monument shall be reviewed and approved by the Cultural Heritage Commission and/or its designee within the Office of Historic Resources.

The courtyard shall incorporate a minimum of 15-foot uninterrupted linear connection to adjacent Western Avenue, a minimum 15% planted area, one permanent seating surface for every 500 square feet of area (inclusive of planted area, seating surfaces may include planter walls that are at least 15 inches in depth), and hardscape surfaces shall be comprised of stone, masonry, interlocking pavers or other similar high-quality, durable, decorative paving surfaces. Treatment of the "rear wall" of the courtyard shall be given additional consideration.

- b. Prior to the issuance of a building permit, the project shall be reviewed by the Urban Design Studio to ensure that the proposed landscape buffer and green-screen adequately and appropriately buffers views of the structure (parking levels) on all sides of the proposed building.

The proposed green-screen shall incorporate an element of architectural rhythm and shall be illuminated at night. The landscape buffer shall include at minimum one 36-inch box shade tree for every 24 feet of street frontage, a mix of low-water ground covering and shrubbery, and a green-screen of living vines capable of substantially covering the proposed parking structure openings within a period of 24 months. Pilasters and other exposed structural components of the parking structure shall be finished with stone, masonry, travertine, or other durable, high-quality finish materials, and shall not be finished with exposed concrete, stucco or glass-reinforced concrete. Any proposed security fencing shall be placed away from the property line and incorporated into the landscaping, so as not to create a "hard edge".

- c. Additional security lighting shall be provided along the north "pedestrian access".

- d. Prior to the issuance of a building permit, the project shall be reviewed by the Urban Design Studio with regard to "improving the horizontal scale" (ie. reduced scale of windows) of the Washington Boulevard façade as it relates to the existing HCM structure.
 - e. If approved by the City Council, the project shall reduce the proposed non-required parking by 20 spaces. 2 of these non-required parking spaces shall be converted to additional bicycle parking.
3. **Parking.** Parking shall be provided pursuant to LAMC Section 12.21 and shall include a minimum of 230 parking spaces (which shall include a minimum of 116 replacement parking spaces). The number of spaces provided, their location and access shall be in substantial conformance with the parking plans marked Exhibit B-3 and attached to the administrative file.
 4. **Transit (Bicycle and TDM).** Prior to issuance of building permits and pursuant to LAMC Section 12.21.A.16, the applicant shall submit to the City Planning Department, revised, detailed plans, which show the number and location of all bicycle parking spaces and the location of showers for employees. The project shall provide a minimum of 5 bicycle spaces. Transportation Demand Management requirements shall be executed pursuant to LAMC Section 12.26.J.3. A copy of the LADOT accepted Covenant and Agreement shall be submitted to the administrative file.
 5. **Height.** The height of all buildings and structures shall be in substantial conformance with the subject elevations signed and dated August 9, 2012, attached to the administrative file. The height of all buildings and structures shall not exceed 75 feet. All heights shall be defined and permitted pursuant to LAMC Sections 12.03 and 12.21.1. Any structures on the roof, such as air conditioning units and other equipment, shall be fully screened from view of any abutting residential properties.
 6. **Trash and Storage Area(s).** Solid masonry block walls, a minimum of six feet in height, shall enclose trash and other storage areas. There shall be no openings except for gates. The areas shall be buffered so as not to result in noise, odor or debris impacts on any adjacent uses. All outside trash containers on the subject property shall be enclosed and shall be located so as not to result in noise or smell impacts on any adjacent use, or upon residents of the subject property.
 7. **Historic Cultural Monument (Courtyard).** Prior to the issuance of any permit, the Project shall obtain clearance from the Los Angeles City Planning Department Office of Historic Resources for the proposed work. (MM V-1)
 8. **Site Plan Review.** The subject Site Plan Review is approved subject to any appeals.
 9. **Design.** The project shall comply with the following:
 - a. A pedestrian entrance shall be provided along the Western Avenue frontage.
 - b. No pick-up/drop-off zone(s) shall be permitted along Western Avenue.
- B. Environmental Conditions.**
10. **Aesthetics.**

Landscape Plan. All open areas not used for buildings, driveways, parking areas, recreational facilities or walks shall be attractively landscaped and maintained in accordance with a landscape

plan and an automatic irrigation plan, prepared by a Landscape Practitioner and to the satisfaction of the decision maker. (MM I-1)

Maintenance. Every building, structure, or portion thereof, shall be maintained in a safe and sanitary condition and good repair, and free from, debris, rubbish, garbage, trash, overgrown vegetation or other similar material, pursuant to Municipal Code Section 91.8104. (MM I-2)

Graffiti. The exterior of all buildings and fences shall be free from graffiti when such graffiti is visible from a street or alley, pursuant to Municipal Code Section 91.8104.15. (MM I-3)

Signs. On-site signs shall be limited to the maximum allowable under the Municipal Code. (MM I-4)

Multiple temporary signs in store windows and along building walls are not permitted. (MM I-5)

Light. Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties or the public right-of-way. (MM I-7)

Glare. The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat. (MM I-8)

11. Air Quality.

All unpaved demolition and construction areas shall be wetted at least twice daily during excavation and construction, and temporary dust covers shall be used to reduce dust emissions and meet SCAQMD District Rule 403. (MM III-1)

The construction area shall be kept sufficiently dampened to control dust caused by grading and hauling, and at all times provide reasonable control of dust caused by wind. (MM III-2)

All clearing, earth moving, or excavation activities shall be discontinued during periods of high winds (i.e., greater than 15 mph), so as to prevent excessive amounts of dust. (MM III-3)

All dirt/soil loads shall be secured by trimming, watering or other appropriate means to prevent spillage and dust. (MM III-4)

All dirt/soil materials transported off-site shall be either sufficiently watered or securely covered to prevent excessive amount of dust. (MM III-5)

General contractors shall maintain and operate construction equipment so as to minimize exhaust emissions. (MM III-6)

Trucks having no current hauling activity shall not idle but be turned off. (MM III-7)

Stationary. An air filtration system shall be installed and maintained with filters meeting- or exceeding the ASHRAE Standard 52.2 Minimum Efficiency Reporting Value (MERV) of 12, to the satisfaction of the Department of Building and Safety. (MM III-8)

12. Biology.

Tree Report. Prior to the issuance of a grading or building permit, the applicant shall prepare and submit a Tree Report, prepared by a Tree Expert as defined in Section 17.02, indicating the location, size, type, and condition of all existing trees on the site. Such report shall also contain a

recommendation of measures to ensure the protection, relocation, or replacement of affected trees during grading and construction activities. (MM IV-1)

Tree Preservation (Grading Activities). Orange fencing" or other similarly highly visible barrier shall be installed outside of the drip line of locally protected and significant (trunk diameter of 8 inches or greater) non-protected trees, or as may be recommended by the Tree Expert. The barrier shall be maintained throughout the grading phase, and shall not be removed until the completion and cessation of all grading activities. (MM IV-2)

Tree Removal (Non-Protected Trees). Prior to the issuance of any permit, a plot plan shall be prepared indicating the location, size, type, and general condition of all existing trees on the site and within the adjacent public right(s)-of-way. (MM IV-3)

All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net, new trees, located within the parkway of the adjacent public right(s)-of-way, may be counted toward replacement tree requirements. (MM IV-4)

Removal or planting of any tree in the public right-of-way requires approval of the Board of Public Works. Contact Urban Forestry Division at: 213-847-3077. All trees in the public right-of-way shall be provided per the current standards of the Urban Forestry Division the Department of Public Works, Bureau of Street Services. (MM IV-5)

Tree Removal (Locally Protected Species). All protected tree removals require approval from the Board of Public Works. A Tree Report shall be submitted to the Urban Forestry Division of the Bureau of Street Services, Department of Public Works, for review and approval (213-847-3077), prior to implementation of the Report's recommended measures. (MM IV-6)

A minimum of two trees (a minimum of 48-inch box in size if available) shall be planted for each protected tree that is removed. The canopy of the replacement trees, at the time they are planted, shall be in proportion to the canopies of the protected tree(s) removed and shall be to the satisfaction of the Urban Forestry Division. (MM IV-7)

The location of trees planted for the purposes of replacing a removed protected tree shall be clearly indicated on the required landscape plan, which shall also indicate the replacement tree species and further contain the phrase "Replacement Tree" in its description. (MM IV-8)

Bonding (Tree Survival):

- a. The applicant shall post a cash bond or other assurances acceptable to the Bureau of Engineering in consultation with the Urban Forestry Division and the decision maker guaranteeing the survival of trees required to be maintained, replaced or relocated in such a fashion as to assure the existence of continuously living trees for a minimum of three years from the date that the bond is posted or from the date such trees are replaced or relocated, whichever is longer. Any change of ownership shall require that the new owner post a new oak tree bond to the satisfaction of the Bureau of Engineering. Subsequently, the original owner's oak tree bond may be exonerated.
- b. The City Engineer shall use the provisions of Section 17.08 as its procedural guide in satisfaction of said bond requirements and processing. Prior to exoneration of the bond, the owner of the property shall provide evidence satisfactory to the City Engineer and Urban Forestry Division that the oak trees were properly replaced, the date of the replacement and the survival of the replacement trees for a period of three years. (MM IV-9)

Tree Removal (Public Right-of-Way). Removal of trees in the public right-of-way requires approval by the Board of Public Works. The required Tree Report shall include the location, size, type, and condition of all existing trees in the adjacent public right-of-way and shall be submitted for review and approval by the Urban Forestry Division of the Bureau of Street Services, Department of Public Works (213-847-3077). (MM IV-10)

The plan shall contain measures recommended by the tree expert for the preservation of as many trees as possible. Mitigation measures such as replacement by a minimum of 24-inch box trees in the parkway and on the site, on a 1:1 basis, shall be required for the unavoidable loss of significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) trees in the public right-of-way. (MM IV-11)

All trees in the public right-of-way shall be provided per the current Urban Forestry Division standards. (MM IV-12)

13. Cultural Resources (Designated Historic-Cultural Resource).

The property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment. (MM V-2)

The historic character of the property shall be retained and preserved. The removal of historic material or alteration of features and spaces shall be avoided. (MM V-3)

The property shall be recognized as a physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other buildings, shall not be undertaken. (MM V-4)

Most properties change over time; those changes that have acquired significance in their own right shall be retained and preserved. (MM V-5)

Distinctive features, finishes and construction techniques or examples of skilled craftsmanship which characterize the historic property shall be preserved. (MM V-6)

New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment. (MM V-10)

New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired. (MM V-11)

The Golden State Building, site features, and parking lot guard building shall be documented according to HABS Level II standards prior to any demolition, abatement or rehabilitation work. The negatives and archival quality prints should be donated to the Los Angeles Public Library. HABS Level II documentation shall consist of the following: short form architectural and historical narrative; archival drawings; if adequate archival drawings are not available, measured drawings will be produced; and large format photography with views of all exterior elevations for each building, important features, key spatial relationships among buildings, and exterior hardscape features. (MM V-12)

A structural engineer with qualifications in completing historic preservation projects that conform to

the Secretary of the Interior's Standards for Rehabilitation shall be consulted and provide monitoring and written review of the engineering and construction of work that is on-site and contiguous with historic resources that are to remain. Particular attention shall be paid with regard to the demolition of buildings and landscaping, shoring, excavation, Project construction below and above grade near and attached to the on-site historic resources, and temporary shoring to mitigate weaknesses of interior removals and additional seismic risk that occur only during the construction phase.

A professional with who has completed historic preservation projects that conform to the Secretary of the Interior's Standards for Rehabilitation and that meets the Secretary of the Interior's Professional Qualifications Standards for Historic Architect (36 CFR 61) shall be consulted and provide monitoring and written review of the work that is related to historic preservation.

14. **Cultural Resources (Arcaeological)**. If any archaeological materials are encountered during the course of the project development, the project shall be halted. The services of an archaeologist shall be secured by contacting the Center for Public Archaeology - Cal State University Fullerton, or a member of the Society of Professional Archaeologist (SOPA) or a SOPA-qualified archaeologist to assess the resources and evaluate the impact. Copies of the archaeological survey, study or report shall be submitted to the South Central Coastal Information Center at Cal State Fullerton (SCCIC). A covenant and agreement shall be recorded prior to obtaining a grading permit.
15. **Cultural Resources (Paleontological)**. If any paleontological materials are encountered during the course of the project development, the project shall be halted. The services of a paleontologist shall be secured by contacting the Center for Public Paleontology - USC, UCLA, Cal State Los Angeles, Cal State Long Beach, or the Los Angeles County Natural History Museum to assess the resources and evaluate the impact. Copies of the paleontological survey, study or report shall be submitted to the Los Angeles County Natural History Museum. A covenant and agreement shall be recorded prior to obtaining a grading permit.
16. **Cultural Resources (Human Remains)**. If human remains are discovered at the project site during construction, work at the construction site shall be suspended, and the City of L.A. Public Works Department and County Coroner shall be immediately notified. If the remains are determined by the County Coroner to be Native American, the Native American Heritage Commission (NAHC) shall be notified within 24 hours, and the guidelines of the NAHC shall be adhered to in the treatment of disposition of the remains.
17. **Geology and Soils.**

Seismic. The design and construction of the project shall conform to the California Building Code seismic standards as approved by the Department of Building and Safety. (MM VI-1)

Erosion/Grading/Short-term Construction Impacts.

The applicant shall provide staked signage at the site with a minimum of 3-inch lettering containing contact information for the Senior Street Use Inspector (Department of Public Works), the Senior Grading Inspector (LADBS) and the hauling or general contractor. (MM VI-2)

Chapter IX, Division 70 of the Los Angeles Municipal Code addresses grading, excavations, and fills. All grading activities require grading permits from the Department of Building and Safety. Additional provisions are required for grading activities within Hillside areas. The application of BMPs includes but is not limited to the following mitigation measures:

- a. Excavation and grading activities shall be scheduled during dry weather periods. If grading occurs during the rainy season (October 15 through April 1), diversion dikes shall be constructed to channel runoff around the site. Channels shall be lined with grass or roughened pavement to reduce runoff

velocity.

b. Stockpiles, excavated, and exposed soil shall be covered with secured tarps, plastic sheeting, erosion control fabrics, or treated with a bio-degradable soil stabilizer. (MM VI-3)

18. Hazards and Hazardous Materials.

Asbestos. Prior to the issuance of any permit for the demolition or alteration of the existing structure(s), the applicant shall provide a letter to the Department of Building and Safety from a qualified asbestos abatement consultant indicating that no Asbestos-Containing Materials (ACM) are present in the building. If ACMs are found to be present, it will need to be abated in compliance with the South Coast Air Quality Management District's Rule 1403 as well as all other applicable State and Federal rules and regulations. (MM VIII-1)

Lead Paint. Prior to issuance of any permit for the demolition or alteration of the existing structure(s), a lead-based paint survey shall be performed to the written satisfaction of the Department of Building and Safety. Should lead-based paint materials be identified, standard handling and disposal practices shall be implemented pursuant to OSHA regulations. (MM VIII-2)

Polychlorinated Biphenyl. Prior to issuance of a demolition permit, a polychlorinated biphenyl (PCB) abatement contractor shall conduct a survey of the project site to identify and assist with compliance with applicable state and federal rules and regulation governing PCB removal and disposal. (MM VIII-3)

Methane Gas.

All commercial, industrial, and institutional buildings shall be provided with an approved Methane Control System, which shall include these minimum requirements; a vent system and gas-detection system which shall be installed in the basements or the lowest floor level on grade, and within under-floor space of buildings with raised foundations. The gas-detection system shall be designed to automatically activate the vent system when an action level equal to 25% of the Lower Explosive Limit (LEL) methane concentration is detected within those areas. (MM VIII-4)

All commercial, industrial, institutional and multiple residential buildings covering over 50,000 square feet of lot area or with more than one level of basement shall be independently analyzed by a qualified engineer, as defined in Section 91.7102 of the Municipal Code, hired by the building owner. The engineer shall investigate and recommend mitigation measures which will prevent or retard potential methane gas seepage into the building. In addition to the other items listed in this section, the owner shall implement the engineer's design recommendations subject to Department of Building and Safety and Fire Department approval. (MM VIII-5)

19. Human Health (Vector Control).

Trash and garbage collection bins shall be maintained in good condition and repair such that there are no holes or points of entry through which a rodent could enter. (MM VIII-9)

Trash and garbage collection containers shall be emptied a minimum of once per week. (MM VIII-10)

Trash and garbage bin collection areas shall be maintained free from trash, litter, garbage, and debris. (MM VIII-11)

20. Emergency Evacuation Plan. Prior to the issuance of a building permit, the applicant shall develop

an emergency response plan in consultation with the Fire Department. The emergency response plan shall include but not be limited to the following: mapping of emergency exits, evacuation routes for vehicles and pedestrians, location of nearest hospitals, and fire departments. (MM VIII-12)

21. Noise (Demolition, Grading, and Construction Activities).

The project shall comply with the City of Los Angeles Noise Ordinance No. 144,331 and 161,574, and any subsequent ordinances, which prohibit the emission or creation of noise beyond certain levels at adjacent uses unless technically infeasible. (MM XII-1)

Construction and demolition shall be restricted to the hours of 7:00 am to 6:00 pm Monday through Friday, and 8:00 am to 6:00 pm on Saturday. (MM XII-2)

Demolition and construction activities shall be scheduled so as to avoid operating several pieces of equipment simultaneously, which causes high noise levels. (MM XII-3)

The project contractor shall use power construction equipment with state-of-the-art noise shielding and muffling devices. (MM XII-4)

22. Hydrology and Water Quality.

In the event a permanent dewatering system is necessary, the Department of Building and Safety requires the following measures to mitigate the impacts to a less than significant level:

(a) Prior to the issuance of any permit for excavation, the applicant shall, in consultation with the Department of Building and Safety, submit a Dewatering Plan to the decision-maker for review and approval. Such plan shall indicate estimates for how much water is anticipated to be pumped and how the extracted water will be utilized and/or disposed of.

(b) Extracted groundwater shall be pumped to a beneficial on-site use such as, but not limited to: 1) landscape irrigation; 2) decorative fountains or lakes; 3) toilet flushing; or 4) cooling towers.

(c) Return water to the groundwater basin by an injection well. (MM IX-1)

Leaks, drips and spills shall be cleaned up immediately to prevent contaminated soil on paved surfaces that can be washed away into the storm drains. (MM IX-2)

All vehicle/equipment maintenance, repair, and washing shall be conducted away from storm drains. All major repairs shall be conducted off-site. Drip pans or drop clothes shall be used to catch drips and spills. (MM IX-3)

Pavement shall not be hosed down at material spills. Dry cleanup methods shall be used whenever possible. (MM IX-4)

Dumpsters shall be covered and maintained. Uncovered dumpsters shall be placed under a roof or be covered with tarps or plastic sheeting. (MM IX-5)

Applicants must meet the requirements of the Standard Urban Stormwater Mitigation Plan (SUSMP) approved by Los Angeles Regional Water Quality Control Board, which may include the following (a copy of the SUSMP can be downloaded at: <http://www.swrcb.ca.gov/rwqcb4/>):

Project applicants are required to implement stormwater BMPs to treat and infiltrate the runoff from a storm event producing $\frac{3}{4}$ inch of rainfall in a 24 hour period. The design of structural BMPs shall be in accordance with the Development Best Management Practices Handbook Part B Planning Activities. A signed certificate from a California licensed civil engineer or licensed architect that the proposed BMPs meet this numerical threshold standard is required.

Post development peak stormwater runoff discharge rates shall not exceed the estimated pre-development rate for developments where the increase peak stormwater discharge rate will result in increased potential for downstream erosion.

Concentrate or cluster development on portions of a site while leaving the remaining land in a natural undisturbed condition.

Maximize trees and other vegetation at each site by planting additional vegetation, clustering tree areas, and promoting the use of native and/or drought tolerant plants.

Any connection to the sanitary sewer must have authorization from the Bureau of Sanitation.

Cut and fill slopes in designated hillside areas shall be planted and irrigated to prevent erosion, reduce run-off velocities and to provide long-term stabilization of soil. Plant materials include: grass, shrubs, vines, ground covers, and trees.

Incorporate appropriate erosion control and drainage devices, such as interceptor terraces, berms, vee-channels, and inlet and outlet structures, as specified by Section 91.7013 of the Building Code. Protect outlets of culverts, conduits or channels from erosion by discharge velocities by installing a rock outlet protection. Rock outlet protection is physical device composed of rock, grouted riprap, or concrete rubble placed at the outlet of a pipe. Install sediment traps below the pipe-outlet. Inspect, repair, and maintain the outlet protection after each significant rain.

All storm drain inlets and catch basins within the project area must be stenciled with prohibitive language (such as NO DUMPING - DRAINS TO OCEAN) and/or graphical icons to discourage illegal dumping.

Signs and prohibitive language and/or graphical icons, which prohibit illegal dumping, must be posted at public access points along channels and creeks within the project area. Legibility of stencils and signs must be maintained.

Materials with the potential to contaminate stormwater must be:

- (1) placed in an enclosure such as, but not limited to, a cabinet, shed, or similar stormwater conveyance system; or
- (2) protected by secondary containment structures such as berms, dikes, or curbs.

The storage area must be paved and sufficiently impervious to contain leaks and spills.

The storage area must have a roof or awning to minimize collection of stormwater within the secondary containment area.

The owner(s) of the property will prepare and execute a covenant and agreement (Planning Department General form CP-6770) satisfactory to the Planning Department binding the owners to post construction maintenance on the structural BMPs in accordance with the Standard Urban Stormwater Mitigation Plan and or per manufacturer's instructions.

Trash container areas must have drainage from adjoining roofs and pavement diverted around the area(s).

Trash container areas must be screened or walled to prevent off-site transport of trash.

Reduce impervious land coverage of parking lot areas.

Infiltrate runoff before it reaches the storm drain system.

Runoff must be treated prior to release into the storm drain. Three types of treatments are available, (1) dynamic flow separator; (2) a filtration or (3) infiltration. Dynamic flow separator uses hydrodynamic force to remove debris, and oil and grease, and are located underground. Filtration involves catch basins with filter inserts. Filter inserts must be inspected every six months and after major storms, cleaned at least twice a year. Infiltration methods are typically constructed on-site and are determined by various factors such as soil types and groundwater table.

Prescriptive Methods detailing BMPs specific to this project category are available. Applicants are encouraged to incorporate the prescriptive methods into the design plans. These Prescriptive Methods can be obtained at the Public Counter or downloaded from the City's website at: www.lastormwater.org. (MM IX-6)

23. Noise (Parking Wall, Parking Structure Ramps).

A 6-foot-high solid decorative masonry wall adjacent to residential use and/or zones shall be constructed if no such wall exists. (MM XII-5)

Concrete, not metal, shall be used for construction of parking ramps. (MM XII-6)

The interior ramps shall be textured to prevent tire squeal at turning areas. (MM XII-7)

Parking lots located adjacent to residential buildings shall have a solid decorative wall adjacent to the residential. (MM XII-8)

24. Public Services.

Construction Activity Near Schools.

The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety. (MM XIV-1)

There shall be no staging or parking of construction vehicles, including vehicles to transport workers on any of the streets adjacent to the school. (MM XIV-2)

Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on these streets during school hours. (MM XIV-3)

The developer and contractors shall maintain ongoing contact with administrators of 24th Street Elementary, Pomeroy Widney High School, and Animo Pat Brown Charter High School. The administrative offices shall be contacted when demolition, grading and construction activity begin on the project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323)342-1400 and guarantee that safe and convenient pedestrian and bus routes to the school be maintained. (MM XIV-4)

Schools Affected By Haul Route.

Haul route scheduling shall be sequenced to minimize conflicts with pedestrians, school buses and cars at the arrival and Section IV. Explanation of Checklist Determination dismissal times of the school day. Haul route trucks shall not be routed past the school during periods when school is in

session especially when students are arriving or departing from the campus. (MM XIV-5)

LADBS shall assign specific haul route hours of operation based upon 24th Street Elementary, Pomeroy Widney High School, and Animo Pat Brown Charter High School's hours of operation. (MM XIV-6)

Public Services (Schools). The applicant shall pay school fees to the Los Angeles Unified School District to offset the impact of additional student enrollment at schools serving the project area. (MM XIV-7)

25. Transportation (Haul Route).

The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety. (MM XVI-1)

Projects involving the import/export of 20,000 cubic yards or more of dirt shall obtain haul route approval by the Department of Building and Safety. (MM XVI-2)

All haul route hours shall be limited to off-peak hours as determined by Board of Building and Safety Commissioners. (MM XVI-3)

The Department of Transportation shall recommend to the Building and Safety Commission Office the appropriate size of trucks allowed for hauling, best route of travel, the appropriate number of flag people. (MM XVI-4)

The Department of Building and Safety shall stagger haul trucks based upon a specific area's capacity, as determined by the Department of Transportation, and the amount of soil proposed to be hauled to minimize cumulative traffic and congestion impacts. (MM XVI-5)

The applicant shall be limited to no more than two trucks at any given time within the site's staging area. (MM XVI-6)

26. Safety Hazards.

The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety. (MM XVI-7)

The applicant shall submit a parking and driveway plan that incorporates design features that reduce accidents, to the Bureau of Engineering and the Department of Transportation for approval. (MM XVI-8)

27. Utilities (Local Water Supplies - All New Construction).

All restroom faucets shall be of a self-closing design. (MM XVII-5)

Prior to the issuance of any demolition or construction permit, the applicant shall provide a copy of the receipt or contract from a waste disposal company providing services to the project, specifying recycled waste service(s), to the satisfaction of the Department of Building and Safety. The demolition and construction contractor(s) shall only contract for waste disposal services with a company that recycles demolition and/or construction-related wastes. (MM XVII-6)

To facilitate on-site separation and recycling of demolition- and construction-related wastes, the contractor(s) shall provide temporary waste separation bins on-site during demolition and

construction. These bins shall be emptied and the contents recycled accordingly as a part of the project's regular solid waste disposal program. (MM XVII-7)

All waste shall be disposed of properly. Use appropriately labeled recycling bins to recycle demolition and construction materials including: solvents, water-based paints, vehicle fluids, broken asphalt and concrete, bricks, metals, wood, and vegetation. Non recyclable materials/wastes shall be taken to an appropriate landfill. Toxic wastes must be discarded at a licensed regulated disposal site. (MM XVII-8)

C. **Administrative Conditions of Approval.**

28. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review or approval, plans, etc., as may be required by the subject conditions, shall be provided to the Department of City Planning for placement in the subject file.
20. **Code Compliance.** Area, height and use regulations of the zone classification of the subject property shall be complied with, except where herein conditions are more restrictive.
30. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assigns. The agreement shall be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the file.
31. **Definition.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public offices, legislation or their successors, designees or amendment to any legislation.
32. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning and any designated agency, or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.
33. **Building Plans.** Page 1 of the grant and all the conditions of approval shall be printed on the building plans submitted to the Department of City Planning and the Department of Building and Safety.
34. **Indemnification.** The applicant shall defend, indemnify and hold harmless the City, its agents, officers, or employees from any claim, action, or proceeding against the City or its agents, officers, or employees to attack, set aside, void or annul this approval which action is brought within the applicable limitation period. The City shall promptly notify the applicant of any claim, action, or proceeding and the City shall cooperate fully in the defense. If the City fails to promptly notify the applicant of any claim, action or proceeding, or if the City fails to cooperate fully in the defense, the applicant shall not thereafter be responsible to defend, indemnify, or hold harmless the City.

Sec. ____ . The City Clerk shall certify to the passage of this ordinance and have it published in accordance with Council policy, either in a daily newspaper circulated in the City of Los Angeles or by posting for ten days in three public places in the City of Los Angeles: one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall; one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall East; and one copy on the bulletin board located at the Temple Street entrance to the Los Angeles County Hall of Records.

I hereby certify that this ordinance was passed by the Council of the City of Los Angeles, at its meeting of _____.


JUNE LAGMAY, City Clerk

By _____
Deputy

Approved _____

Mayor

Pursuant to Section 558 of the City Charter,
the City Planning Commission on August 9, 2012,
recommended this ordinance be adopted by the City Council.



James K. Williams, Commission Executive Assistant II
City Planning Commission

File No. _____

FINDINGS

1. **General Plan/Charter Findings.** The subject property is located within the South Los Angeles Community Plan, which was adopted by the City Council on March 22, 2000 (City Planning Case No. 96-0399 CPR). The South Los Angeles Community Plan map designates the subject property as Community Commercial (Corresponding Zones include CR, C2, C4, RAS3 and RAS4) and Low Medium II Residential (Corresponding Zones include RD1.5, RD2, RW2 and RZ2,5). The subject site contains dual zoning of RD1.5-1 and C2-1, with General Plan Land Use (GPLU) designations of Low Medium II Residential and Community Commercial.

2. **City Charter Sections 556 and 558.** The recommended General Plan Amendment from Low Medium II Residential to Community Commercial (for a portion of the site) and to amend Footnote Number 1 of the South Los Angeles Community Plan to exclude the project site from the Height District 1 (1.5:1 Floor Area Ratio) limitation (applicable to all properties with the Community Commercial General Plan Land Use designation), will create a site with a single General Plan Land Use designation of Community Commercial and zoning of C2-2D. The recommended General Plan Amendment **does comply** with Charter Sections 556 and 558 in that the recommended amendment reflects the land use patterns, trends and uses in the immediate area and does further the intent, purposes and objectives of the South Los Angeles Community Plan.

The proposed office building would be constructed on a site that has, for over 50 years, been developed with a commercial surface parking lot serving the existing Golden State Mutual Life Insurance Building (Historic Cultural Monument No. LA-1000, 1999 W. Adams Boulevard, located at the northeast corner of Adams Boulevard and Western Avenue and Listed 6/11/11). The property is located within the South Los Angeles Community Plan, the CRA/LA Normandie 5 Redevelopment Project Area, and is to the east (across Western Avenue) of the West Adams Terrace Historic Preservation Overlay Zone area. The Redevelopment Plan provides that the floor area ratio (FAR) applicable to property designated for commercial use shall not exceed 2.0:1 (the recommended maximum permitted FAR shall be 2:1 through approval of a Development Limitation). The proposed new office building would be occupied by the South Central Los Angeles Regional Center ("SCLARC"), and would serve as SCLARC's headquarters. SCLARC is a private, non-profit, community based organization which contracts with the State Department of Developmental Services (DDS) to coordinate services for individuals with developmental disabilities and their families.

The South Los Angeles Community Plan contains the following applicable Goals, Objectives Policies and Programs:

GOAL 2. A STRONG AND COMPETITIVE COMMERCIAL SECTOR WHICH BEST SERVES THE NEEDS OF THE COMMUNITY THROUGH MAXIMUM EFFICIENCY AND ACCESSIBILITY WHILE PRESERVING THE HISTORIC COMMERCIAL AND CULTURAL CHARACTER OF THE DISTRICT.

- Objective 2-1** To conserve and strengthen viable commercial development.
- Policy 2-1.1** New commercial uses shall be located in existing, established commercial areas or existing shopping centers.

- Policy 2-1.3** Commercial areas should be consolidated and deepened to stimulate existing businesses, create opportunities for new development and off-street parking, expand the variety of goods and services, and improve shopping convenience as well as offer local employment.
- Policy 2-1.5** Require that projects be designed and developed to achieve a high level of quality, distinctive character, and compatibility with existing uses and development.

The proposed office building would be constructed on a portion of the site that is both zoned and planned for medium density residential use along the eastern half, and commercial use along the western half, but that had been developed with a surface parking lot serving the existing office building (located at the northeast corner of Adams Boulevard and Western Avenue) for approximately 50 years. The requested General Plan Amendment would re-designate for commercial/office use, that portion of the site presently designated for residential uses, and would bring the entire site under a single zone (C2-2D) and GPLU (Community Commercial). Having a unified Zone and General Plan designation on the site, support both the development of site in an orderly manner and is consistent with the Community Plan's policy to consolidate and deepen commercial areas to create opportunities for new development. In order to achieve a high level of quality and distinctive character in its design, and to ensure that it would be compatible with existing uses and development, the project would be designed incorporating applicable design guidelines as set forth in the South Los Angeles Community Plan, and per the recommendation(s) of the City of Los Angeles Office of Historic Resources/Cultural Heritage Commission.

- Objective 2-3** To attract uses which strengthen the economic base and expand market opportunities for existing and new businesses.

The recommended General Plan Amendment and Vesting Zone Change will support the development of the proposed office/parking use, the construction of which, would allow the proposed tenant, the South Los Angeles Regional Center ("SCLARC"), to expand its services while remaining within the South Los Angeles Community Plan area, where many of its users are located, while increasing parking on the site.

- Objective 2-5** To enhance the appearance of commercial districts.
- Policy 2-5.1** Improve the appearance and landscaping of commercial properties.
- Policy 2-5.2** Preserve community character, scale and architectural diversity.
- Policy 2-5.3** Improve safety and aesthetics of parking areas in commercial areas.

The proposed project would be consistent with this objective and these policies in that it would replace a surface parking lot with the new office building and parking structure. The proposed abundant landscaping and courtyard area, will significantly improve the appearance of the property. The project would be designed to incorporate applicable design guidelines from the Community Plan, including screening the parking from public views, and providing a clearly defined pedestrian entrance and courtyard along the Western Avenue frontage. Furthermore, the parking structure would provide increased safety as compared to the existing surface lot in that it would provide adequate security lighting and other measures, including surveillance cameras and on-site security personnel. As indicated above, the new construction will be designed to support and complement the existing historic Golden Mutual Insurance Company building (HCM-1000).

Objective 2-6 To maintain and increase the commercial employment base for community residents whenever possible.

Policy 2-6.1 Protect commercial plan designations so that commercial development is encouraged.

The proposed project would be consistent with this objective and policy. The requested Zone and Height District Change to C2-2D and General Plan Amendment from Low Medium II Residential to Community Commercial will support development of a larger and updated office use at the scale needed for the SCLARC to remain within the community where it has operated nearby for many years, and will enable the tenant to provide additional services for the community and to expand its employee base.

Policy 11-1.2 New development projects should be designated to minimize disturbance to existing traffic flow with proper ingress and egress to parking.

Policy 11-2.1 No increase in density and intensity shall be effectuated by zone change, variance, conditional use, parcel map or subdivision unless it is determined that the transportation system can accommodate the increased traffic generated by the project.

Policy 11-2.2 Require new development projects to mitigate off-site traffic impacts to the maximum extent feasible.

Policy 11-2.3 Require that driveway access points onto major and secondary highways, arterials, and collector streets be limited in number and be located to ensure the smooth and safe flow of vehicles and bicycles.

Policy 14-1.1 Encourage non-residential development to provide employee incentives for utilizing alternatives to the automobile (i.e., carpools, vanpools, buses, flex time, bicycles, and walking, etc.).

Policy 14-1.3 Require the proposals for major new non-residential development projects include submission of a TDM Plan to the City.

Policy 16-1.4 Encourage the provision of showers, changing rooms and bicycle storage at new and existing non-residential developments and public places.

The proposed project would be consistent with these policies. The ingress to the parking area would be provided from Hobart Boulevard as it has since the property was first developed with the existing use. No driveway access would be provided to or from Western Avenue, a designated Major Highway. The project would provide bicycle parking and shower facilities for employees commuting by bicycle, and a Transportation Demand Management Plan, including designated carpool space and on-site information on available transit, would be implemented by the project, as required by the Los Angeles Municipal Code.

Policy 17-1.1 Consolidate parking, where appropriate, to eliminate the number of ingress and egress points onto arterials.

Policy 17-1.5 New parking lots and new parking garages shall be developed in accordance with design standards.

The access to the proposed parking structure would be provided from a single driveway from Hobart Boulevard, consistent with the use and operation of the property for more than 50 years. The parking structure would be subterranean and the above ground parking areas would not be visible from surrounding properties as they will be buffered with screening (plant and architectural).

GOAL 19 ***A COMMUNITY WHICH PRESERVES AND RESTORES THE MONUMENTS, CULTURAL RESOURCES, NEIGHBORHOODS, AND LANDMARKS WHICH HAVE HISTORICAL AND/OR CULTURAL SIGNIFICANCE.***

Objective 19-1 To ensure that the Plan Area's significant cultural and historical resources are protected, preserved, and/or enhanced.

Policy 19-1.2 Identify all designated City of Los Angeles Historic and Cultural Monuments in order to foster public appreciation of the City of Los Angeles' valuable historic resources and to promote education of the public by preserving Los Angeles' historic past and to promote that any other appropriate landmarks of unique architectural and historical significance continue to be identified for the purpose of inclusion in the list.

Objective 19-2 To protect and enhance historic and architectural resources in commercial areas in a manner that will encourage revitalization and investment in these areas.

Policy 19-2.1 Encourage the preservation, maintenance, enhancement and adaptive reuse of existing buildings in commercial areas through the restoration of original facades and the design of new construction which complements old in a harmonious fashion, enhancing the historic pattern.

Objective 19-4 To encourage private owners of historic properties/resources to conserve the integrity of such resources.

Policy 19-4.1 To assist private owners of historic resources to maintain and/or enhance their properties in a manner that will preserve the integrity of such resources in the best possible condition.

Program: *Adherence to the City's historic properties preservation ordinances and the Cultural Heritage board requirements for preservation and design, implementation of the design standards.*

The proposed office use is located on the site of a designated Historic Cultural Monument, The Golden State Mutual Life Insurance Company Building (Historic Cultural Monument No. LA-1000, 1999 W. Adams Boulevard, Listed 6/11/11). The proposed office/parking building is located on a portion of the site that contained surface parking for the cultural resource. The proposed building will provide parking for both the existing building and for the new use. The Applicant has proposed a "courtyard area" which will physically connect the two buildings and provide a monument to the architect of the original building, Mr. Paul Revere Williams. The new structure and courtyard area will be reviewed by the Office of Historic Resources for compatibility with the existing historic structure and site design.

As indicated above, the subject General Plan Amendment from Low Medium II Residential to Community Commercial would create a single General Plan Land Use designation over the entire site. The elimination of the Height District limitation (HD-1) imposed on all properties designated as Community Commercial (Footnote No. 1 South Los Angeles Community Plan) will permit the redevelopment of the site at a density and intensity that is both compatible with surrounding uses, within the maximum FAR limitation recommended by the LA/CRA (site is located within the Normandy 5 Project Area) to promote redevelopment and investment in the

area, and is consistent with the Goals and Objectives of the South Los Angeles Community Plan. Approval of the subject General Plan Amendment in conjunction with the proposed Zone/Height District Change (C2-2D) will support the development of the proposed office/parking use, the construction of which, would allow the proposed tenant (a non-profit, community based service organization), to expand its services and its employee base, while remaining within the South Los Angeles Community Plan area where many of its users are located, and will increase secure, needed on-site and neighborhood parking. While there are currently no C2 zoned properties in the area designated as HD-2, the requested Height District Change to 2D would not result in an introduction of the maximum permitted density, or FAR of 6:1. The proposed Development Limitation-"D" would limit the maximum permitted amount of floor area that could be constructed on the property to a maximum FAR of 2:1, which is sufficient enough to allow for the development of the new uses on the site while retaining the existing Historic Cultural Resource.

3. **Vesting Zone and Height District Change.** *Pursuant to Section 12.32-C of the Municipal Code, and based on these findings, the recommended action is deemed consistent with public necessity, convenience, general welfare, and good zoning practice.*

The recommended Zone/Height District Change from RD1.5-1 and C2-1 to [T][Q]C2-2D is consistent with the proposed General Plan Land Use Designation of Community Commercial and is within the range of corresponding zone(s) (which include CR, C2, C4, RAS3 and RAS4) permitted by such. The subject site is located within the South Los Angeles Community Plan area at the Northeast corner of Western Avenue and Adams Boulevard and is presently developed with a five story office building, and surface parking.

The subject property is an L shaped site with dual zoning and General Plan Land Use (GPLU) designations. That portion of the lot fronting Western Avenue is zoned C2-1 and the portion fronting Hobart Boulevard is zoned RD1.5-1. The requested Zone Change is to rezone that portion of the property which currently contains surface parking from RD1.5-1 (C2-1 for the remainder of the site) to [Q]C2-2D. A Height District change has also been requested to change the Height District from Height District 1 (unlimited height/ maximum FAR of 1.5:1) to Height District 2D (unlimited height/maximum FAR of 6:1, D Limitation(s)) over the entire property. The subject D limitation will permit a maximum Floor Area Ratio of 2:1. In addition, a General Plan Amendment has been requested to change the existing GPLU designation from Low Medium II Residential (for the RD1.5-1 portion of the site) to Community Commercial, and that would remove the entire site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan, which limits all properties with a General Plan Land Use designation of Community Commercial to Height District 1.

The property is surrounded by a variety of commercial (office and retail), institutional (educational and religious) and residential (multiple family, single family) uses in buildings ranging from one to five stories in height. The property abuts a one-story commercial center to the southeast, is adjacent to a five-story apartment building on the east side of Hobart Boulevard, a three-story, independent living and senior housing apartment building to the north, and a three-story Retirement and Care Center located on the west side of Western Avenue. At four-stories, the First African Methodist Episcopal Church's Assistance Corporation and Renaissance Center is located beyond, on the south side of Adams Boulevard. The five-story, historically designated (HCM-1000) Golden State Mutual Life Insurance Company building is located at the southwest portion of the subject property.

Properties located to the north of the subject site are zoned: RD1.51; [Q]RD2-1 and C2-1 with commercially zoned properties facing Western Avenue and residentially zoned properties facing Hobart Boulevard. Properties to the south (across Adams Boulevard) are zoned: C2-1-O-HPOZ; C1-1VL-O; RD1.5-1-O; and R4-1-O. Properties to the immediate east are zoned:

RD1.5-1 and C2-1, and to the west across Western Avenue are zoned: C2-1-O-HPOZ; R1-1-O-HPOZ; C2-1-O, P-1-O, PF-1-O and R4-1-O-HPOZ.

Height District 2 (HD-2) permits unlimited height and a maximum Floor Area Ratio (FAR) of 6:1 for C2 zoned properties. While there are currently no C2 zoned properties in the area designated as HD2, the requested Height District Change to 2D would not result in an introduction of the maximum permitted density, or FAR of 6:1. The proposed Development Limitation-"D" would limit the maximum permitted amount of floor area that could be constructed on the property to two times the area of the lot, or a FAR of 2:1. In addition, the subject D will be permanent and can only be removed through a subsequent Legislative Action (Zone/Height District Change).

The addition of the proposed 56,960 square foot building to the existing 54,234 square foot building (per ZIMAS) on the property would result in a total floor area of 111,194 square feet, and an FAR of approximately 1.68:1¹. Within the immediately surrounding area, there are properties developed at similar or higher FAR levels, including the apartment building immediately to the east with an FAR of 2.82:1 and the FAME Assistance Corporation and Renaissance Center to the south with an FAR of 1.89:1. Thus, the proposed change to Height District 2 with the imposition of the D Limitation (limiting maximum permitted FAR to 2:1), would not result in a building that would be out of scale and character with existing development within the immediate vicinity of the property.

The action, as recommended, has been made contingent upon compliance with the [T] and [Q] conditions imposed herein. Such limitations are necessary to protect the best interests of and to assure a development more compatible with surrounding properties, to secure an appropriate development in harmony with the General Plan, and to prevent or mitigate the potential adverse environmental effects of the subject recommended action.

4. Site Plan Review.

That the project is in substantial conformance with the purposes, intent and provisions of the General Plan, applicable community plan and any applicable specific plan.

The project site has a General Plan Land Use Designation of Community Commercial on the western portion and Low Medium II Residential on the eastern portion. The project is requesting a change from Low Medium II Residential to Community Commercial so that the entire property is designated for commercial use. The use proposed is consistent with South Los Angeles Community Plan's land use objectives and policies, which include conserving and strengthening viable commercial development, and consolidating and deepening commercial areas to stimulate existing businesses and create opportunities for new development. Therefore, upon approval of the requested General Plan Amendment, the project would be consistent with the General Plan.

That the project consists of an arrangement of buildings and structures (including height, bulk and setbacks), off-street parking facilities, loading areas, lighting, landscaping, trash collections, and other such pertinent improvements, which is or will be compatible with existing and future development on the neighboring properties.

The project consists of an arrangement of buildings and structures that is consistent with its

¹ FAR calculated on buildable area of 66,325 square feet (70,763 square foot lot area less 177.5' x 25' (4,438 square foot) Building Line area along Hobart Boulevard. 111,194 square feet total building area / 66,325 square foot buildable area = FAR of 1.676:1.

surroundings both in use and scale. The proposed three story building height and scale is compatible with several multi-story buildings in the area, including a five story apartment building on the east side of Hobart Boulevard, the three story FAME Arms independent living and senior housing apartment building to the north, the Western headquarters of the Brothers of St. John of God and the St. John of God Retirement and Care Center located on the west side of Western Avenue, and the five story, former Golden State Mutual Life Insurance Company building located at the southwest portion of the property.

The project features landscaped setback areas along Hobart Boulevard, Western Avenue, and the northerly property line, adjoining the FAME Arms building. Access to the new subterranean parking garage would be from Hobart Boulevard, similar to access currently provided to the existing surface parking lot. No vehicular access would be provided from Western Avenue to avoid interfering with the flow of traffic on this designated Major Highway.

A service area would be provided in the center of the site between the existing and proposed building to eliminate any adverse effects from loading/unloading and trash collection activities upon residential uses located to the north of the site. Exterior lighting would be directional and focused on site, avoiding spillover effects on adjoining properties. Therefore, as demonstrated by the foregoing, the project would be compatible with existing and future development on the neighboring properties.

That any project containing residential uses provides its residents with appropriate type and placement of recreational facilities and service amenities in order to improve habitability for the residents and minimize impacts on neighboring properties where appropriate.

The project does not include any residential uses.

CEQA Findings

Mitigated Negative Declaration No. ENV-2010-329-MND has been amended as follows:

Mitigation Measure V-7: Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive historic feature, the new feature shall match the old in design, color, texture, and other visual qualities, and where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.

Mitigation Measure V-8: Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used.

Mitigation Measure V-9: The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.

The above proposed Mitigation Measures related to Cultural Resources (Designated Historic-Cultural Resource) have been removed. These measures are directly related to the physical preservation/restoration of a cultural resource. The proposed Project is the construction of an in-fill structure (office/parking building) located on the same site as the subject Historic Cultural Monument (HCM LA-1000). There is no physical expansion, restoration or alteration is proposed for the existing HCM.

A Mitigated Negative Declaration (ENV-2010-328-MND) was prepared for the proposed project. On the basis of the whole of the record before the lead agency including any comments received, the lead agency finds that, with imposition of the mitigation measures described in the MND, there is no substantial evidence that the proposed project will have a significant effect on the environment. The

attached Mitigated Negative Declaration reflects the lead agency's independent judgment and analysis. The records upon which this decision is based are with the Environmental Review Section of the Planning Department in Room 750, 200 North Spring Street. I hereby adopt that the Mitigated Negative Declaration, imposed the conditions shown in that document on this approval.

RESOLUTION

WHEREAS, the subject property is located within the area covered by the South Los Angeles Community Plan, which was adopted by the City Council on March 22, 2000 (City Planning Case No. 96-0399 CPR; Council File No. 98-1192); and

WHEREAS, the applicant requested a proposed General Plan Amendment within the South Los Angeles Community Plan from Low Medium II Residential to Community Commercial and removing the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan which limits all properties designated Community Commercial to Height District 1; and

WHEREAS, the proposed project is for the construction of a two-story office building with approximately 56,960 square feet of floor area located over five levels of parking (two subterranean levels, one partially subterranean level, and two above ground levels) with 314 parking stalls; and

WHEREAS, the City Planning Commission at its meeting of August 9, 2012, approved a General Plan Amendment from Low Medium II Residential to Community Commercial and removing the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan which limits all properties designated Community Commercial to Height District 1, for property located at 2500 S. Western Avenue; 2490-2552 S. Western Avenue; 2501 S. Hobart Boulevard and 1985-99 W. Adams Boulevard; and

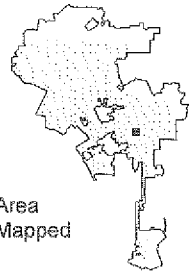
WHEREAS, pursuant to the provisions of the Los Angeles City Charter, the Mayor and City Planning Commission have transmitted their recommendations; and

WHEREAS, the recommended General Plan Amendment IS CONSISTENT with the intent and purpose of the adopted South Los Angeles Community Plan to designate land uses in an orderly and unified manner; and

WHEREAS, approval of the Community Commercial land use designation and removal the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan will allow the proposed office development on the site which will be consistent with the Plan and the zone; and

WHEREAS, the subject project has a Mitigated Negative Declaration No. ENV 2012-328-MND, in accordance with the City's Guidelines for implementation of the California Environmental Quality Act (CEQA).

NOW, THEREFORE, BE IT RESOLVED that the South Los Angeles Community Plan shall be amended as shown on the attached General Plan Amendment Map.



C.M. 123 B 193 | CPC 2010-0760-GPA-VZC-HD-SPR

AA/ *[Signature]*

082712

SOUTH LOS ANGELES



ITEM 14



DEPARTMENT OF CITY PLANNING
RECOMMENDATION REPORT

Los Angeles City Planning Commission

Date: Thursday August 9, 2012
Time: After 8:30 a.m.
Place: Los Angeles City Hall
200 North Spring Street
Public Works Board Room – 350
Los Angeles, CA 90012

Case No.: CPC-2010-0760-GPA-VZC-
HD-SPR
CEQA No.: ENV-2010-328-MND
Council No.: 8 – Bernard C. Parks
Plan Area: South Los Angeles
Certified NC: Empowerment Congress
North Area
GPLU: Low Medium II Residential;
Community Commercial
Zone: RD1.5-1; C2-1

Public Hearing: June 8, 2012
Appeal Status: LAMC Section 12.36 C.1(a); Zone
Change appealable to City Council
by Applicant if disapproved in
whole or in part;
Expiration Date: August 10, 2012

Applicant: Community Impact
Development II, LLC
Representative: Marcos Velayos, Park &
Velayos LLP

PROJECT LOCATION: 2500 S. Western Avenue (Includes: 2490-2552 S. Western Avenue; 2501 S Hobart Boulevard and 1985-1999 W. Adams Boulevard)

PROPOSED PROJECT: The Proposed Project is a two-story office building with approximately 56,960 square feet of floor area located over five levels of parking (two subterranean levels, one partially subterranean level, and two above ground levels) with 314 parking stalls, all on an approximately 70,763 square-foot site. The site is currently zoned RD1.5-1 and C2-1.

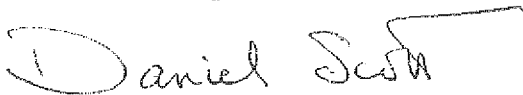
REQUESTED ACTION:

1. Pursuant to Section 11.5.6 of the Municipal Code, a **General Plan Amendment** to the South Los Angeles Community Plan as follows:
 - a. From Low Medium II Residential to Community Commercial, and
 - b. To remove the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan which limits all properties designated Community Commercial to Height District 1.
2. Pursuant to Section 12.32 of the Municipal Code, a **Vesting Zone Change and Height District Change** from RD1.5-1 (Restricted Density Multiple Family-Maximum Height 45 feet/ Maximum Floor Area Ratio – 3:1) and C2-1 (General Commercial-Unlimited Height/ Maximum Floor Area Ratio - 1.5:1) to [Q]C2-2D (General Commercial-Unlimited Height/ Maximum Floor Area Ratio - 2:1).
3. Pursuant Section 16.05 of the Municipal code, **Site Plan Review** for a for a development project which results in greater than 50,000 gross square feet of nonresidential floor area.
4. Pursuant to the California Public Resources Code, **Adopt** Mitigated Negative Declaration No. ENV-2010-328-MND, for the above referenced project.

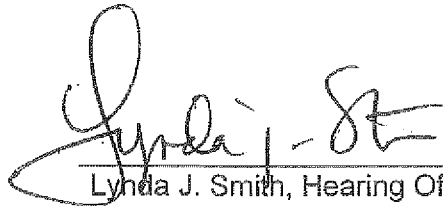
RECOMMENDED ACTIONS:

1. Deny as submitted.
2. **Approve and Recommend** that the City Council **Adopt a General Plan Amendment** to the South Los Angeles Community Plan removing the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan which limits all properties designated Community Commercial to Height District 1.
3. **Approve and Recommend** that the City Council **Adopt a General Plan Amendment** to the South Los Angeles Community Plan from Low Medium II Residential to Community Commercial.
4. **Approve and Recommend** that the City Council **Adopt a Vesting Zone Change and Height District Change** from RD1.5-1 and C2-1 to [T][Q]C2-2D.
5. **Approve** the requested **Site Plan Review**.
6. Pursuant to the California Public Resources Code, **Adopt** Mitigated Negative Declaration No. ENV-2010-328-MND and associated Findings.
7. **Adopt** the attached **Findings**.
8. **Recommend** that the applicant be advised that time limits for effectuation of a zone in the "T" Tentative classification or "Q" Qualified Classification are specified in Section 12.32.G of the L.A.M.C. Conditions must be satisfied prior to the issuance of building permits and, that the "T" Tentative classification be removed in the manner indicated on the attached pages.

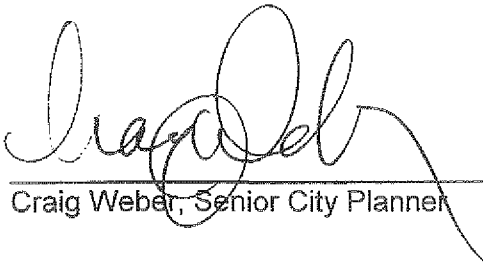
MICHAEL J. LOGRANDE
Director of Planning



Daniel Scott, Principal City Planner



Lynda J. Smith, Hearing Officer
Telephone: (213) 978-1196



Craig Weber, Senior City Planner

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B - Plans

- Site Plan
- Elevation Plan
- Landscape Plan

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PROJECT ANALYSIS

Project Summary

The proposed project involves the development of a two-story office building with approximately 56,960 square feet of floor area located over five levels of parking (two subterranean levels, one partially subterranean level, and two above ground levels) with 314 parking stalls, on an approximately 70,763-square-foot site. The site is currently zoned RD1.5-1 and C2-1.

Background

The subject property is an L-shaped parcel located within the South Los Angeles Community Plan area at the northeast corner of Adams Boulevard and Western Ave and contains dual General Plan Land Use (GPLU) designations and zones. The Applicant has requested a General Plan Amendment (GPA) from Low Medium II Residential to Community Commercial (for a portion of the site) with an additional amendment to the South Los Angeles Community Plan which would remove the entire site from the applicability of footnote No. 1. A Zone Change and Height District Change (ZC/HD) from RD1.5-1 and C2-1 to C2-2D has also been requested.

The proposed new office building would be occupied by the South Central Los Angeles Regional Center ("SCLARC"), which is currently also located within the South Los Angeles Community Plan area. SCLARC intends to relocate its facilities from 650 W. Adams Boulevard, approximately 1.5 miles to the east of the property. SCLARC is a private, non-profit, community based organization which contracts with the State Department of Developmental Services (DDS) to coordinate services for individuals with developmental disabilities and their families. SCLARC is one of 21 regional centers that serve communities throughout California. SCLARC serves the area bounded by Washington Boulevard on the north, La Cienega Boulevard on the west, and the eastern and southern boundaries of the cities of Downey, Paramount, Compton, Dominguez Hills and Carson.

SCLARC would occupy the office building proposed to be constructed to the north of the existing building on the site. In addition, a 314 space parking structure would be constructed underneath the new building, with access continued to be provided from Hobart Avenue, as it is for the current surface parking lot. The parking structure would provide parking for the new building and the existing building located on the property.

The Project Site is also located within the boundaries of the Conditional Use Approval for Sale of Alcoholic Beverages Specific Plan, the former LACRA Normandie 5 Redevelopment project area, a Los Angeles Enterprise Zone, the Los Angeles Fast Food Interim Control Ordinance and the West Adams Targeted Neighborhood Initiative Area, which is designated by the non-profit organization Los Angeles Neighborhood Initiative (LANI) to stimulate community-driven neighborhood revitalization. The project site is located to the east (across Western Avenue) of the West Adams Terrace Historic Preservation Overlay Zone (HPOZ), and northeast (across Adams Boulevard) of the Jefferson Park HPOZ.

General Plan Amendment

As stated above, the subject site contains dual General Plan Land Use designations. The portion of the subject site fronting Western Avenue (extending south to Adams Boulevard) is designated by the South Los Angeles Community Plan, as Community Commercial and the

remainder of the site (fronting Hobart Boulevard), is designated Low Medium II Residential. In addition, the subject request includes a General Plan Amendment which would remove the subject site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan. This Footnote limits properties designated as Community Commercial to the restrictions/limitations of Height District 1 regardless of the underlying zone. Height District 1 permits a maximum Floor Area Ratio/FAR of 1.5:1 for properties zoned C2 and a maximum FAR of 3:1 for properties zoned RD1.5.

The proposed office building would be constructed on a site that has, for over 50 years, been developed with a commercial parking lot serving the existing office building (the historic Golden State Mutual Life Insurance Building – Historic Cultural Monument No. LA - 1000) which is located at the southwest corner of the site. The subject General Plan Amendment would create a single GPLU designation (Community Commercial) and Floor Area Ratio over the entire site. This will permit the redevelopment of the site at a density and intensity that is both consistent with surrounding uses, and compatible with the existing historic resource.

Vesting Zone Change and Height District Change

The subject application contains a request for a Vesting Zone Change and Height District Change from C2-1 and RD1.5-1 to [Q]C2-2D. The subject site is a dual zoned, L-shaped property (Flag-Lot) located at the intersection of Western Avenue and Adams Boulevard, and has street frontages along Western Avenue, Adams Boulevard and Hobart Boulevard. The western portion of the Project Site (from Adams Boulevard north along Western Avenue) is zoned C2-1 (General Commercial), while the easternmost 150 feet of the Project Site (along Hobart Boulevard) is zoned RD1.5-1 (Restricted Density Multiple Dwelling). The C2 zoned portion permits commercial, office and retail uses, while the RD1.5 zoned portion permits residential uses at a density not exceeding one dwelling unit per 1,500 square feet of lot area. The "-1" component of each of these respective zones indicates that both portions of the site are located in Height District 1. The C2-1 zone permits a maximum floor area ratio (FAR) of 1.5:1, with no limitation on building height, and in the RD1.5-1 zone, permits a maximum FAR of 3:1, with a 45 foot height limit. While there are no properties in the immediate area designated as Height District No. 2, the recommended Zone of [T][Q]C2-2D will include a Development "D" Limitation which will limit the maximum permitted Floor Area Ratio on the site, to 2:1. This will bring the entire site under a single zone classification which will permit both, the orderly development of the site and will allow a use that is compatible with existing on-site development and adjacent uses. Approval of the requested Zone and Height District is also consistent with the South Los Angeles Community Plan, which seeks:

Goal 2 *A STRONG AND COMPETITIVE COMMERCIAL SECTOR WHICH BEST SERVES THE NEEDS OF THE COMMUNITY THROUGH MAXIMUM EFFICIENCY AND ACCESSIBILITY WHILE PRESERVING THE HISTORIC COMMERCIAL AND CULTURAL CHARACTER OF THE DISTRICT.*

Surrounding Land Uses

The site is located within the boundaries of the South Los Angeles Community Plan, the Conditional Use Approval for sale of Alcoholic Beverage Specific Plan, the LA/CRA Normandie 5 Redevelopment Project area, and the Los Angeles State Enterprise Zone.

Properties to the north of the subject site are zoned: RD1.51; [Q]RD2-1 and C2-1. These properties have a General Plan Land Use (GPLU) Designation of Low Medium II Residential.

Existing uses include: Commercial, institutional (private school, LAUSD 24th Street Elementary School) and medical (Los Angeles Metropolitan Hospital, Country Villa Convalescent Center) facing Western Avenue and residential facing Hobart Boulevard. An independent living and senior housing apartment building is located immediately to the north.

Properties to the south (across Adams Boulevard) are zoned: C2-1-O-HPOZ; C1-1VL-O; RD1.5-1-O; and R4-1-O, with GPLU designations of Community Commercial, Low Medium II Residential and High Medium Residential.

Existing uses include: Retail Commercial; institutional (church); office; parking and multiple family residential. The FAME Assistance Corporation and Renaissance Center and the Adams West senior apartments are located across Adams Boulevard. The Jefferson Park HPOZ is located south of Adams Boulevard and east of Western Avenue.

Properties to the immediate east are zoned: RD1.5-1 and C2-1, with GPLU designations of Low Medium II Residential and Community Commercial.

Existing uses include: Office; parking; retail; multiple-family residential uses. The First African Methodist Episcopal Church - FAME and related services are located to the east along Hobart and Harvard Boulevards.

Properties to the west across Western Avenue are zoned: C2-1-O-HPOZ; C2-1-O; P-1-O; PF-1-O; R1-1-O-HPOZ and R4-1-O-HPOZ, with GPLU designations of Community Commercial, Low Density Residential; Medium Density Residential; High Density Residential and Public Facility.

Existing uses include: Single family; parking; Institutional-The Western headquarters of the Brothers of St. John of God and the St. John of God Retirement and Care Center. The Ramsey-Durfee Estate consists of a large, Tudor-Revival style mansion (named Villa Maria) set back on the property and buffered from Western Avenue by extensive landscaping. The Estate is now used as the headquarters for the Brothers of St. John of God. The Estate is listed as a historic Cultural Monument by the Los Angeles Cultural Heritage Commission and is also listed on the NRHP. The West Adams Terrace HPOZ is located on the east side of Western Avenue.

Adams Boulevard is a designated Scenic Major Highway Class II improved to a width of 90 Feet, with curb and gutter.

Western Avenue is a designated Major Highway Class II, improved to a varied width of 80 to 90 Feet, with curb and gutter.

Hobart Boulevard is a designated Local Street, improved to a width of 75 Feet, with curb and gutter.

Historic Resource

The proposed parking and office building will be located on the existing surface parking lot which is located to the north of the historic Golden State Mutual Life Insurance Building (HCM No. 1000). The new office and parking project will replace surface parking that was developed in association with the former Golden State Mutual Life Insurance Building to provide parking for employees. The Golden State Building is located on the southern portion of the project site, at the northeast corner of the intersection of Western Avenue and Adams Boulevard, while the surface parking lot is located on the northern portion of the L-shaped site. Other on-site structures include a guard shack along the Site's southern boundary, a swing gate at the

existing driveway entrance along Hobart Boulevard, a service area at the rear of the Golden State Building, and pole-mounted flood lights along the perimeter of the surface parking lot. The service area at the rear of the Golden State Building is currently accessed by a driveway on Western Avenue.

The proposed Project would also include the construction of a tiered pedestrian plaza and a pedestrian walkway between the existing on-site five-story Golden State Mutual Life Insurance Building and the proposed office building. A monument to Paul R. Williams, architect, is proposed within the garden area of the plaza. Primary pedestrian access to the proposed office building would be along Western Avenue, while access to the Project's parking structure would be available via a driveway along Hobart Boulevard, in the same general location as the existing access driveway.

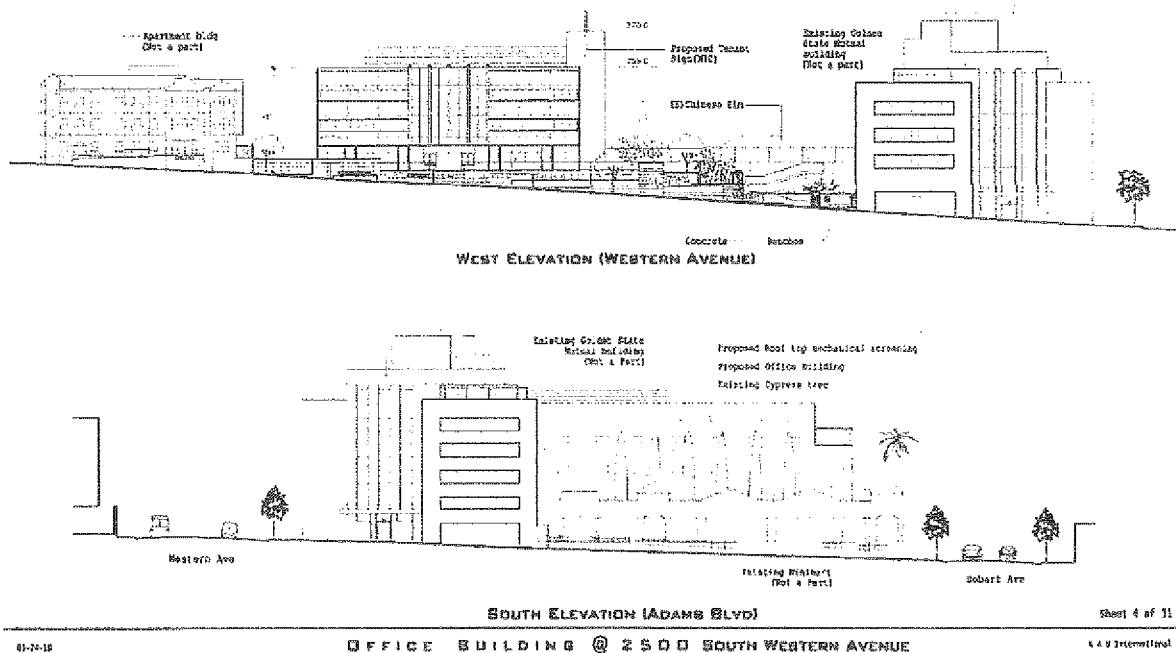
The five-story Golden State Building consists of approximately 39,800 square feet of floor area (58,820 gross sq ft.) and was dedicated in 1949. The Golden State Mutual Life Insurance Company was one of the largest African-American owned insurance companies in the United States. It was established by William Nickerson, Jr., Norman O. Houston, and George A. Beavers, prominent Los Angeles African-American entrepreneurs of the 1920s. The Golden State Guarantee Fund Insurance Company was founded in 1925 to address the critical lack of insurance available to African Americans in Los Angeles at the time. The name was changed to Golden State Mutual in 1931. The company began operation in a second story office at 1435 Central Avenue, which was the center for Los Angeles' African-American community. In 1931 the company built its first permanent home office at 4621 Central Avenue. In 1949 Golden State Mutual moved into new headquarters at 1999 West Adams Boulevard (the current site). Paul R. Williams served as the architect for new headquarters, erected on the corner of Western and Adams Boulevards. Mr. Williams was one of the most successful architects to practice in Los Angeles during the twentieth century. Certified as an architect in 1915, Williams was the first African American Fellow in the American Institute of Architects (AIA) and was the first President of the Los Angeles City Planning Commission.

The proposed office/parking building is larger in footprint than the historic building. The new building has two stories of office space above five levels of parking (three levels are subterranean), though the top level of parking "daylights" to form a plaza on the south side of the new building because the existing site and adjacent streets slope down from north to south. The south wall of the new office building is more than 100 feet north of the north wall of the existing historic building. The parapet height of the new building is not as high above adjacent grades as the main wings of the historic building. The central wing of the historic building is higher than the main body of the proposed new building, and the top of the proposed elevator tower on the new building is not taller than the highest features of the historic building. Even with a change in grade increasing from the historic structure to the new construction of approximately 18 Feet, the new structure will not contain any elements that are taller than the existing cultural monument.

The west wall of the proposed new building is set back from the property line along Western Avenue. The west wall of the historic building (along Western Avenue) is at the property line (no set-back), while the new building is set back approximately 32 feet from the sidewalk and is therefore subordinate (set-back from) to the façade of the cultural monument when viewed from the sidewalk. The east wall of the proposed new building is set back 25 feet from Hobart Avenue. The new building occupies the area that was originally and is currently used as a surface parking lot, therefore changing a historic spatial relationship. However, this parking lot has not been identified as a feature of significance. As indicated above, the new building, however, is set back far enough from Western Avenue that it is visually subordinated to the historic building. The new building does not alter the existing building's dominance as the principal visual feature at the northeast corner of Adams Boulevard and Western Avenue.

Because the Historic Monument building is located at the southwest corner of the site and is the prominent feature at the corner of Adams Boulevard and Western Avenue, the new building will not be visible from Adams Boulevard when looking toward the historic building in a due north direction. The new building will be visible if approaching from the east, above the one-story retail strip mall that is on adjacent property east of the historic building along Adams Boulevard. Again, because of the proposed set-back of the new structure, when approaching the historic building from the north (south bound on Western Avenue), the existing office building will still be visible before the proposed building is seen.

The new construction will not encroach upon the cultural monument either from its location on the site, or from its architectural style. The proposed structure will be built setback from the property line and not at the corner of the site. The architecture of the building will include an ivy green-screen which will screen one level of at-grade and one level of above grade, parking. This will appear as a soft landscape buffer from the street. The remainder of the building will be composed of horizontal elements such as a wall of glazing with vertical aluminum framing and a vertical stone/masonry element (tower for elevator/stairs), which complement the primary horizontal and strong vertical features of the 'Late Moderne' architectural style of the historic structure.



Please note: The above elevations include an earlier version of the proposed project which was taller in height (and included a tower element). The illustration is only to show the location of the new construction in relation to the Historic Cultural Monument.

Dedications and Improvements

The following Dedications and Improvements have been recommended related to Western Avenue (Major Highway – Class II):

Dedication. A 12-foot wide and variable width strip of land along the property frontage except at the location of the existing building to remain near the intersection with Adams boulevard to complete a 52-foot half right-of-way in accordance with Major Highway – Class II standards, including a 15-foot by 15-foot cut corner at the intersection with Adams Boulevard, a certified survey map showing the existing buildings and areas to be dedicated shall be submitted along with the dedication application. In addition, submit a legal description describing the area to be dedicated with the dedication application.

Improvement. – Construct additional surfacing to provide a 40-foot half roadway in accordance with Major Highway-Class II standards, including asphalt pavement, integral concrete curb, 2-foot gutter and a 12-foot wide full width concrete sidewalk. At the location of the existing building to remain, repair any broken, off-grade or bad order concrete curb, gutter, sidewalks and close any unused driveways. These improvements should suitably transition and join the existing improvements.

At the Public Hearing conducted on June 8, 2012, the Applicant's representative requested that no street improvements (widening of street) be required. The subject site is an L-shaped lot with frontages on three streets: Western Avenue - A dedicated Major Highway-Class II; Hobart Boulevard – A designated Local Street; and, Adams Boulevard – A designated Scenic Major Highway-Class II. The proposed new construction has its primary frontage along Western Boulevard and is set back approximately 32 Feet from the existing sidewalk to observe the required dedication (there are features located within the setback area such as the building sign, the main entry stairs and a portion of the entry plaza) and to provide a landscape buffer. The Golden State Mutual Life Insurance bank building (HCM No. LA-1000), is located at the corner of Adams Boulevard and Western Avenue, immediately south of the new construction, and is built to the existing sidewalk. There is no widening proposed or required along the street frontage located adjacent to the cultural monument.

As indicated above, a 12-foot wide and variable width of land along the front of the new building is required for dedication, along with physical improvements such as additional surfacing to complete the 40-foot half roadway adjacent to the structure. The Applicant has asked to have this improvement waived (the widening) as there can be no physical widening of the street adjacent to the existing cultural monument, located immediately to the south on the same lot.

[Q] QUALIFIED CONDITIONS OF APPROVAL

Pursuant to Section 12.32 G of the Municipal Code, the following limitations are hereby imposed upon the use of the subject property, subject to the "Q" Qualified classification.

A. Entitlement Conditions.

1. **Use/Density-D Limitation.** The development of the subject property shall comply with all area, use and height provisions of the C2-2D Zone, except as may be conditioned herein. The maximum permitted Floor Area Ratio (FAR) shall not exceed 2:1.
2. **Site Plan.** Prior to the issuance of any building permit, detailed development plans, including a complete landscape and irrigation plan and a parking area and driveway plan, shall be submitted to the Planning Department for review and sign-off clearance. These plans shall be in substantial conformance with the plot plan, elevations and landscape plans signed dated **August 9, 2012**, attached to the administrative file. The plans shall comply with applicable provisions of the Municipal Code, the subject conditions herein and the intent of the subject permit authorization.
3. **Parking.** Parking shall be provided pursuant to LAMC Section 12.21 and shall include a minimum of 230 parking spaces (which shall include a minimum of 116 replacement parking spaces). The number of spaces provided, their location and access shall be in substantial conformance with the parking plans marked Exhibit B-3 and attached to the administrative file.
4. **Transit (Bicycle and TDM).** Prior to issuance of building permits and pursuant to LAMC Section 12.21.A.16, the applicant shall submit to the City Planning Department, revised, detailed plans, which show the number and location of all bicycle parking spaces and the location of showers for employees. The project shall provide a minimum of 5 bicycle spaces. Transportation Demand Management requirements shall be executed pursuant to LAMC Section 12.26.J.3. A copy of the LADOT accepted Covenant and Agreement shall be submitted to the administrative file.
5. **Height.** The height of all buildings and structures shall be in substantial conformance with the subject elevations signed and dated August 9, 2012, attached to the administrative file. The height of all buildings and structures shall not exceed 75 feet. All heights shall be defined and permitted pursuant to LAMC Sections 12.03 and 12.21.1. Any structures on the roof, such as air conditioning units and other equipment, shall be fully screened from view of any abutting residential properties.
6. **Trash and Storage Area(s).** Solid masonry block walls, a minimum of six feet in height, shall enclose trash and other storage areas. There shall be no openings except for gates. The areas shall be buffered so as not to result in noise, odor or debris impacts on any adjacent uses. All outside trash containers on the subject property shall be enclosed and shall be located so as not to result in noise or smell impacts on any adjacent use, or upon residents of the subject property.

7. **Historic Cultural Monument (Courtyard)**. Prior to the issuance of any permit, the Project shall obtain clearance from the Los Angeles City Planning Department Office of Historic Resources for the proposed work. (MM V-1)
8. **Site Plan Review**. The subject Site Plan Review is approved subject to any appeals.
9. **Design**. The project shall comply with the following:
 - a. A pedestrian entrance shall be provided along the Western Avenue frontage.
 - b. No fencing shall be permitted along the Western Avenue frontage.
 - c. No pick-up/drop-off zone(s) shall be permitted along Western Avenue.

B. Environmental Conditions.

10. Aesthetics.

Landscape Plan. All open areas not used for buildings, driveways, parking areas, recreational facilities or walks shall be attractively landscaped and maintained in accordance with a landscape plan and an automatic irrigation plan, prepared by a Landscape Practitioner and to the satisfaction of the decision maker. (MM I-1)

Maintenance. Every building, structure, or portion thereof, shall be maintained in a safe and sanitary condition and good repair, and free from, debris, rubbish, garbage, trash, overgrown vegetation or other similar material, pursuant to Municipal Code Section 91.8104. (MM I-2)

Graffiti. The exterior of all buildings and fences shall be free from graffiti when such graffiti is visible from a street or alley, pursuant to Municipal Code Section 91.8104.15. (MM I-3)

Signs. On-site signs shall be limited to the maximum allowable under the Municipal Code. (MM I-4)

Multiple temporary signs in store windows and along building walls are not permitted. (MM I-5)

Light. Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties or the public right-of-way. (MM I-7)

Glare. The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat. (MM I-8)

11. Air Quality.

All unpaved demolition and construction areas shall be wetted at least twice daily during excavation and construction, and temporary dust covers shall be used to reduce dust emissions and meet SCAQMD District Rule 403. (MM III-1)

The construction area shall be kept sufficiently dampened to control dust caused by grading and hauling, and at all times provide reasonable control of dust caused by wind. (MM III-2)

All clearing, earth moving, or excavation activities shall be discontinued during periods of high winds (i.e., greater than 15 mph), so as to prevent excessive amounts of dust. (MM III-3)

All dirt/soil loads shall be secured by trimming, watering or other appropriate means to prevent spillage and dust. (MM III-4)

All dirt/soil materials transported off-site shall be either sufficiently watered or securely covered to prevent excessive amount of dust. (MM III-5)

General contractors shall maintain and operate construction equipment so as to minimize exhaust emissions. (MM III-6)

Trucks having no current hauling activity shall not idle but be turned off. (MM III-7)

Stationary. An air filtration system shall be installed and maintained with filters meeting- or exceeding the ASHRAE Standard 52.2 Minimum Efficiency Reporting Value (MERV) of 12, to the satisfaction of the Department of Building and Safety. (MM III-8)

12. Biology.

Tree Report. Prior to the issuance of a grading or building permit, the applicant shall prepare and submit a Tree Report, prepared by a Tree Expert as defined in Section 17.02, indicating the location, size, type, and condition of all existing trees on the site. Such report shall also contain a recommendation of measures to ensure the protection, relocation, or replacement of affected trees during grading and construction activities. (MM IV-1)

Tree Preservation (Grading Activities). Orange fencing" or other similarly highly visible barrier shall be installed outside of the drip line of locally protected and significant (trunk diameter of 8 inches or greater) non-protected trees, or as may be recommended by the Tree Expert. The barrier shall be maintained throughout the grading phase, and shall not be removed until the completion and cessation of all grading activities. (MM IV-2)

Tree Removal (Non-Protected Trees). Prior to the issuance of any permit, a plot plan shall be prepared indicating the location, size, type, and general condition of all existing trees on the site and within the adjacent public right(s)-of-way. (MM IV-3)

All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net, new trees, located within the parkway of the adjacent public right(s)-of-way, may be counted toward replacement tree requirements. (MM IV-4)

Removal or planting of any tree in the public right-of-way requires approval of the Board of Public Works. Contact Urban Forestry Division at: 213-847-3077. All trees in the public right-of-way shall be provided per the current standards of the Urban Forestry Division the Department of Public Works, Bureau of Street Services. (MM IV-5)

Tree Removal (Locally Protected Species). All protected tree removals require approval from the Board of Public Works. A Tree Report shall be submitted to the Urban Forestry Division of the Bureau of Street Services, Department of Public Works, for review and approval (213-847-3077), prior to implementation of the Report's recommended measures. (MM IV-6)

A minimum of two trees (a minimum of 48-inch box in size if available) shall be planted for each protected tree that is removed. The canopy of the replacement trees, at the time they are

planted, shall be in proportion to the canopies of the protected tree(s) removed and shall be to the satisfaction of the Urban Forestry Division. (MM IV-7)

The location of trees planted for the purposes of replacing a removed protected tree shall be clearly indicated on the required landscape plan, which shall also indicate the replacement tree species and further contain the phrase "Replacement Tree" in its description. (MM IV-8)

Bonding (Tree Survival):

- a. The applicant shall post a cash bond or other assurances acceptable to the Bureau of Engineering in consultation with the Urban Forestry Division and the decision maker guaranteeing the survival of trees required to be maintained, replaced or relocated in such a fashion as to assure the existence of continuously living trees for a minimum of three years from the date that the bond is posted or from the date such trees are replaced or relocated, whichever is longer. Any change of ownership shall require that the new owner post a new oak tree bond to the satisfaction of the Bureau of Engineering. Subsequently, the original owner's oak tree bond may be exonerated.
- b. The City Engineer shall use the provisions of Section 17.08 as its procedural guide in satisfaction of said bond requirements and processing. Prior to exoneration of the bond, the owner of the property shall provide evidence satisfactory to the City Engineer and Urban Forestry Division that the oak trees were properly replaced, the date of the replacement and the survival of the replacement trees for a period of three years. (MM IV-9)

Tree Removal (Public Right-of-Way). Removal of trees in the public right-of-way requires approval by the Board of Public Works. The required Tree Report shall include the location, size, type, and condition of all existing trees in the adjacent public right-of-way and shall be submitted for review and approval by the Urban Forestry Division of the Bureau of Street Services, Department of Public Works (213-847-3077). (MM IV-10)

The plan shall contain measures recommended by the tree expert for the preservation of as many trees as possible. Mitigation measures such as replacement by a minimum of 24-inch box trees in the parkway and on the site, on a 1:1 basis, shall be required for the unavoidable loss of significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) trees in the public right-of-way. (MM IV-11)

All trees in the public right-of-way shall be provided per the current Urban Forestry Division standards. (MM IV-12)

13. Cultural Resources (Designated Historic-Cultural Resource).

The property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment. (MM V-2)

The historic character of the property shall be retained and preserved. The removal of historic material or alteration of features and spaces shall be avoided. (MM V-3)

The property shall be recognized as a physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other buildings, shall not be undertaken. (MM V-4)

Most properties change over time; those changes that have acquired significance in their own right shall be retained and preserved. (MM V-5)

Distinctive features, finishes and construction techniques or examples of skilled craftsmanship which characterize the historic property shall be preserved. (MM V-6)

New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment. (MM V-10)

New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired. (MM V-11)

The Golden State Building, site features, and parking lot guard building shall be documented according to HABS Level II standards prior to any demolition, abatement or rehabilitation work. The negatives and archival quality prints should be donated to the Los Angeles Public Library. HABS Level II documentation shall consist of the following: short form architectural and historical narrative; archival drawings; if adequate archival drawings are not available, measured drawings will be produced; and large format photography with views of all exterior elevations for each building, important features, key spatial relationships among buildings, and exterior hardscape features. (MM V-12)

A structural engineer with qualifications in completing historic preservation projects that conform to the Secretary of the Interior's Standards for Rehabilitation shall be consulted and provide monitoring and written review of the engineering and construction of work that is on-site and contiguous with historic resources that are to remain. Particular attention shall be paid with regard to the demolition of buildings and landscaping, shoring, excavation, Project construction below and above grade near and attached to the on-site historic resources, and temporary shoring to mitigate weaknesses of interior removals and additional seismic risk that occur only during the construction phase.

A professional with who has completed historic preservation projects that conform to the Secretary of the Interior's Standards for Rehabilitation and that meets the Secretary of the Interior's Professional Qualifications Standards for Historic Architect (36 CFR 61) shall be consulted and provide monitoring and written review of the work that is related to historic preservation.

14. **Cultural Resources (Arcaeological).** If any archaeological materials are encountered during the course of the project development, the project shall be halted. The services of an archaeologist shall be secured by contacting the Center for Public Archaeology - Cal State University Fullerton, or a member of the Society of Professional Archaeologist (SOPA) or a SOPA-qualified archaeologist to assess the resources and evaluate the impact. Copies of the archaeological survey, study or report shall be submitted to the South Central Coastal Information Center at Cal State Fullerton (SCCIC). A covenant and agreement shall be recorded prior to obtaining a grading permit.
15. **Cultural Resources (Paleontological).** If any paleontological materials are encountered during the course of the project development, the project shall be halted. The services of a paleontologist shall be secured by contacting the Center for Public Paleontology - USC, UCLA, Cal State Los Angeles, Cal State Long Beach, or the Los Angeles County Natural History Museum to assess the resources and evaluate the impact. Copies of the paleontological survey, study or report shall be submitted to the Los Angeles County Natural History Museum. A covenant and agreement shall be recorded prior to obtaining a grading permit.

16. **Cultural Resources (Human Remains).** If human remains are discovered at the project site during construction, work at the construction site shall be suspended, and the City of L.A. Public Works Department and County Coroner shall be immediately notified. If the remains are determined by the County Coroner to be Native American, the Native American Heritage Commission (NAHC) shall be notified within 24 hours, and the guidelines of the NAHC shall be adhered to in the treatment of disposition of the remains.

17. **Geology and Soils.**

Seismic. The design and construction of the project shall conform to the California Building Code seismic standards as approved by the Department of Building and Safety. (MM VI-1)

Erosion/Grading/Short-term Construction Impacts.

The applicant shall provide staked signage at the site with a minimum of 3-inch lettering containing contact information for the Senior Street Use Inspector (Department of Public Works), the Senior Grading Inspector (LADBS) and the hauling or general contractor. (MM VI-2)

Chapter IX, Division 70 of the Los Angeles Municipal Code addresses grading, excavations, and fills. All grading activities require grading permits from the Department of Building and Safety. Additional provisions are required for grading activities within Hillside areas. The application of BMPs includes but is not limited to the following mitigation measures:

a. Excavation and grading activities shall be scheduled during dry weather periods. If grading occurs during the rainy season (October 15 through April 1), diversion dikes shall be constructed to channel runoff around the site. Channels shall be lined with grass or roughened pavement to reduce runoff velocity.

b. Stockpiles, excavated, and exposed soil shall be covered with secured tarps, plastic sheeting, erosion control fabrics, or treated with a bio-degradable soil stabilizer. (MM VI-3)

18. **Hazards and Hazardous Materials.**

Asbestos. Prior to the issuance of any permit for the demolition or alteration of the existing structure(s), the applicant shall provide a letter to the Department of Building and Safety from a qualified asbestos abatement consultant indicating that no Asbestos-Containing Materials (ACM) are present in the building. If ACMs are found to be present, it will need to be abated in compliance with the South Coast Air Quality Management District's Rule 1403 as well as all other applicable State and Federal rules and regulations. (MM VIII-1)

Lead Paint. Prior to issuance of any permit for the demolition or alteration of the existing structure(s), a lead-based paint survey shall be performed to the written satisfaction of the Department of Building and Safety. Should lead-based paint materials be identified, standard handling and disposal practices shall be implemented pursuant to OSHA regulations. (MM VIII-2)

Polychlorinated Biphenyl. Prior to issuance of a demolition permit, a polychlorinated biphenyl (PCB) abatement contractor shall conduct a survey of the project site to identify and assist with compliance with applicable state and federal rules and regulation governing PCB removal and disposal. (MM VIII-3)

Methane Gas.

All commercial, industrial, and institutional buildings shall be provided with an approved Methane Control System, which shall include these minimum requirements; a vent system and gas-detection system which shall be installed in the basements or the lowest floor level on grade, and within under-floor space of buildings with raised foundations. The gas-detection system shall be designed to automatically activate the vent system when an action level equal to 25% of the Lower Explosive Limit (LEL) methane concentration is detected within those areas. (MM VIII-4)

All commercial, industrial, institutional and multiple residential buildings covering over 50,000 square feet of lot area or with more than one level of basement shall be independently analyzed by a qualified engineer, as defined in Section 91.7102 of the Municipal Code, hired by the building owner. The engineer shall investigate and recommend mitigation measures which will prevent or retard potential methane gas seepage into the building. In addition to the other items listed in this section, the owner shall implement the engineer's design recommendations subject to Department of Building and Safety and Fire Department approval. (MM VIII-5)

19. Human Health (Vector Control).

Trash and garbage collection bins shall be maintained in good condition and repair such that there are no holes or points of entry through which a rodent could enter. (MM VIII-9)

Trash and garbage collection containers shall be emptied a minimum of once per week. (MM VIII-10)

Trash and garbage bin collection areas shall be maintained free from trash, litter, garbage, and debris. (MM VIII-11)

20. Emergency Evacuation Plan. Prior to the issuance of a building permit, the applicant shall develop an emergency response plan in consultation with the Fire Department. The emergency response plan shall include but not be limited to the following: mapping of emergency exits, evacuation routes for vehicles and pedestrians, location of nearest hospitals, and fire departments. (MM VIII-12)

21. Noise (Demolition, Grading, and Construction Activities).

The project shall comply with the City of Los Angeles Noise Ordinance No. 144,331 and 161,574, and any subsequent ordinances, which prohibit the emission or creation of noise beyond certain levels at adjacent uses unless technically infeasible. (MM XII-1)

Construction and demolition shall be restricted to the hours of 7:00 am to 6:00 pm Monday through Friday, and 8:00 am to 6:00 pm on Saturday. (MM XII-2)

Demolition and construction activities shall be scheduled so as to avoid operating several pieces of equipment simultaneously, which causes high noise levels. (MM XII-3)

The project contractor shall use power construction equipment with state-of-the-art noise shielding and muffling devices. (MM XII-4)

22. Hydrology and Water Quality.

In the event a permanent dewatering system is necessary, the Department of Building and Safety requires the following measures to mitigate the impacts to a less than significant level:

(a) Prior to the issuance of any permit for excavation, the applicant shall, in consultation with the Department of Building and Safety, submit a Dewatering Plan to the decision-maker for review and approval. Such plan shall indicate estimates for how much water is anticipated to be pumped and how the extracted water will be utilized and/or disposed of.

(b) Extracted groundwater shall be pumped to a beneficial on-site use such as, but not limited to: 1) landscape irrigation; 2) decorative fountains or lakes; 3) toilet flushing; or 4) cooling towers.

(c) Return water to the groundwater basin by an injection well. (MM IX-1)

Leaks, drips and spills shall be cleaned up immediately to prevent contaminated soil on paved surfaces that can be washed away into the storm drains. (MM IX-2)

All vehicle/equipment maintenance, repair, and washing shall be conducted away from storm drains. All major repairs shall be conducted off-site. Drip pans or drop clothes shall be used to catch drips and spills. (MM IX-3)

Pavement shall not be hosed down at material spills. Dry cleanup methods shall be used whenever possible. (MM IX-4)

Dumpsters shall be covered and maintained. Uncovered dumpsters shall be placed under a roof or be covered with tarps or plastic sheeting. (MM IX-5)

Applicants must meet the requirements of the Standard Urban Stormwater Mitigation Plan (SUSMP) approved by Los Angeles Regional Water Quality Control Board, which may include the following (a copy of the SUSMP can be downloaded at: <http://www.swrcb.ca.gov/rwqcb4/>):

Project applicants are required to implement stormwater BMPs to treat and infiltrate the runoff from a storm event producing $\frac{3}{4}$ inch of rainfall in a 24 hour period. The design of structural BMPs shall be in accordance with the Development Best Management Practices Handbook Part B Planning Activities. A signed certificate from a California licensed civil engineer or licensed architect that the proposed BMPs meet this numerical threshold standard is required.

Post development peak stormwater runoff discharge rates shall not exceed the estimated pre-development rate for developments where the increase peak stormwater discharge rate will result in increased potential for downstream erosion.

Concentrate or cluster development on portions of a site while leaving the remaining land in a natural undisturbed condition.

Maximize trees and other vegetation at each site by planting additional vegetation, clustering tree areas, and promoting the use of native and/or drought tolerant plants.

Any connection to the sanitary sewer must have authorization from the Bureau of Sanitation.

Cut and fill slopes in designated hillside areas shall be planted and irrigated to prevent erosion, reduce run-off velocities and to provide long-term stabilization of soil. Plant materials include: grass, shrubs, vines, ground covers, and trees.

Incorporate appropriate erosion control and drainage devices, such as interceptor terraces, berms, vee-channels, and inlet and outlet structures, as specified by Section 91.7013 of the Building Code. Protect outlets of culverts, conduits or channels from erosion by discharge velocities by installing a rock outlet protection. Rock outlet protection is physical device

composed of rock, grouted riprap, or concrete rubble placed at the outlet of a pipe. Install sediment traps below the pipe-outlet. Inspect, repair, and maintain the outlet protection after each significant rain.

All storm drain inlets and catch basins within the project area must be stenciled with prohibitive language (such as NO DUMPING - DRAINS TO OCEAN) and/or graphical icons to discourage illegal dumping.

Signs and prohibitive language and/or graphical icons, which prohibit illegal dumping, must be posted at public access points along channels and creeks within the project area. Legibility of stencils and signs must be maintained.

Materials with the potential to contaminate stormwater must be:

- (1) placed in an enclosure such as, but not limited to, a cabinet, shed, or similar stormwater conveyance system; or
- (2) protected by secondary containment structures such as berms, dikes, or curbs.

The storage area must be paved and sufficiently impervious to contain leaks and spills.

The storage area must have a roof or awning to minimize collection of stormwater within the secondary containment area.

The owner(s) of the property will prepare and execute a covenant and agreement (Planning Department General form CP-6770) satisfactory to the Planning Department binding the owners to post construction maintenance on the structural BMPs in accordance with the Standard Urban Stormwater Mitigation Plan and or per manufacturer's instructions.

Trash container areas must have drainage from adjoining roofs and pavement diverted around the area(s).

Trash container areas must be screened or walled to prevent off-site transport of trash.

Reduce impervious land coverage of parking lot areas.

Infiltrate runoff before it reaches the storm drain system.

Runoff must be treated prior to release into the storm drain. Three types of treatments are available, (1) dynamic flow separator; (2) a filtration or (3) infiltration. Dynamic flow separator uses hydrodynamic force to remove debris, and oil and grease, and are located underground. Filtration involves catch basins with filter inserts. Filter inserts must be inspected every six months and after major storms, cleaned at least twice a year. Infiltration methods are typically constructed on-site and are determined by various factors such as soil types and groundwater table.

Prescriptive Methods detailing BMPs specific to this project category are available. Applicants are encouraged to incorporate the prescriptive methods into the design plans. These Prescriptive Methods can be obtained at the Public Counter or downloaded from the City's website at: www.lastormwater.org. (MM IX-6)

23. Noise (Parking Wall, Parking Structure Ramps).

A 6-foot-high solid decorative masonry wall adjacent to residential use and/or zones shall be constructed if no such wall exists. (MM XII-5)

Concrete, not metal, shall be used for construction of parking ramps. (MM XII-6)

The interior ramps shall be textured to prevent tire squeal at turning areas. (MM XII-7)

Parking lots located adjacent to residential buildings shall have a solid decorative wall adjacent to the residential. (MM XII-8)

24. Public Services.

Construction Activity Near Schools.

The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety. (MM XIV-1)

There shall be no staging or parking of construction vehicles, including vehicles to transport workers on any of the streets adjacent to the school. (MM XIV-2)

Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on these streets during school hours. (MM XIV-3)

The developer and contractors shall maintain ongoing contact with administrators of 24th Street Elementary, Pomeroy Widney High School, and Animo Pat Brown Charter High School. The administrative offices shall be contacted when demolition, grading and construction activity begin on the project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323)342-1400 and guarantee that safe and convenient pedestrian and bus routes to the school be maintained. (MM XIV-4)

Schools Affected By Haul Route.

Haul route scheduling shall be sequenced to minimize conflicts with pedestrians, school buses and cars at the arrival and Section IV. Explanation of Checklist Determination dismissal times of the school day. Haul route trucks shall not be routed past the school during periods when school is in session especially when students are arriving or departing from the campus. (MM XIV-5)

LADBS shall assign specific haul route hours of operation based upon 24th Street Elementary, Pomeroy Widney High School, and Animo Pat Brown Charter High School's hours of operation. (MM XIV-6)

Public Services (Schools). The applicant shall pay school fees to the Los Angeles Unified School District to offset the impact of additional student enrollment at schools serving the project area. (MM XIV-7)

25. Transportation (Haul Route).

The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety. (MM XVI-1)

Projects involving the import/export of 20,000 cubic yards or more of dirt shall obtain haul route approval by the Department of Building and Safety. (MM XVI-2)

All haul route hours shall be limited to off-peak hours as determined by Board of Building and Safety Commissioners. (MM XVI-3)

The Department of Transportation shall recommend to the Building and Safety Commission Office the appropriate size of trucks allowed for hauling, best route of travel, the appropriate number of flag people. (MM XVI-4)

The Department of Building and Safety shall stagger haul trucks based upon a specific area's capacity, as determined by the Department of Transportation, and the amount of soil proposed to be hauled to minimize cumulative traffic and congestion impacts. (MM XVI-5)

The applicant shall be limited to no more than two trucks at any given time within the site's staging area. (MM XVI-6)

26. Safety Hazards.

The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety. (MM XVI-7)

The applicant shall submit a parking and driveway plan that incorporates design features that reduce accidents, to the Bureau of Engineering and the Department of Transportation for approval. (MM XVI-8)

27. Utilities (Local Water Supplies - All New Construction).

All restroom faucets shall be of a self-closing design. (MM XVII-5)

Prior to the issuance of any demolition or construction permit, the applicant shall provide a copy of the receipt or contract from a waste disposal company providing services to the project, specifying recycled waste service(s), to the satisfaction of the Department of Building and Safety. The demolition and construction contractor(s) shall only contract for waste disposal services with a company that recycles demolition and/or construction-related wastes. (MM XVII-6)

To facilitate on-site separation and recycling of demolition- and construction-related wastes, the contractor(s) shall provide temporary waste separation bins on-site during demolition and construction. These bins shall be emptied and the contents recycled accordingly as a part of the project's regular solid waste disposal program. (MM XVII-7)

All waste shall be disposed of properly. Use appropriately labeled recycling bins to recycle demolition and construction materials including: solvents, water-based paints, vehicle fluids, broken asphalt and concrete, bricks, metals, wood, and vegetation. Non recyclable materials/wastes shall be taken to an appropriate landfill. Toxic wastes must be discarded at a licensed regulated disposal site. (MM XVII-8)

C. Administrative Conditions of Approval.

28. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review or approval, plans, etc., as may be required by the subject conditions, shall be provided to the Department of City Planning for placement in the subject file.
20. **Code Compliance.** Area, height and use regulations of the zone classification of the subject property shall be complied with, except where herein conditions are more restrictive.
30. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assigns. The agreement shall be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the file.
31. **Definition.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public offices, legislation or their successors, designees or amendment to any legislation.
32. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning and any designated agency, or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.
33. **Building Plans.** Page 1 of the grant and all the conditions of approval shall be printed on the building plans submitted to the Department of City Planning and the Department of Building and Safety.
34. **Indemnification.** The applicant shall defend, indemnify and hold harmless the City, its agents, officers, or employees from any claim, action, or proceeding against the City or its agents, officers, or employees to attack, set aside, void or annul this approval which action is brought within the applicable limitation period. The City shall promptly notify the applicant of any claim, action, or proceeding and the City shall cooperate fully in the defense. If the City fails to promptly notify the applicant of any claim, action or proceeding, or if the City fails to cooperate fully in the defense, the applicant shall not thereafter be responsible to defend, indemnify, or hold harmless the City.

CONDITIONS FOR EFFECTUATING [T] TENTATIVE CLASSIFICATION REMOVAL

Pursuant to Los Angeles Municipal Code Section 12.32 G, the [T] Tentative Classification shall be removed by the recordation of a final tract map or by posting of guarantees through the B-permit process of the City Engineer to secure the following without expense to the City of Los Angeles, with copies of any approvals or guarantees provided to the Planning Department for attachment to the subject City Plan case file.

Dedication(s) and Improvements. Prior to the issuance of any building permits, public improvements and dedications for streets and other rights-of-way adjoining the subject property shall be guaranteed to the satisfaction of the Bureau of Engineering, Department of Transportation, Fire Department (and other responsible City, regional, and Federal government agencies, as may be necessary).

Responsibilities/Guarantees.

- 1.) As part of early consultation, plan review, and/or project permit review, the applicant/developer shall contact the responsible agencies to ensure that any necessary dedications and improvements are specifically acknowledged by the applicant/developer.
- 2.) Prior to issuance of sign-offs for final site plan approval and/or project permits by the Department of City Planning, the applicant/developer shall provide written verification to the Department of City Planning from the responsible agency acknowledging the agency's consultation with the applicant/developer. The required dedications and improvements may necessitate redesign of the project. Any changes to the project design required by a public agency shall be documented in writing and submitted for review by the Department of City Planning.

A. Dedications Required:

Western Avenue (Major Highway – Class II). A 12-foot wide and variable width strip of land along the property frontage except at the location of the existing building to remain near the intersection with Adams boulevard to complete a 52-foot half right-of-way in accordance with Major Highway – Class II standards, including a 15-foot by 15-foot cut corner at the intersection with Adams Boulevard, a certified survey map showing the existing buildings and areas to be dedicated shall be submitted along with the dedication application. In addition, submit a legal description describing the area to be dedicated with the dedication application.

Hobard Boulevard (Local Street) – None.

Adams Boulevard (Scenic Major Highway – Class II). None at this time. The existing building to remain precludes the required dedication.

B. Improvements Required:

Western Avenue – Construct additional surfacing to provide a 40-foot half roadway in accordance with Major Highway-Class II standards, including asphalt pavement, integral

concrete curb, 2-foot gutter and a 12-foot wide full width concrete sidewalk. At the location of the existing building to remain, repair any broken, off-grade or bad order concrete curb, gutter, sidewalks and close any unused driveways. These improvements should suitably transition and join the existing improvements.

Hobart Boulevard – Repair any broken, off-grade or bad order concrete curb, gutters and sidewalks and close any unused driveways adjoining the property.

Adams Boulevard - Repair any broken, off-grade or bad order concrete curb, gutters and sidewalks and close any unused driveways adjoining the property.

Sewer lines exist in Western Avenue, Hobart Boulevard and Adams Boulevard. Extension of the 6-inch house connection laterals to the new property line may be required. All Sewerage Facilities Charges and Bonded Sewer Fees are to be paid prior to obtaining a building permit.

An investigation by the Bureau of Engineering Central District Office Sewer Counter may be necessary to determine the capacity of the existing public sewers to accommodate the proposed development. Submit a request to the Central District Office of the Bureau of Engineering (213) 482-7050.

Roof drainage and surface run-off from the property shall be collected and treated at the site and drained to the street through drain pipes constructed under the sidewalk and through curb drains or connections to the catch basins.

Street Lighting: Installation of street lights to the satisfaction of the Bureau of Street Lighting. If new street light(s) are required (satisfactory to the Bureau of Street Lighting (213) 847-1551), the property within the boundary of the development shall be formed or annexed into a Street Lighting Maintenance Assessment District prior to final recordation or issuance of the Certificate of Occupancy.

Install tree wells with root barriers and plant street trees satisfactory to the City Engineer and the Urban Forestry Division of the Bureau of Street Services. Some tree removal in conjunction with the street improvement project may require Board of Public Works Approval. The applicant should contact the Urban Forestry Division for further information (213) 847-3077.

Trees: Board of Public Works approval shall be obtained prior to the issuance of the Certificate of Occupancy of the development project, for the removal of any tree in the existing or proposed public right-of-way area associated with the improvement requirement outlined herein. The Bureau of Street Services Urban Forestry Division is the lead agency for obtaining Board of Public Works approval for the removal of such trees.

Submit a parking area and driveway plan to the Central District Office of the Bureau of Engineering and the Department of Transportation for review and approval.

Submit shoring and lateral support plans to the Bureau of Engineering Excavation Counter for review and approval prior to excavating adjacent to the right-of-way.

Notes:

The Department of Transportation may have additional requirements for dedications and improvements.

Relocate traffic signs, equipment and parking meters to the satisfaction of the Department of Transportation (213) 482-7024.

Refer to the Department of Water and power regarding power pole (213) 367-2715.

Refer to the Fire Department regarding fire hydrants (213) 482-6543.

Notice. If conditions dictate, connections to the public sewer system may be postponed until adequate capacity is available.

Notice. Certificates of Occupancies for the subject property will not be issued by the City until the construction of all the public improvements (streets, sewers, storm drains, etc.), as required herein, are completed to the satisfaction of the City Engineer.

FINDINGS

1. **General Plan/Charter Findings.** The subject property is located within the South Los Angeles Community Plan, which was adopted by the City Council on March 22, 2000 (City Planning Case No. 96-0399 CPR). The South Los Angeles Community Plan map designates the subject property as Community Commercial (Corresponding Zones include CR, C2, C4, RAS3 and RAS4) and Low Medium II Residential (Corresponding Zones include RD1.5, RD2, RW2 and RZ2,5). The subject site contains dual zoning of RD1.5-1 and C2-1, with General Plan Land Use (GPLU) designations of Low Medium II Residential and Community Commercial.

2. **City Charter Sections 556 and 558.** The recommended General Plan Amendment from Low Medium II Residential to Community Commercial (for a portion of the site) and to amend Footnote Number 1 of the South Los Angeles Community Plan to exclude the project site from the Height District 1 (1.5:1 Floor Area Ratio) limitation (applicable to all properties with the Community Commercial General Plan Land Use designation), will create a site with a single General Plan Land Use designation of Community Commercial and zoning of C2-2D. The recommended General Plan Amendment **does comply** with Charter Sections 556 and 558 in that the recommended amendment reflects the land use patterns, trends and uses in the immediate area and does further the intent, purposes and objectives of the South Los Angeles Community Plan.

The proposed office building would be constructed on a site that has, for over 50 years, been developed with a commercial surface parking lot serving the existing Golden State Mutual Life Insurance Building (Historic Cultural Monument No. LA-1000, 1999 W. Adams Boulevard, located at the northeast corner of Adams Boulevard and Western Avenue and Listed 6/11/11). The property is located within the South Los Angeles Community Plan, the CRA/LA Normandie 5 Redevelopment Project Area, and is to the east (across Western Avenue) of the West Adams Terrace Historic Preservation Overlay Zone area. The Redevelopment Plan provides that the floor area ratio (FAR) applicable to property designated for commercial use shall not exceed 2.0:1 (the recommended maximum permitted FAR shall be 2:1 through approval of a Development Limitation). The proposed new office building would be occupied by the South Central Los Angeles Regional Center ("SCLARC"), and would serve as SCLARC's headquarters. SCLARC is a private, non-profit, community based organization which contracts with the State Department of Developmental Services (DDS) to coordinate services for individuals with developmental disabilities and their families.

The South Los Angeles Community Plan contains the following applicable Goals, Objectives Policies and Programs:

GOAL 2. A STRONG AND COMPETITIVE COMMERCIAL SECTOR WHICH BEST SERVES THE NEEDS OF THE COMMUNITY THROUGH MAXIMUM EFFICIENCY AND ACCESSIBILITY WHILE PRESERVING THE HISTORIC COMMERCIAL AND CULTURAL CHARACTER OF THE DISTRICT.

Objective 2-1 To conserve and strengthen viable commercial development.

- Policy 2-1.1** New commercial uses shall be located in existing, established commercial areas or existing shopping centers.
- Policy 2-1.3** Commercial areas should be consolidated and deepened to stimulate existing businesses, create opportunities for new development and off-street parking, expand the variety of goods and services, and improve shopping convenience as well as offer local employment.
- Policy 2-1.5** Require that projects be designed and developed to achieve a high level of quality, distinctive character, and compatibility with existing uses and development.

The proposed office building would be constructed on a portion of the site that is both zoned and planned for medium density residential use along the eastern half, and commercial use along the western half, but that had been developed with a surface parking lot serving the existing office building (located at the northeast corner of Adams Boulevard and Western Avenue) for approximately 50 years. The requested General Plan Amendment would re-designate for commercial/office use, that portion of the site presently designated for residential uses, and would bring the entire site under a single zone (C2-2D) and GPLU (Community Commercial). Having a unified Zone and General Plan designation on the site, support both the development of site in an orderly manner and is consistent with the Community Plan's policy to consolidate and deepen commercial areas to create opportunities for new development. In order to achieve a high level of quality and distinctive character in its design, and to ensure that it would be compatible with existing uses and development, the project would be designed incorporating applicable design guidelines as set forth in the South Los Angeles Community Plan, and per the recommendation(s) of the City of Los Angeles Office of Historic Resources/Cultural Heritage Commission.

- Objective 2-3** To attract uses which strengthen the economic base and expand market opportunities for existing and new businesses.

The recommended General Plan Amendment and Vesting Zone Change will support the development of the proposed office/parking use, the construction of which, would allow the proposed tenant, the South Los Angeles Regional Center ("SCLARC"), to expand its services while remaining within the South Los Angeles Community Plan area, where many of its users are located, while increasing parking on the site.

- Objective 2-5** To enhance the appearance of commercial districts.

- Policy 2-5.1** Improve the appearance and landscaping of commercial properties.
- Policy 2-5.2** Preserve community character, scale and architectural diversity.
- Policy 2-5.3** Improve safety and aesthetics of parking areas in commercial areas.

The proposed project would be consistent with this objective and these policies in that it would replace a surface parking lot with the new office building and parking structure. The proposed abundant landscaping and courtyard area, will significantly improve the appearance of the property. The project would be designed to incorporate applicable design guidelines from the Community Plan, including screening the parking from public

views, and providing a clearly defined pedestrian entrance and courtyard along the Western Avenue frontage. Furthermore, the parking structure would provide increased safety as compared to the existing surface lot in that it would provide adequate security lighting and other measures, including surveillance cameras and on-site security personnel. As indicated above, the new construction will be designed to support and complement the existing historic Golden Mutual Insurance Company building (HCM-1000).

Objective 2-6 To maintain and increase the commercial employment base for community residents whenever possible.

Policy 2-6.1 Protect commercial plan designations so that commercial development is encouraged.

The proposed project would be consistent with this objective and policy. The requested Zone and Height District Change to C2-2D and General Plan Amendment from Low Medium II Residential to Community Commercial will support development of a larger and updated office use at the scale needed for the SCLARC to remain within the community where it has operated nearby for many years, and will enable the tenant to provide additional services for the community and to expand its employee base.

Policy 11-1.2 New development projects should be designated to minimize disturbance to existing traffic flow with proper ingress and egress to parking.

Policy 11-2.1 No increase in density and intensity shall be effectuated by zone change, variance, conditional use, parcel map or subdivision unless it is determined that the transportation system can accommodate the increased traffic generated by the project.

Policy 11-2.2 Require new development projects to mitigate off-site traffic impacts to the maximum extent feasible.

Policy 11-2.3 Require that driveway access points onto major and secondary highways, arterials, and collector streets be limited in number and be located to ensure the smooth and safe flow of vehicles and bicycles.

Policy 14-1.1 Encourage non-residential development to provide employee incentives for utilizing alternatives to the automobile (i.e., carpools, vanpools, buses, flex time, bicycles, and walking, etc.).

Policy 14-1.3 Require the proposals for major new non-residential development projects include submission of a TDM Plan to the City.

Policy 16-1.4 Encourage the provision of showers, changing rooms and bicycle storage at new and existing non-residential developments and public places.

The proposed project would be consistent with these policies. The ingress to the parking area would be provided from Hobart Boulevard as it has since the property was first developed with the existing use. No driveway access would be provided to or from Western Avenue, a designated Major Highway. The project would provide bicycle

parking and shower facilities for employees commuting by bicycle, and a Transportation Demand Management Plan, including designated carpool space and on-site information on available transit, would be implemented by the project, as required by the Los Angeles Municipal Code.

Policy 17-1.1 Consolidate parking, where appropriate, to eliminate the number of ingress and egress points onto arterials.

Policy 17-1.5 New parking lots and new parking garages shall be developed in accordance with design standards.

The access to the proposed parking structure would be provided from a single driveway from Hobart Boulevard, consistent with the use and operation of the property for more than 50 years. The parking structure would be subterranean and the above ground parking areas would not be visible from surrounding properties as they will be buffered with screening (plant and architectural).

GOAL 19 ***A COMMUNITY WHICH PRESERVES AND RESTORES THE MONUMENTS, CULTURAL RESOURCES, NEIGHBORHOODS, AND LANDMARKS WHICH HAVE HISTORICAL AND/OR CULTURAL SIGNIFICANCE.***

Objective 19-1 To ensure that the Plan Area's significant cultural and historical resources are protected, preserved, and/or enhanced.

Policy 19-1.2 Identify all designated City of Los Angeles Historic and Cultural Monuments in order to foster public appreciation of the City of Los Angeles' valuable historic resources and to promote education of the public by preserving Los Angeles' historic past and to promote that any other appropriate landmarks of unique architectural and historical significance continue to be identified for the purpose of inclusion in the list.

Objective 19-2 To protect and enhance historic and architectural resources in commercial areas in a manner that will encourage revitalization and investment in these areas.

Policy 19-2.1 Encourage the preservation, maintenance, enhancement and adaptive reuse of existing buildings in commercial areas through the restoration of original facades and the design of new construction which complements old in a harmonious fashion, enhancing the historic pattern.

Objective 19-4 To encourage private owners of historic properties/resources to conserve the integrity of such resources.

Policy 19-4.1 To assist private owners of historic resources to maintain and/or enhance their properties in a manner that will preserve the integrity of such resources in the best possible condition.

Program: Adherence to the City's historic properties preservation ordinances and the Cultural Heritage board requirements for preservation and design, implementation of the design standards.

The proposed office use is located on the site of a designated Historic Cultural Monument, The Golden State Mutual Life Insurance Company Building (Historic Cultural Monument No. LA-1000, 1999 W. Adams Boulevard, Listed 6/11/11). The proposed office/parking building is located on a portion of the site that contained surface parking for the cultural resource. The proposed building will provide parking for both the existing building and for the new use. The Applicant has proposed a "courtyard area" which will physically connect the two buildings and provide a monument to the architect of the original building, Mr. Paul Revere Williams. The new structure and courtyard area will be reviewed by the Office of Historic Resources for compatibility with the existing historic structure and site design.

3. Vesting Zone and Height District Change. Pursuant to Section 12.32-C of the Municipal Code, and based on these findings, the recommended action is deemed consistent with public necessity, convenience, general welfare, and good zoning practice.

The recommended Zone/Height District Change from RD1.5-1 and C2-1 to [T][Q]C2-2D is consistent with the proposed General Plan Land Use Designation of Community Commercial and is within the range of corresponding zone(s) (which include CR, C2, C4, RAS3 and RAS4) permitted by such. The subject site is located within the South Los Angeles Community Plan area at the Northeast corner of Western Avenue and Adams Boulevard and is presently developed with a five story office building, and surface parking.

The subject property is an L shaped site with dual zoning and General Plan Land Use (GPLU) designations. That portion of the lot fronting Western Avenue is zoned C2-1 and the portion fronting Hobart Boulevard is zoned RD1.5-1. The requested Zone Change is to rezone that portion of the property which currently contains surface parking from RD1.5-1 (C2-1 for the remainder of the site) to [Q]C2-2D. A Height District change has also been requested to change the Height District from Height District 1 (unlimited height/ maximum FAR of 1.5:1) to Height District 2D (unlimited height/maximum FAR of 6:1, D Limitation(s)) over the entire property. The subject D limitation will permit a maximum Floor Area Ratio of 2:1. In addition, a General Plan Amendment has been requested to change the existing GPLU designation from Low Medium II Residential (for the RD1.5-1 portion of the site) to Community Commercial, and that would remove the entire site from the applicability of Footnote No. 1 of the South Los Angeles Community Plan, which limits all properties with a General Plan Land Use designation of Community Commercial to Height District 1.

The property is surrounded by a variety of commercial (office and retail), institutional (educational and religious) and residential (multiple family, single family) uses in buildings ranging from one to five stories in height. The property abuts a one-story commercial center to the southeast, is adjacent to a five-story apartment building on the east side of Hobart Boulevard, a three-story, independent living and senior housing apartment building to the north, and a three-story Retirement and Care Center located on the west side of Western Avenue. At four-stories, the First African Methodist Episcopal Church's Assistance Corporation and Renaissance Center is located beyond, on the south side of Adams Boulevard. The five-story, historically designated (HCM-1000) Golden State

Mutual Life Insurance Company building is located at the southwest portion of the subject property.

Properties located to the north of the subject site are zoned: RD1.51; [Q]RD2-1 and C2-1 with commercially zoned properties facing Western Avenue and residentially zoned properties facing Hobart Boulevard. Properties to the south (across Adams Boulevard) are zoned: C2-1-O-HPOZ; C1-1VL-O; RD1.5-1-O; and R4-1-O. Properties to the immediate east are zoned: RD1.5-1 and C2-1, and to the west across Western Avenue are zoned: C2-1-O-HPOZ; R1-1-O-HPOZ; C2-1-O, P-1-O, PF-1-O and R4-1-O-HPOZ.

Height District 2 (HD-2) permits unlimited height and a maximum Floor Area Ratio (FAR) of 6:1 for C2 zoned properties. While there are currently no C2 zoned properties in the area designated as HD2, the requested Height District Change to 2D would not result in an introduction of the maximum permitted density, or FAR of 6:1. The proposed Development Limitation-"D" would limit the maximum permitted amount of floor area that could be constructed on the property to two times the area of the lot, or a FAR of 2:1. In addition, the subject D will be permanent and can only be removed through a subsequent Legislative Action (Zone/Height District Change).

The addition of the proposed 56,960 square foot building to the existing 54,234 square foot building (per ZIMAS) on the property would result in a total floor area of 111,194 square feet, and an FAR of approximately 1.68:1¹. Within the immediately surrounding area, there are properties developed at similar or higher FAR levels, including the apartment building immediately to the east with an FAR of 2.82:1 and the FAME Assistance Corporation and Renaissance Center to the south with an FAR of 1.89:1. Thus, the proposed change to Height District 2 with the imposition of the D Limitation (limiting maximum permitted FAR to 2:1), would not result in a building that would be out of scale and character with existing development within the immediate vicinity of the property.

The action, as recommended, has been made contingent upon compliance with the [T] and [Q] conditions imposed herein. Such limitations are necessary to protect the best interests of and to assure a development more compatible with surrounding properties, to secure an appropriate development in harmony with the General Plan, and to prevent or mitigate the potential adverse environmental effects of the subject recommended action.

4. Site Plan Review.

That the project is in substantial conformance with the purposes, intent and provisions of the General Plan, applicable community plan and any applicable specific plan.

The project site has a General Plan Land Use Designation of Community Commercial on the western portion and Low Medium II Residential on the eastern portion. The project is requesting a change from Low Medium II Residential to Community Commercial so that the entire property is designated for commercial use. The use proposed is consistent with South Los Angeles Community Plan's land use objectives and policies, which include conserving and strengthening viable commercial development, and consolidating and deepening commercial areas to stimulate existing businesses and create

¹ FAR calculated on buildable area of 66,325 square feet (70,763 square foot lot area less 177.5' x 25' (4,438 square foot) Building Line area along Hobart Boulevard. 111,194 square feet total building area / 66,325 square foot buildable area = FAR of 1.676:1.

opportunities for new development. Therefore, upon approval of the requested General Plan Amendment, the project would be consistent with the General Plan.

That the project consists of an arrangement of buildings and structures (including height, bulk and setbacks), off-street parking facilities, loading areas, lighting, landscaping, trash collections, and other such pertinent improvements, which is or will be compatible with existing and future development on the neighboring properties.

The project consists of an arrangement of buildings and structures that is consistent with its surroundings both in use and scale. The proposed three story building height and scale is compatible with several multi-story buildings in the area, including a five story apartment building on the east side of Hobart Boulevard, the three story FAME Arms independent living and senior housing apartment building to the north, the Western headquarters of the Brothers of St. John of God and the St. John of God Retirement and Care Center located on the west side of Western Avenue, and the five story, former Golden State Mutual Life Insurance Company building located at the southwest portion of the property.

The project features landscaped setback areas along Hobart Boulevard, Western Avenue, and the northerly property line, adjoining the FAME Arms building. Access to the new subterranean parking garage would be from Hobart Boulevard, similar to access currently provided to the existing surface parking lot. No vehicular access would be provided from Western Avenue to avoid interfering with the flow of traffic on this designated Major Highway. A service area would be provided in the center of the site between the existing and proposed building to eliminate any adverse effects from loading/unloading and trash collection activities upon residential uses located to the north of the site. Exterior lighting would be directional and focused on site, avoiding spillover effects on adjoining properties. Therefore, as demonstrated by the foregoing, the project would be compatible with existing and future development on the neighboring properties.

That any project containing residential uses provides its residents with appropriate type and placement of recreational facilities and service amenities in order to improve habitability for the residents and minimize impacts on neighboring properties where appropriate.

The project does not include any residential uses.

CEQA Findings

Mitigated Negative Declaration No. ENV-2010-329-MND has been amended as follows:

Mitigation Measure V-7: Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive historic feature, the new feature shall match the old in design, color, texture, and other visual qualities, and where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.

Mitigation Measure V-8: Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used.

Mitigation Measure V-9: The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.

The above proposed Mitigation Measures related to Cultural Resources (Designated Historic-Cultural Resource) have been removed. These measures are directly related to the physical preservation/restoration of a cultural resource. The proposed Project is the construction of an in-fill structure (office/parking building) located on the same site as the subject Historic Cultural Monument (HCM LA-1000). There is no physical expansion, restoration or alteration is proposed for the existing HCM.

A Mitigated Negative Declaration (ENV-2010-328-MND) was prepared for the proposed project. On the basis of the whole of the record before the lead agency including any comments received, the lead agency finds that, with imposition of the mitigation measures described in the MND, there is no substantial evidence that the proposed project will have a significant effect on the environment. The attached Mitigated Negative Declaration reflects the lead agency's independent judgment and analysis. The records upon which this decision is based are with the Environmental Review Section of the Planning Department in Room 750, 200 North Spring Street. I hereby adopt that the Mitigated Negative Declaration, imposed the conditions shown in that document on this approval.

PUBLIC HEARING AND COMMUNICATIONS

The public hearing on this matter was held Friday June 8, 2012 at the Los Angeles City Hall 200 North Spring Street, Los Angeles, California, 10th floor hearing room. There were approximately 30 persons present.

Comments presented by the Applicant and Applicant's Representatives were as follows:

- The proposed project represents a 31 million dollar investment in the site;
- The project will become the permanent home of the "South Central Los Angeles Regional Center" (SCLARC);
- Center provides program support/case management for development disabled persons;
- The proposed location aids in providing needed space for the organization and centralizes resources;
- New construction will be first new project in the area for quite some time, will be Cal-Green use and will provide for local hires;
- New use is located behind the existing Golden Mutual Insurance building and will include a landscape plaza as a link between the new use and the historic monument building (dedication to historic project architect Paul R. Williams will be included);
- The new structure will be located on a portion of the lot that is currently surface parking;
- The new project will include 4 levels of parking with over 300 spaces;
- The set-back along the Western Avenue frontage is approximately 30 feet to accommodate the proposed plaza area;
- The Western Avenue frontage will be pedestrian oriented with window boxes, a tower and a green-screen along the parking level façade;
- The proposed use/requested Zone and General Plan Amendment are consistent with the CRA/LA Redevelopment Plan;
- Requested General Plan Amendment would overlay Community Commercial over entire site;
- Height District change to HD-2 would meet consistency with redevelopment plan;
- Applicant is requesting "no improvements along Western Avenue frontage", as existing building limits ability to widen street to only immediately abutting new structure.

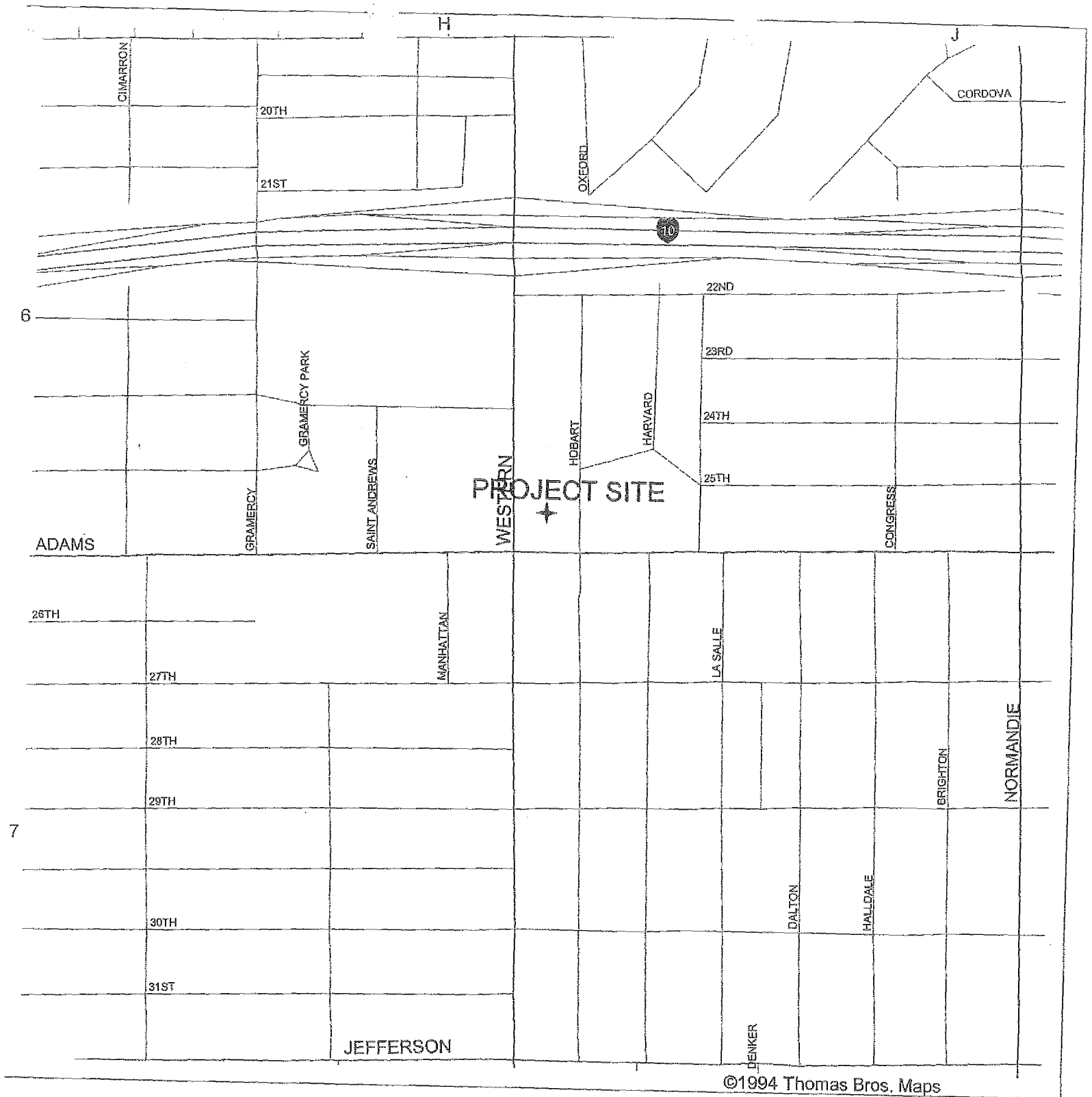
Comments in Support of Request:

- Request supports permanent home for SCLARC and redevelopment of the area;
- Actually first new project in area in 40 years;
- Will transform vacant, blighted parking area and will add to improvement of area;
- Approximately 250 member workforce is envisioned;
- Project conforms to Community Plan and will support community members and their families;
- The health, safety, and welfare of the community is fundamental and the SCLARC supports families and access to needed resources;
- The SCLARC supports both the physical and social needs of the community (consumers) and aids in developing independent users;
- Project should create economic impact of 33 million dollar capital improvement to area;
- 400+ jobs for local workforce;

- The project will create a permanent home for the SCLARC and a destination for consumers;
- Project will support the legacy of architect Paul Williams through dedication of the courtyard monument;
- This location will put access for users right in the community where it is needed;
- Project will also support/enhance historic aspects of the existing site;
- New location will allow for reduced wait times, increased access for users and build better communication between community and vendors;
- SCLARC's current location has parking/access issues (which limits resources);
- New facility will become community center for consumers;
- New site and facility will give better access and stability to SCLARC;
- New facility will allow for expansion of program needs and services such as transportation/access;
- Area is changing for the better and proposed project helps with improvement;
- Center is a great use of the property and supports historic community ties.
- For many families the SCLARC has been a real support base and support is well deserved.

Hearing Officer Comments

There was no testimony presented in opposition to the proposed project.



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VICINITY MAP

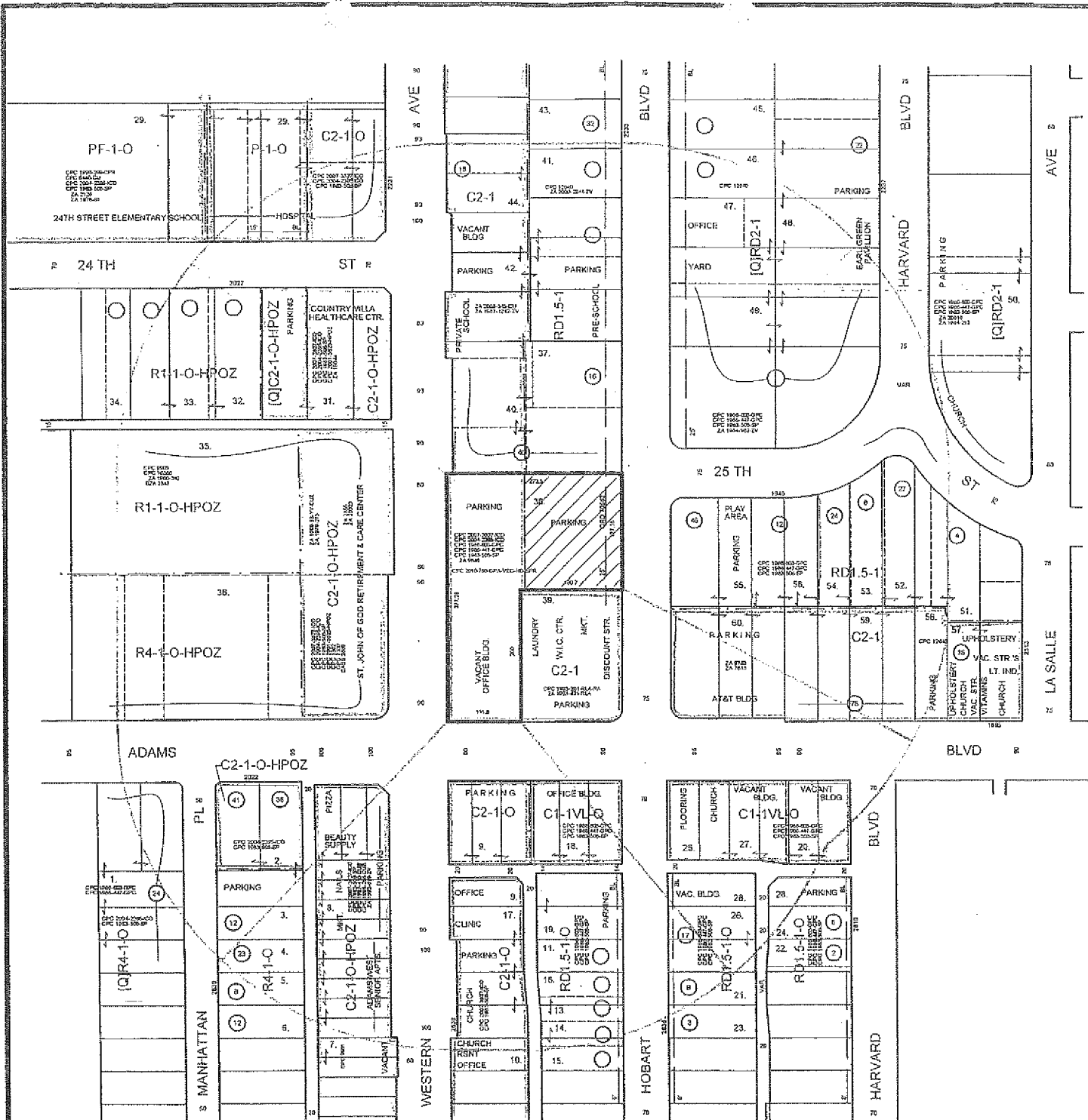
SITE : 1999 W. ADAMS BLVD.

CPC 2010-760

GC MAPPING SERVICE INC

3055 WEST VALLEY BOULEV/
 ALHAMBRA CA 91803
 (626) 441-1080, FAX (626) 441-
 GCMAPPING@RADIUSMAPS.C


EXHIBIT A-1
 VICINITY MAP
 CPC-2010-0760-GPA-VZC-HD-
 SPR



LEGAL: PORTION OF BLOCK 18, WEST ADAMS HEIGHTS, M.B. 2-53-54. (SEE APPLICATION)

**GENERAL PLAN AMENDMENT
VESTING ZONE CHANGE
HEIGHT DISTRICT CHANGE**

 RD1.5 TO C2

 HEIGHT DISTRICT 1 TO 2D FOR ENTIRE SITE.

C.D. 8
C.T. 2215.00
P.A. SOUTHEAST LOS ANGELES

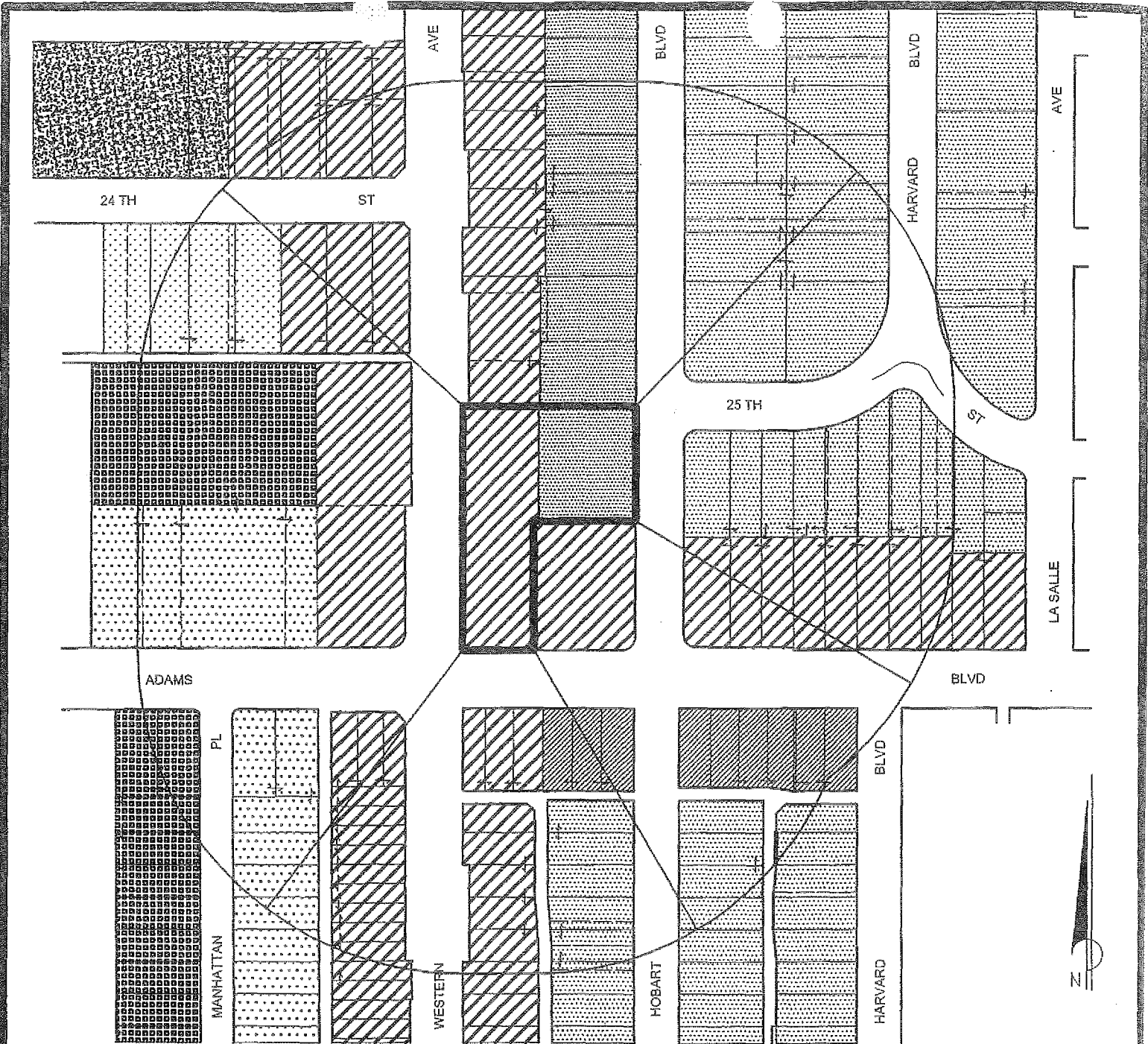


GC MAPPING SERVICE, INC.








3055 WEST VALLEY BOULEVARD
ALHAMBRA CA 91803
(626) 441-1080 FAX (626) 441-8850
gcmapping@radiusmaps.com

CASE NO.
DATE: 05-03-2012

**EXHIBIT A-2
RADIUS MAP
CPC-2010-0760-GPA-VZC-HD-**



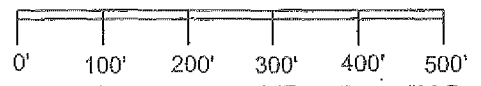
EXISTING - SOUTH LOS ANGELES COMMUNITY PLAN

-  LOW RESIDENTIAL - RE9, RS, R1, RU, RD6, RD5
-  LOW MEDIUM II RESIDENTIAL - RD1.5, RD2, RW2, RZ2.5
-  MEDIUM RESIDENTIAL - R3
-  HIGH MEDIUM RESIDENTIAL - R4
-  GENERAL COMMERCIAL - C1.5, C2, C4, RAS3, RAS4
-  COMMUNITY COMMERCIAL - CR, C2, C4, RAS3, RAS4
-  PUBLIC FACILITIES - PF

CASE NO.

DATE: 02 - 02 - 2010

SCALE:

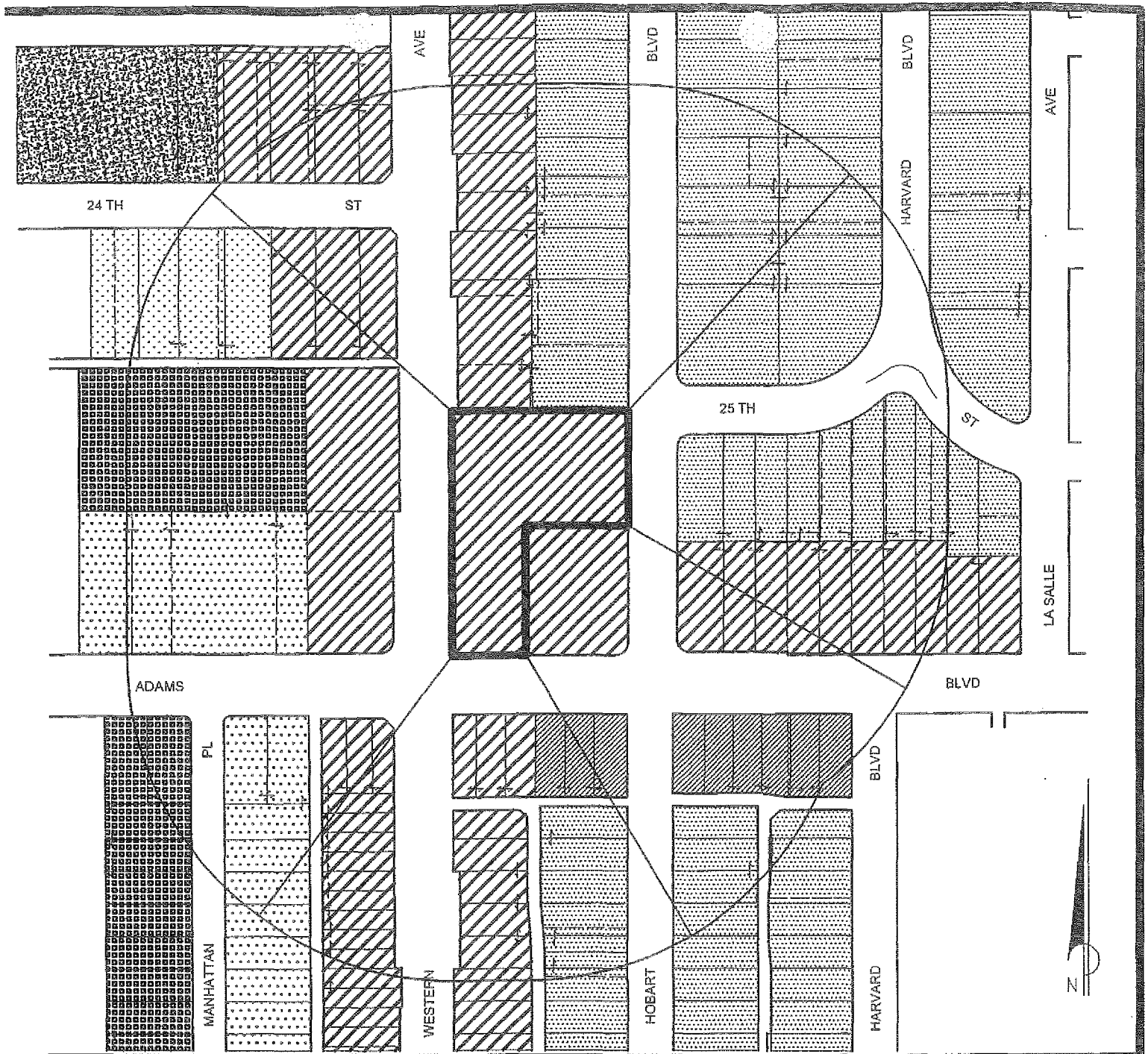


GC MAPPING SERVICE INC








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 ALHAMBRA CA 91803
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CPC 2010-76

**EXHIBIT A-3
 PLAN MAP - EXISTING
 CPC-2010-0760-GPA-VZC-HD-SPR**



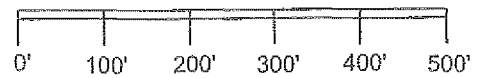
REQUESTED - SOUTH LOS ANGELES COMMUNITY PLAN

-  LOW RESIDENTIAL - RE9, RS, R1, RU, RD6, RD5
-  LOW MEDIUM II RESIDENTIAL - RD1.5, RD2, RW2, RZ2.5
-  MEDIUM RESIDENTIAL - R3
-  HIGH MEDIUM RESIDENTIAL - R4
-  GENERAL COMMERCIAL - C1.5, C2, C4, RAS3, RAS4
-  COMMUNITY COMMERCIAL - CR, C2, C4, RAS3, RAS4
-  PUBLIC FACILITIES - PF

CASE NO.

DATE: 02 - 02 - 2010

SCALE:

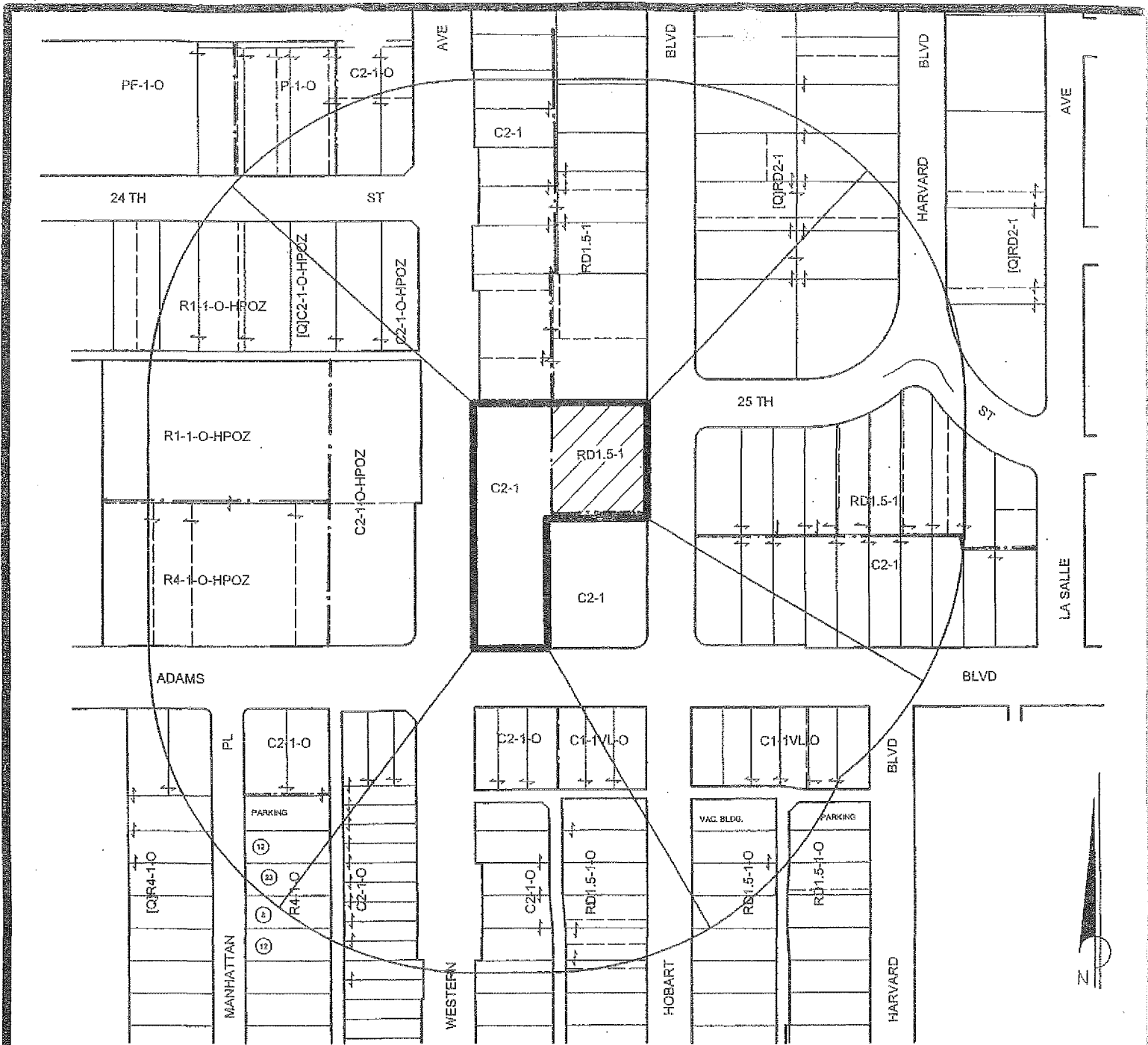


GC MAPPING SERVICE INC

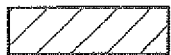
3055 WEST VALLEY BOULEVARD
 ALHAMBRA CA 91803
 OFF (626) 441-1080 FAX (626) 441-8850

CPC 2010-7

**EXHIBIT A-4
 PLAN MAP - REQUESTED
 CPC-2010-0760-GPA-VZC-HD-
 SPR**



EXISTING ZONING



RD1.5 TO C2



HEIGHT DISTRICT 1 TO 2D FOR ENTIRE SITE

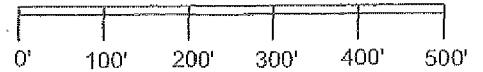


ZONE BOUNDARY LINE

CASE NO.

DATE: 02 - 02 - 2010

SCALE:



GC MAPPING SERVICE INC

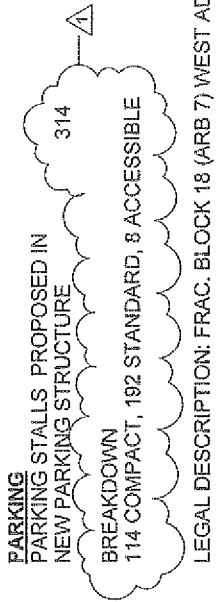
3055 WEST VALLEY BOULEVARD

ALHAMBRA CA 91803

OFF (626) 441-1080 FAX (626) 441-8850

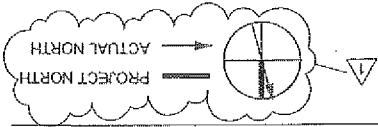
PROPOSED BUILDING & LOT TABLE

SITE AREA					
TOTAL SITE AREA	70,763 SF (1.624 ACRES)				
BUILDABLE AREA	66,325 SF				FLOOR AREA RATIO (FAR)
					66,325 SF
					96,760 SF
					1.46
LOT COVERAGE					
NEW BUILDING LOT COVERAGE	28,290 SF				
1999 ADAMS BLVD. (EXISTING)	9,412 SF				
TOTAL BUILDING COVERAGE	37,702 SF (56.8%)				
NEW PAVING	15,924 SF				
EXISTING PAVING	2,391 SF				
TOTAL PAVING (NEW)	18,315 SF (27.6%)				
LANDSCAPE & PLANTERS	10,308 SF (15.6%)				
LOT COVERAGE RATIO	0.56				
STRUCTURED AREA / SITE AREA					
BUILDING FLOOR AREAS					
NEW OFFICE BLDG FLOOR AREA	56,960 SF				
EXISTING 1999 ADAMS BLVD BLDG:	39,800 SF				
TOTAL NEW & EXISTING FLOOR AREA	96,760 SF				

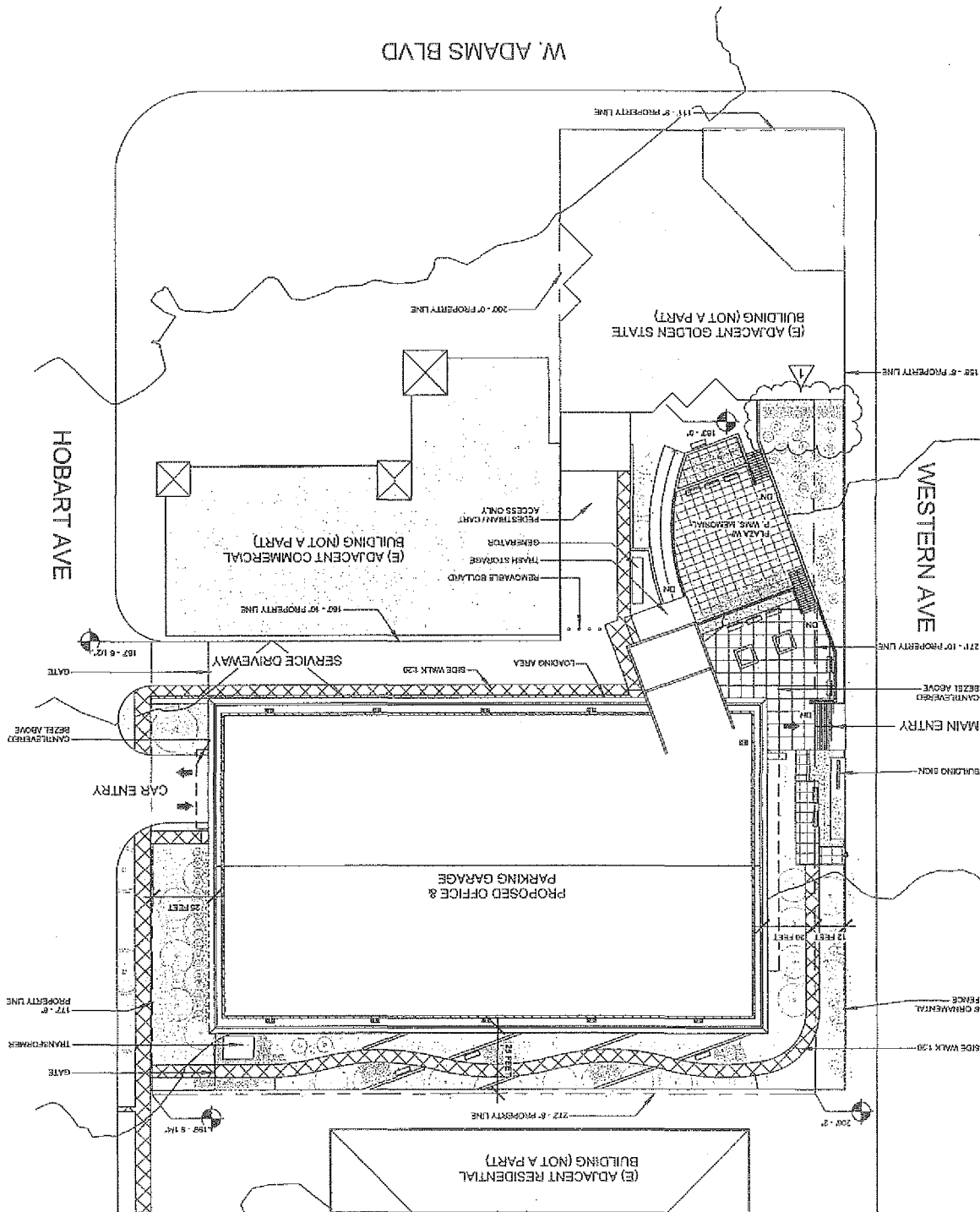


NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	CAD BACKGROUND NOW FROM 3D PROGRAM INSTEAD OF 2D FILE; MINOR CORRECTIONS

03/26/2012
3 OF 17



1" = 40' - 0"
SITE PLAN

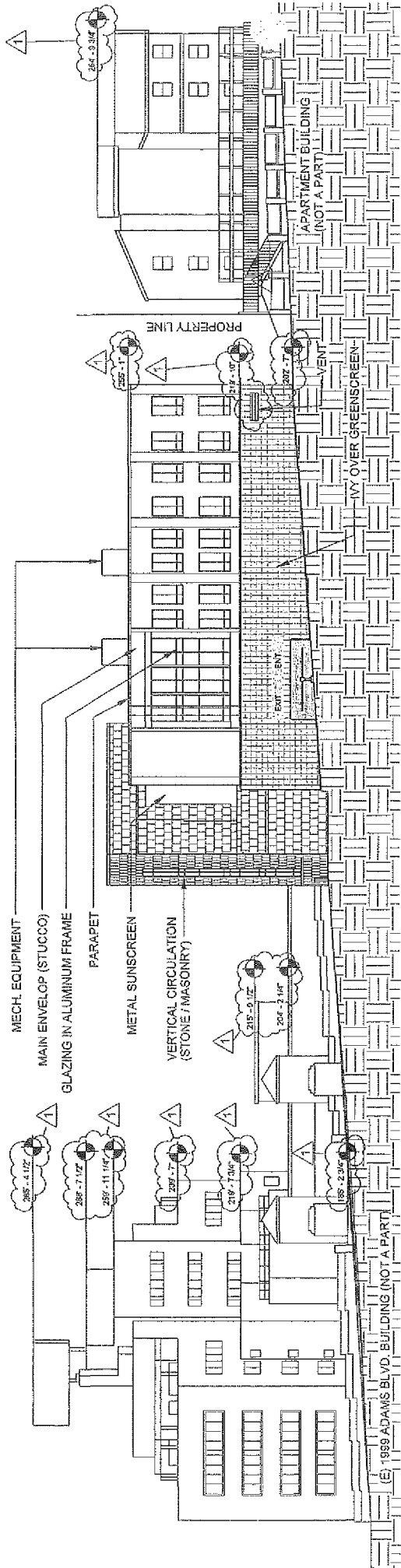


NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	CAD RACKING AND DIMENSIONS

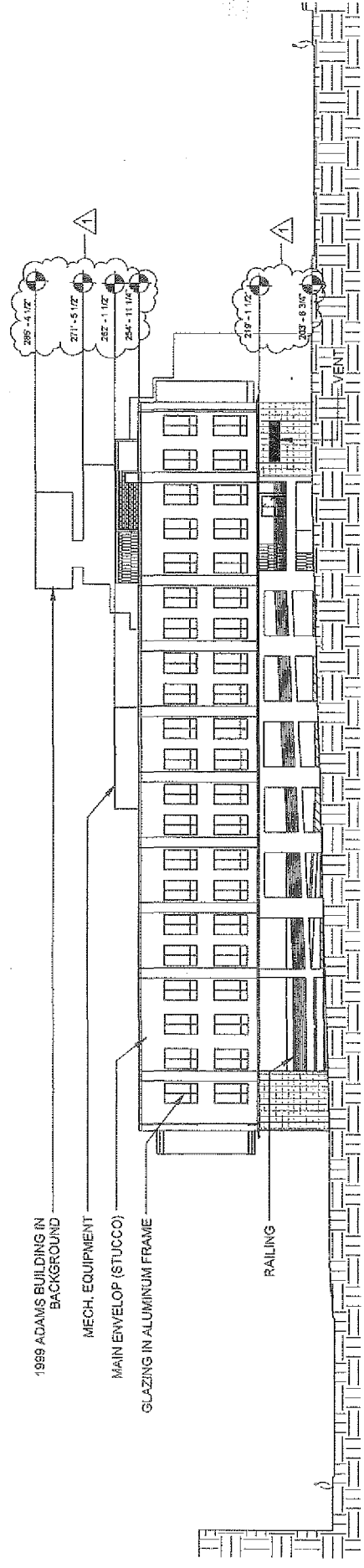
AE3 Partners | Steinberg Architects

EXHIBIT B-1
SITE PLAN
CPC-2010-0760-GPA-VZC-HD-SPR

03/28/2012
11 OF 17



EAST ELEVATION
1" = 30' - 0"



NORTH ELEVATION
1" = 30' - 0"

NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	FOR APPROVAL AND CONTRACT PREPARATION

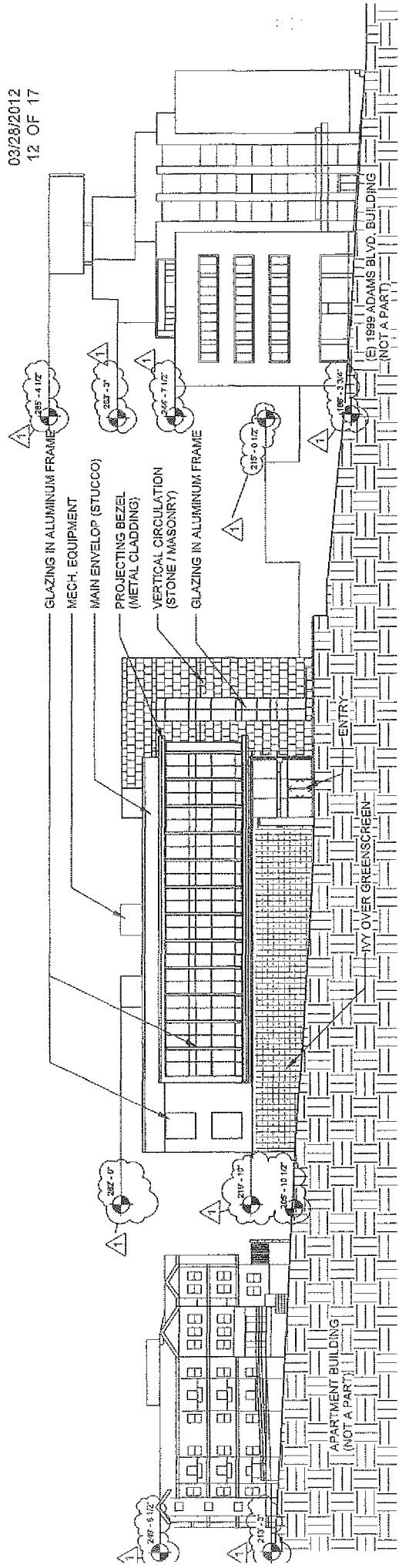
OFFICE BUILDING
2500 SOUTH WESTERN AVENUE

AE³ Partners + Steinberg Architects

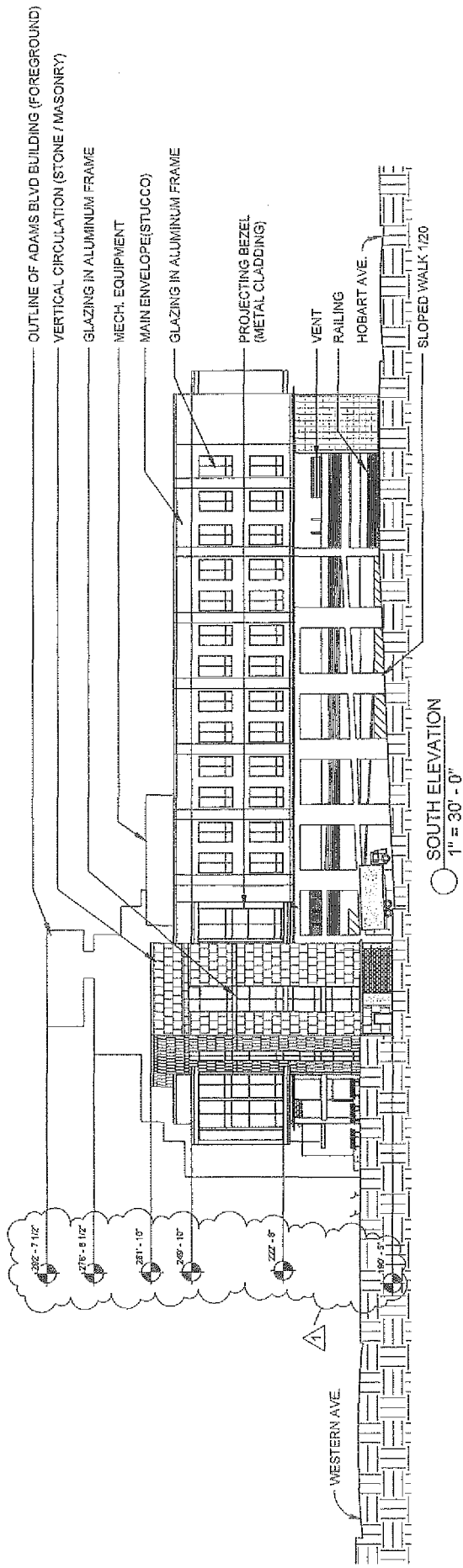
EXHIBIT B-2
ELEVATIONS

CPC-2010-0760-GPA-VZC-HD-
SPR

03/28/2012
12 OF 17



○ WEST ELEVATION
1" = 30' - 0"



○ SOUTH ELEVATION
1" = 30' - 0"

OFFICE BUILDING

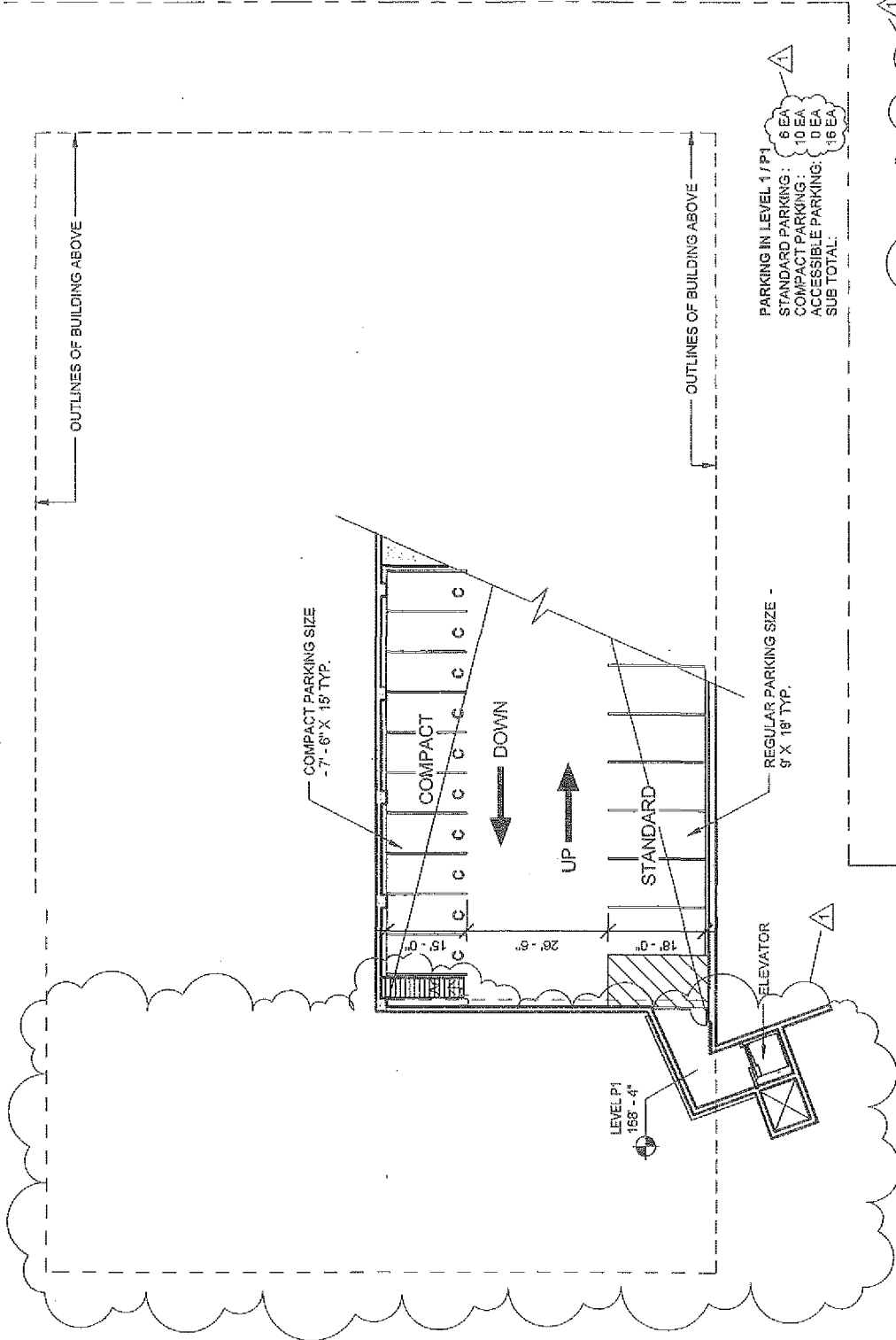
2500 SOUTH WESTERN AVENUE

AE³ Partners | Steinberg Architects

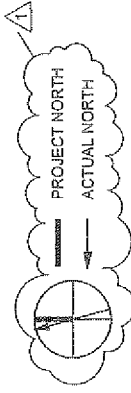
NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	CAD REVISIONS IN NEW EGM 3D DDC/DB/AM

EXHIBIT B-2 ELEVATIONS

CPC-2010-0760-GPA-VZC-HD-
SPR



PARKING IN LEVEL 1/P1
STANDARD PARKING: 6 EA
COMPACT PARKING: 10 EA
ACCESSIBLE PARKING: 0 EA
SUB TOTAL: 16 EA



LEVEL 1/P1 - PARKING
1" = 20'-0"

OFFICE BUILDING

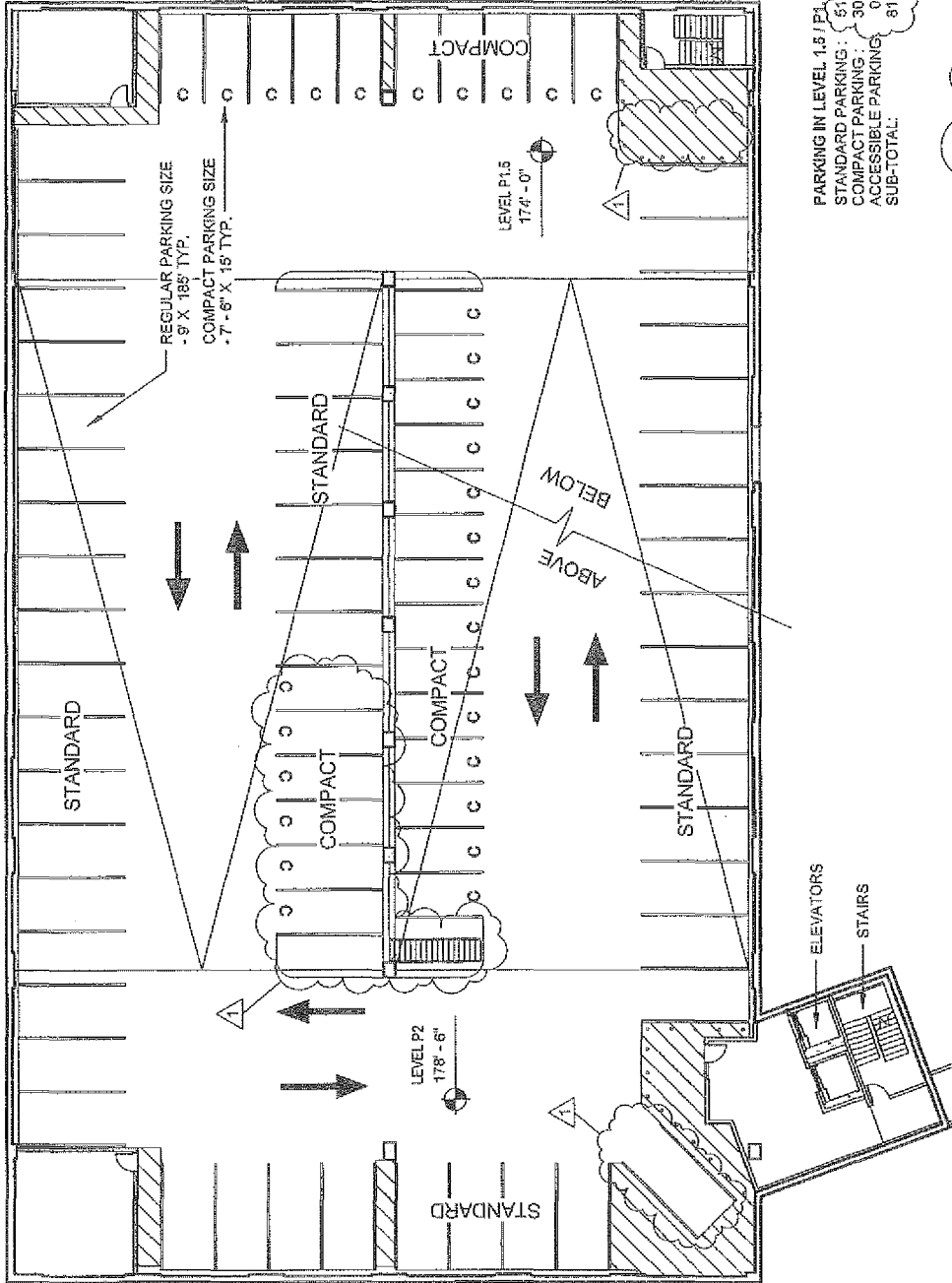
2500 SOUTH WESTERN AVENUE

AE3 Partners + Steinberg Architects

NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	

EXHIBIT B-3 PARKING PLAN

CPC-2010-0760-GPA-VZC-HD-
SPR



LEVEL 2/P2 - PARKING
1" = 20'-0"

PARKING IN LEVEL 1.5 / P1.5 THRU 2 / P2
STANDARD PARKING : 61 EA
COMPACT PARKING : 30 EA
ACCESSIBLE PARKING : 0 EA
SUB-TOTAL: 91 EA

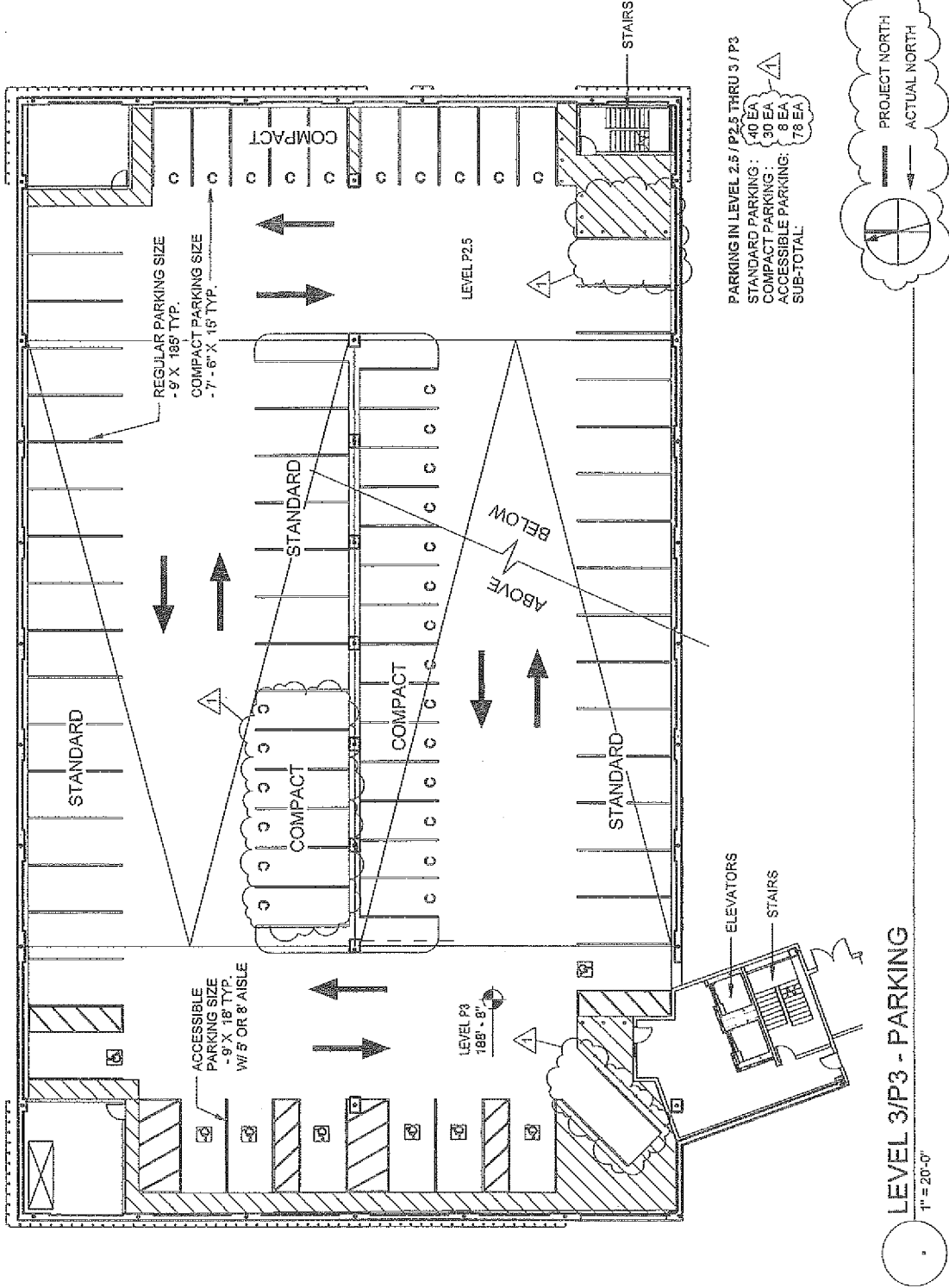


OFFICE BUILDING
2500 SOUTH WESTERN AVENUE

NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	

AE³ Partners + Steinberg Architects

EXHIBIT B-3
PARKING PLAN
CPC-2010-0760-GPA-VZC-HD-
SPR



PARKING IN LEVEL 2.5 / P2.5 THRU 3 / P3
 STANDARD PARKING: 40 EA
 COMPACT PARKING: 30 EA
 ACCESSIBLE PARKING: 8 EA
 SUB-TOTAL: 78 EA



LEVEL 3/P3 - PARKING
 1" = 20'-0"

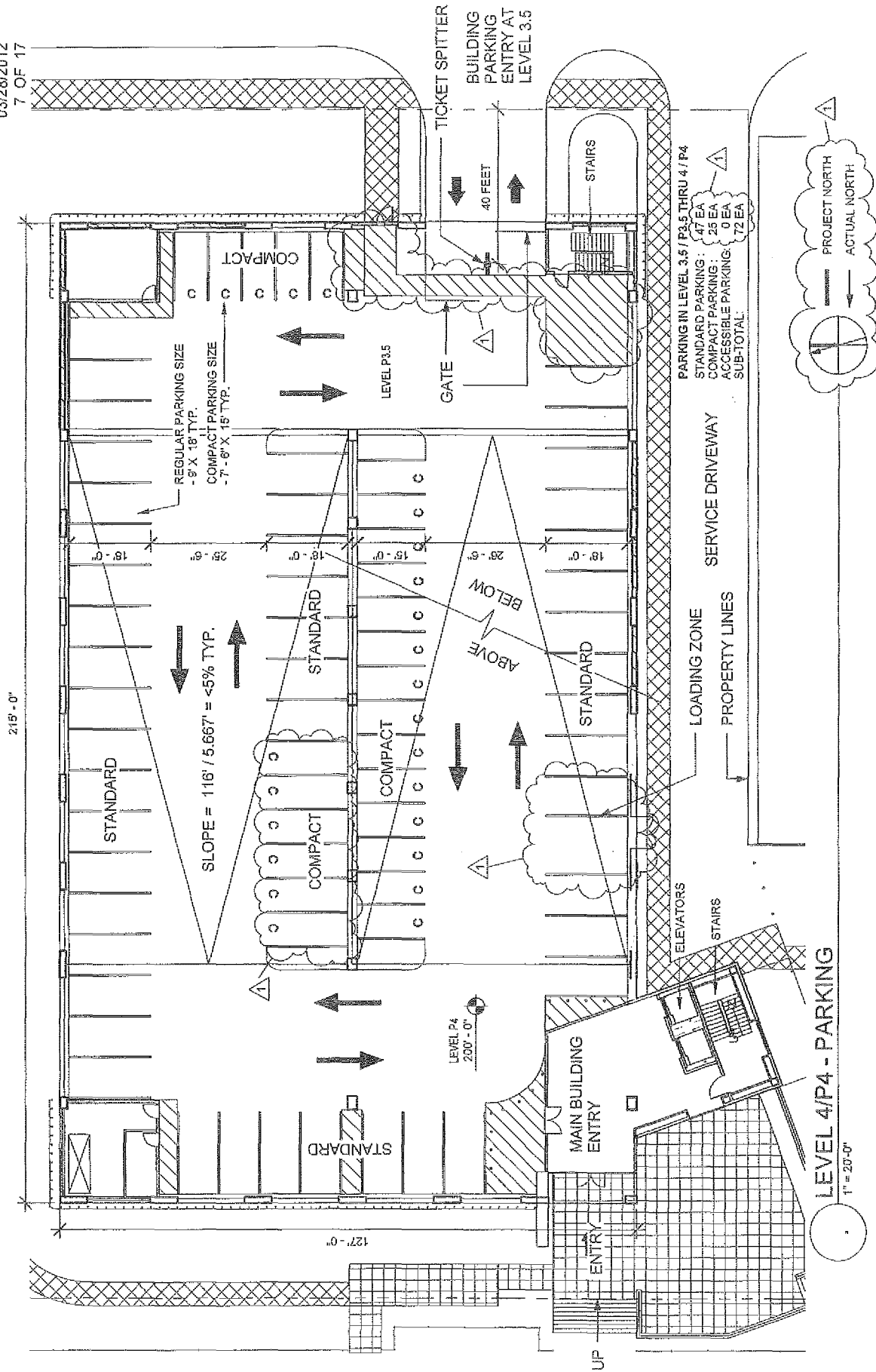
NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	CAD PAKING/REVISED/REVISIONS/REVISED/REVISED

OFFICE BUILDING

2500 SOUTH WESTERN AVENUE

AE³ Partners + Steinberg Architects

EXHIBIT B-3
 PARKING PLAN
 CPC-2010-0760-GPA-VZC-HD-
 SPR



PARKING IN LEVEL 3.5/ P3.5 THRU 4/ P4
 STANDARD PARKING: 47 EA
 COMPACT PARKING: 25 EA
 ACCESSIBLE PARKING: 0 EA
 SUB-TOTAL: 72 EA

OFFICE BUILDING

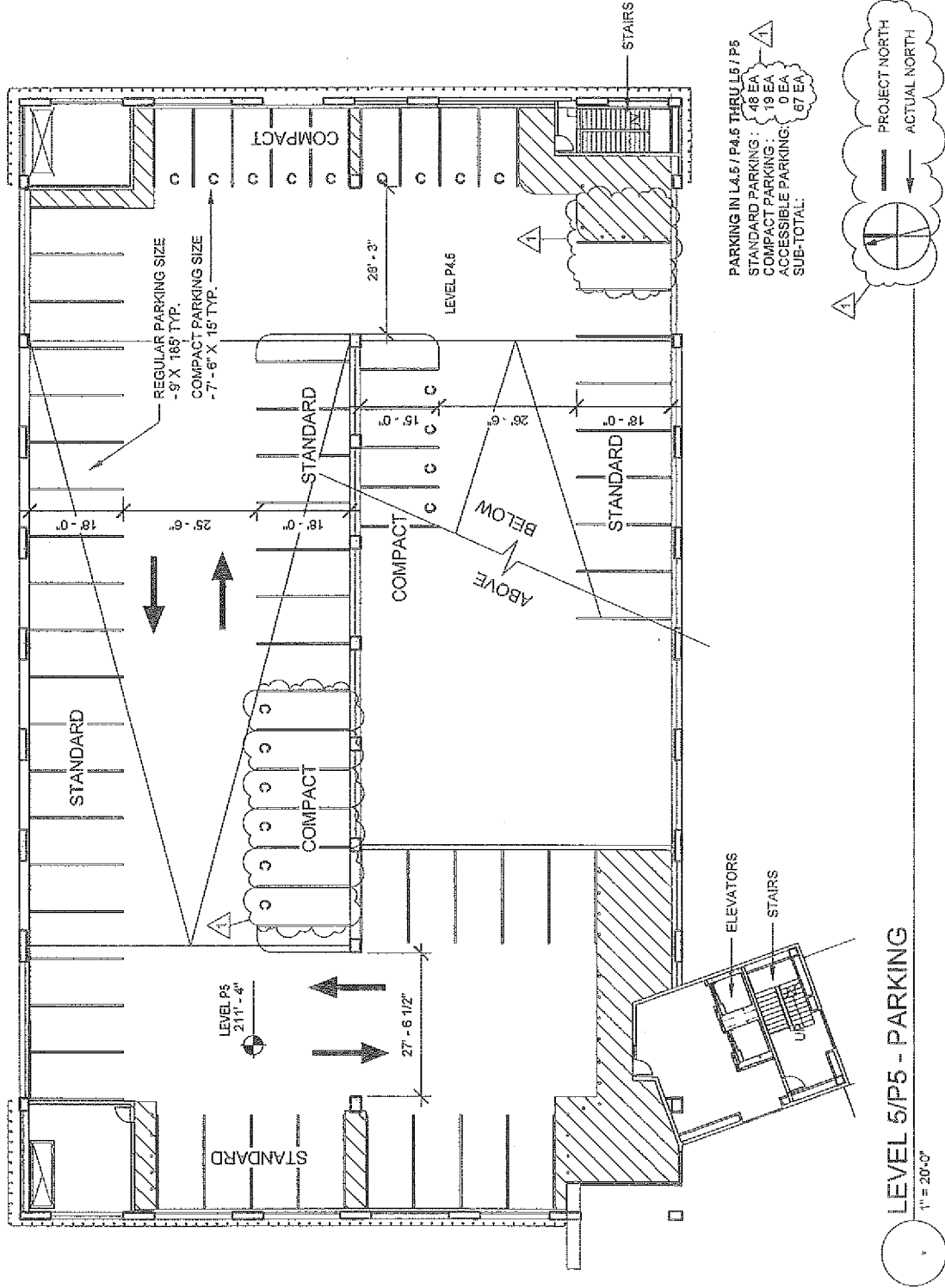
2500 SOUTH WESTERN AVENUE

AE³ Partners + Steinberg Architects

NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	CAD REVISIONS FOR APPROVAL 20.00000000

EXHIBIT B-3
 PARKING PLAN

CPC-2010-0760-GPA-VZC-HD-
 SPR



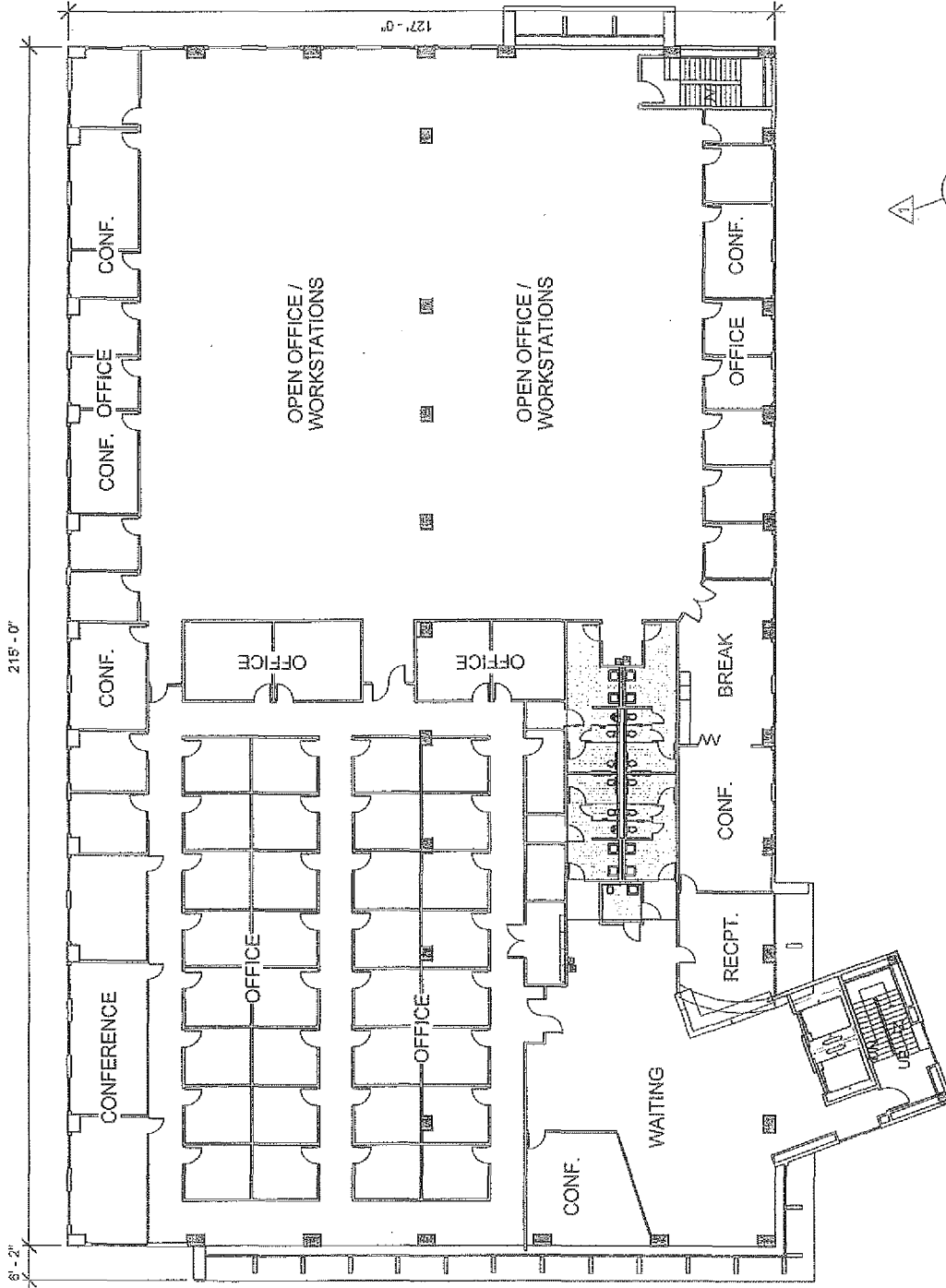
OFFICE BUILDING
2500 SOUTH WESTERN AVENUE

NO.	DATE	REVISION DESCRIPTION
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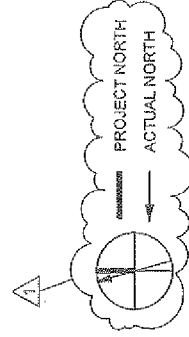
AE³ Partners + Steinberg Architects

EXHIBIT B-3
PARKING PLAN

**CPC-2010-0760-GPA-VZC-HD-
SPR**



LEVEL 6 - OFFICE
1" = 20'-0"



OFFICE BUILDING
2500 SOUTH WESTERN AVENUE

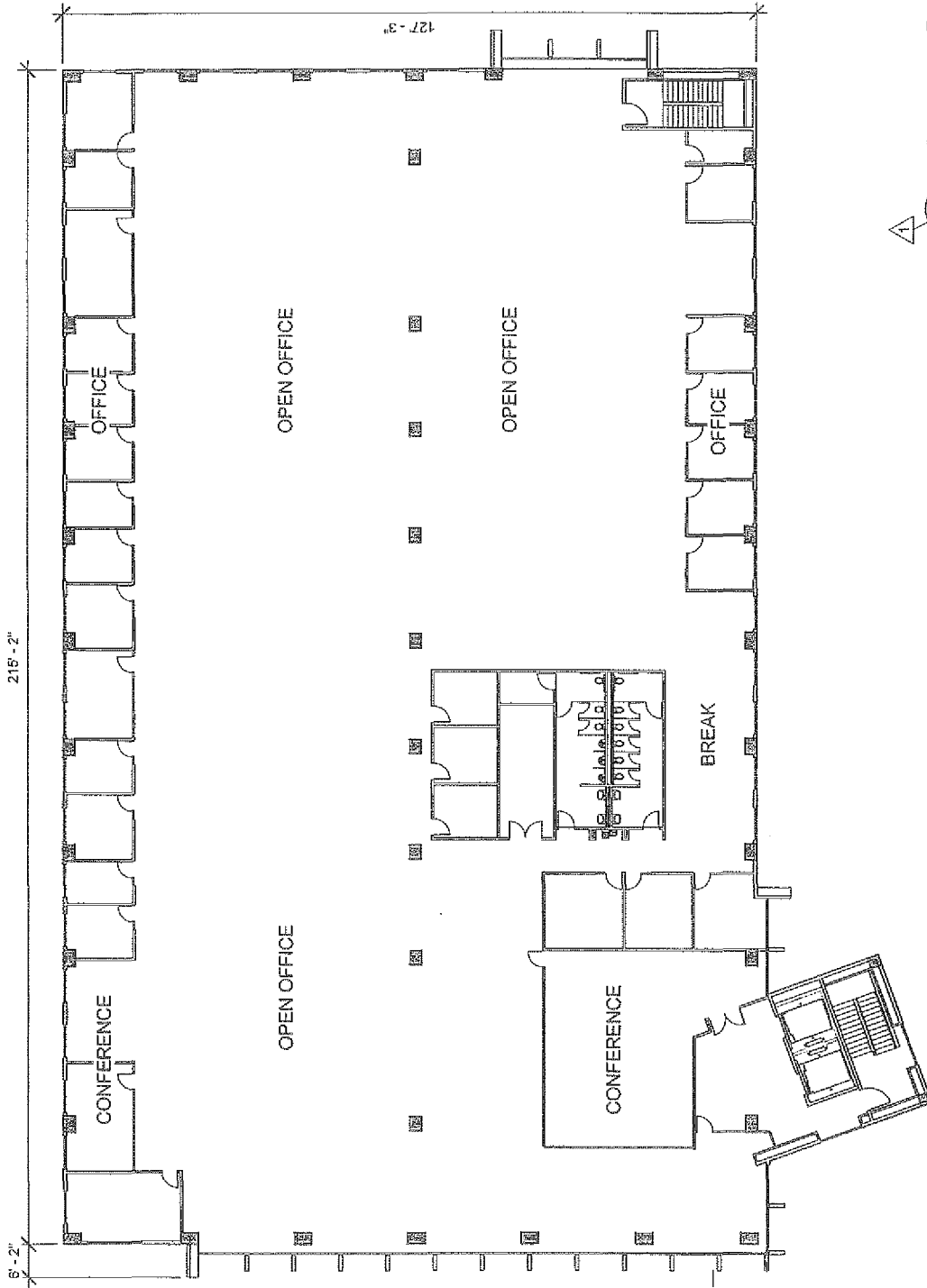
NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	

EXHIBIT B-4
FLOOR PLAN

AE³ Partners | Steinberg Architects

CPC-2010-0760-GPA-VZC-HD-
SPR

03/28/2012
10 OF 17



LEVEL 7 - OFFICE
1" = 20'-0"

NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	REVISION FROM 3D PROGRAM

OFFICE BUILDING
2500 SOUTH WESTERN AVENUE

AE³ Partners | Steinberg Architects

EXHIBIT B-4
FLOOR PLAN

CPC-2010-0760-GPA-VZC-HD-
SPR

03/28/2012
 15 OF 17
 CLASS CITY
 COMMON NAME

PLANT LIST

SYMBOL BOTANICAL NAME
 QUANTITY

TREES	SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QUANTITY	CITY
▲	AM	ARECATHA NANA	STRAWBERRY TREE	24" BOX	1	
●	GB	CHINKO BILOBA	WAGBERRY TREE	24" BOX	2	
○	JM	JACARANDA	JACARANDA	24" BOX	3	
○	MG	MANGROVIA	SANGAL SOMMER MANGROVIA	24" BOX	3	
○	PC	PIRUS CALLERYANA	CHAMTULERA PEAR	24" BOX	14	
○	PG	PODOPHYLLUM	PERNIPINE	24" BOX	16	
○	UP	ULMUS PARVIFOLIA	CHINESE EVERGREEN ELM	24" BOX	3	

EXISTING TREE TO REMAIN
 EXISTING PALM TREE TO REMAIN

SHRUBS

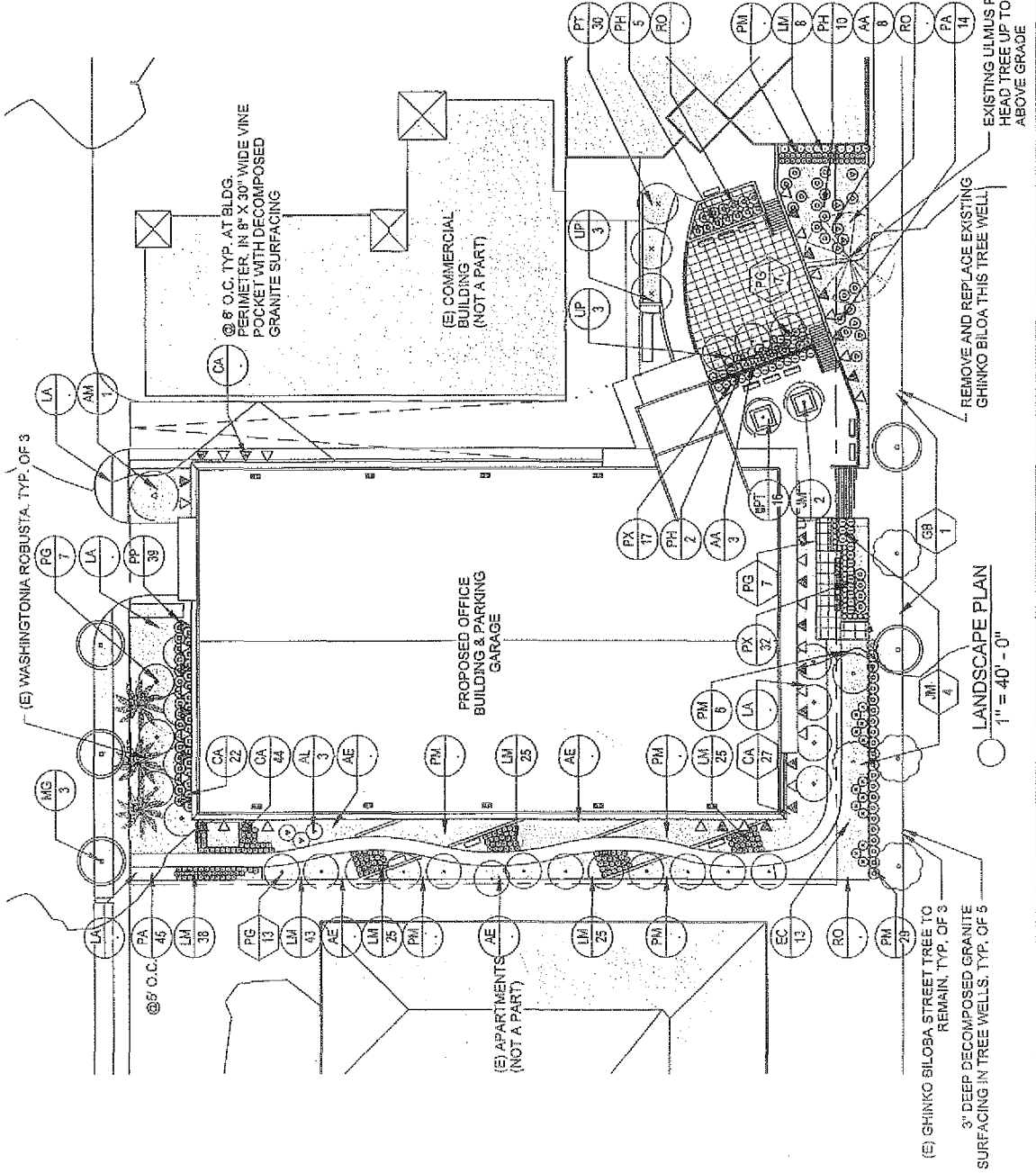
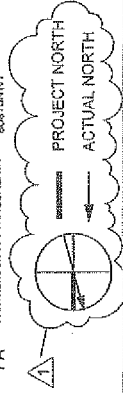
SYMBOL	SYMBOL	QUANTITY	BOTANICAL NAME
○	AL	6 GAL	FOXTAIL AGAVE
○	AA	19 GAL	TRIE ALICE
○	EC	5 GAL	PRIDE OF MADEIRA
○	LA	5 GAL	LANTANA
○	LM	5 GAL	LEUCOPHYLLUM
○	PX	5 GAL	PHLOX
○	PH	5 GAL	PHORADENDRON
○	PP	5 GAL	PHORADENDRON
○	PT	5 GAL	PHORADENDRON
○	PE	5 GAL	PHORADENDRON

GROUND COVER AND TURF

SYMBOL	BOTANICAL NAME	QUANTITY
▨	ARTEMISA	12 FLATS @ 1' O.C.
▨	ASPIDISTRA	1,100 SQ FT @ 1' O.C.
▨	LA	3,967 SF @ 1' O.C.
▨	PM	7,255 SF @ 1' O.C.
▨	RO	3,048 SF @ 1' O.C.

VINES

SYMBOL	BOTANICAL NAME	QUANTITY
▲	CA	5 GAL
▲	PA	5 GAL



OFFICE BUILDING
 2500 SOUTH WESTERN AVENUE

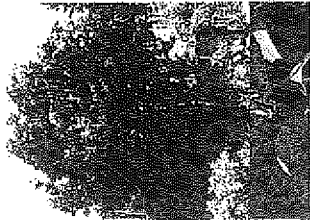
AE3 Partners + Steinberg Architects

NO.	DATE	REVISION DESCRIPTION
1	07/16/2012	

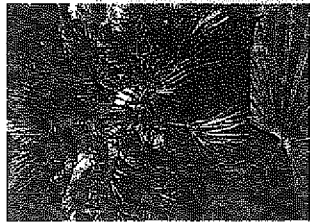
EXHIBIT B-5
 LANDSCAPE PLAN
 CPC-2010-0760-GPA-VZC-HD-
 SPR

TREES

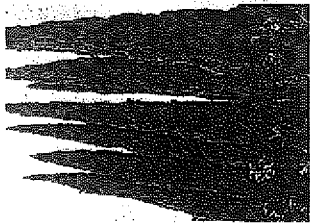
03/28/2012
16 OF 17



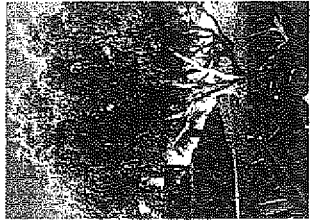
ARBUTUS 'MARINA'
Strawberry Tree
24" box
Evergreen Accent Tree



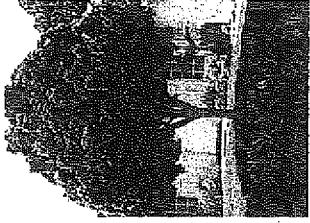
CHAMAEROPS HUMILIS
Mediterranean Fan Palm
24" box



CUPRESSUS SEMPERVIRENS
Italian Cypress
24" box
Evergreen Screen Tree



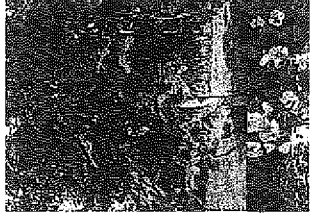
OLEA EUROPAEA 'SWAN HILL'
Fruitless Olive
24" box
Evergreen Accent Tree



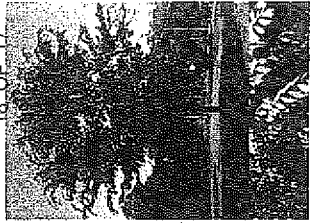
PODOCARPUS GRACILIOR
Fern Pine
24" box
Evergreen Screen Tree



LAGERSTROEMIA INDICA
Grape Myrtle
24" box
Deciduous Accent Tree

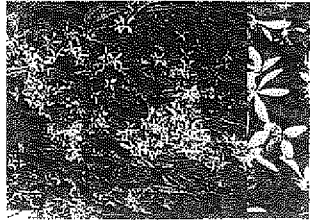


TIPUANA TIPU
Tipu Tree
24" box
Semi-evergreen Canopy Tree



ULMUS PARVIFOLIA
Chinese Evergreen Elm
24" box
Semi-evergreen Canopy Tree

VINES



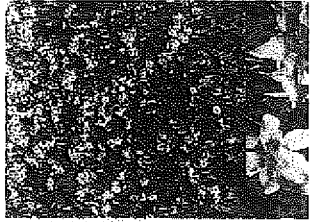
CLEMATIS SPP.
Clematis
15 gal



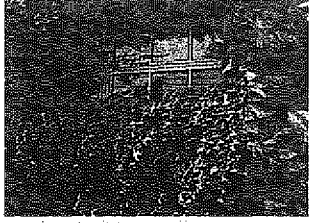
CYTOSTOMA CALLISTEGIOIDES
Violet Trumpet Vine
15 gal



DISTICTUS BUCCINATORIA
Blood Red Trumpet Vine
15 gal



GELSEMIUM SEMPERVIRENS
Carolina Jessamine
15 gal



PARTHENOCISSUS TRICUSPIDATA
Boston Ivy
15 gal

LANDSCAPE PALETTE

OFFICE BUILDING
2500 SOUTH WESTERN AVENUE

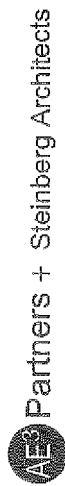
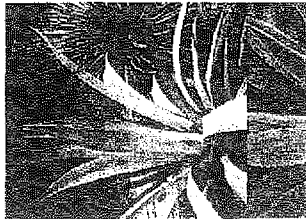


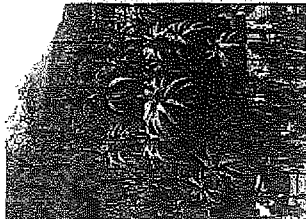
EXHIBIT B-5
LANDSCAPE PLAN
CPC-2010-0760-GPA-VZC-HD-SPR

SHRUBS & GROUND COVER

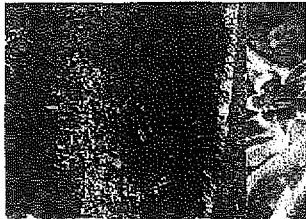
03/28/2012



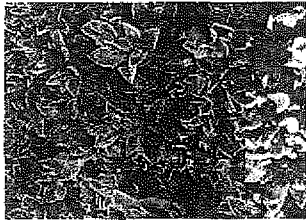
AGAVE AMERICANA
Century Plant
15 gal



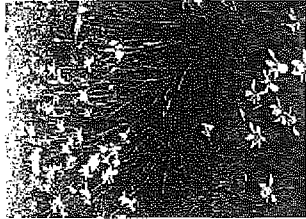
ALOE ARBORESCENS
Tree Aloe
5 gal



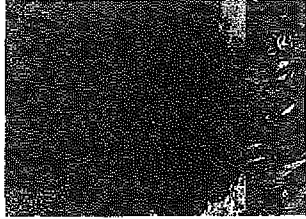
ANIGOZANTHOS FLAVIDUS
Kangaroo Paw
1 gal



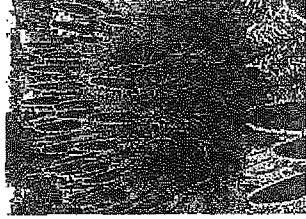
APTENIA 'RED APPLE'
Red Apple Ice Plant
1 gal



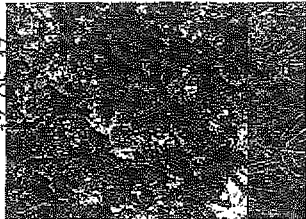
DIETES IRIDOIDES
Fornight Lily
5 gal



DODONAEA V. 'ATROPURPUREA'
Purple Hopseed Bush
5 gal



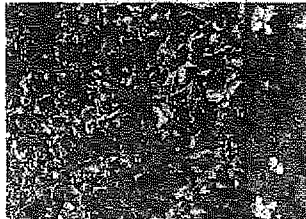
ECHIUM CANDICANS
Pride of Madeira
5 gal



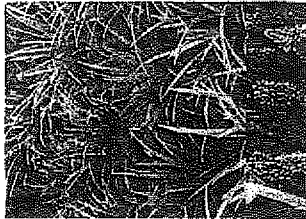
GREVILLEA LANIGERA
Prostrate Woolly Grevillea
5 gal



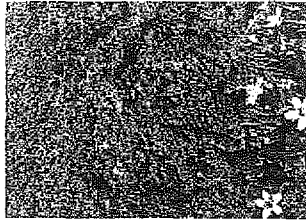
KALANCHOE THYRSIFLORA
Paddle Plant
5 gal



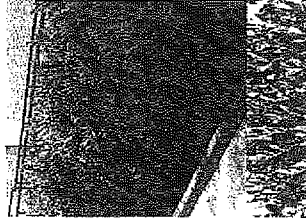
LANTANA CVS.
Lantana
5 gal



LIRIOPE GIGANTEA
Big Blue Lily, Turf
1 gal



MYOPORUM PARVIFOLIUM
Prostrate Myoporum
1 gal



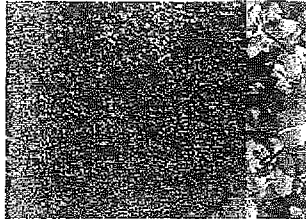
OLEA EUROPAEA 'LITTLE OLLIE'
Little Ollie Olive
5 gal



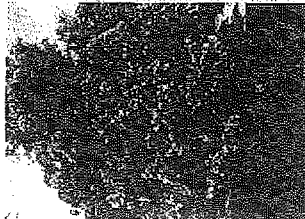
PHILODENDRON SELLOUM
Cut-leaf Philodendron
3 gal



PHORMIUM HYBRID
Variegated New Zealand Flax
5 gal



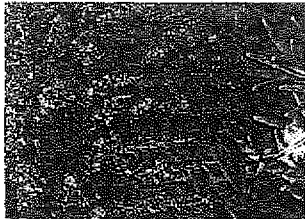
PITOSPORUM TENUIFOLIUM CVS.
Kohuhu
5 gal



PODOCARPUS GRACILIOR
Fern Pine
5 gal



POLYSTICHUM MUNITUM
Sword Fern
5 gal



ROSMARINUS OFFICINALIS
Rosemary
5 gal

LANDSCAPE PALETTE

DEPARTMENT OF
CITY PLANNING
200 N. SPRING STREET, ROOM 525
LOS ANGELES, CA 90012-4801
AND
6262 VAN NUYS BLVD., SUITE 351
VAN NUYS, CA 91401

CITY PLANNING COMMISSION

WILLIAM ROSCHEN
PRESIDENT
REGINA M. FREER
VICE-PRESIDENT
SEAN O. BURTON
DIEGO CARDOSO
MATT EPSTEIN
BARBARA ROMERO
MICHAEL K. WOOD
VACANT
VACANT
JAMES WILLIAMS
COMMISSION EXECUTIVE ASSISTANT
(213) 978-1300

CITY OF LOS ANGELES
CALIFORNIA



ANTONIO R. VILLARAIGOSA
MAYOR

EXECUTIVE OFFICES

MICHAEL J. LOGRANDE
DIRECTOR
(213) 978-1271
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EVA YUAN-MCDANIEL
DEPUTY DIRECTOR
(213) 978-1273
VACANT
DEPUTY DIRECTOR
(213) 978-1274
FAX: (213) 978-1275
INFORMATION
www.planning.lacity.org

April 18, 2012:

Community Impact Development II, LLC (O)(A)
Attn: James D. Howard, Jr.
1968 W. Adams Blvd., Suite 209
Los Angeles, CA 90018

Park & Velayos LLP (R)
Attn: Marcos D. Velayos
801 South Figueroa Street, Suite 350
Los Angeles, CA 90017

EXHIBIT C
ENV-2010-0328-MND
CPC-2010-0760-GPA-VZC-HD-
SPR

Re: Addendum to ENV-2010-0328-MND; 2500 S. Western Avenue; CPC-2010-760-GPA-VCZ-HD-SPR

On April 20, 2011, the Department of City Planning issued Mitigated Negative Declaration ENV-2010-0328-MND to address potential environmental impacts of proposed CPC-2010-760-GPA-VCZ-HD-SPR, with the following project description:

Construction of a new 3-story, 75-foot tall office building (70,000 square feet of floor area), that includes a roof-top mechanical screening (approximately 10 feet in height), 2 solar solar screens (approximately 18 feet in height), and one proposed future new sign (approximately 32 feet in height) located 20 feet above the proposed building's parapet wall, a pedestrian bridge on the 2nd floor that connects to the Golden State Mutual Office Building, and 3 levels of subterranean parking with 320 parking stalls, all on an approximately 70,763 square-foot site in the C2-1 Zone, in conjunction with a: (1) haul route, (2) general plan amendment, (3) vested zone change, (4) height district change, and (5) site plan review, as follows:

- (1) a Haul Route approval for proposed excavation of approximately 45,200 cubic yards of earth material to be exported for the proposed construction of an office building;
- (2) a General Plan Amendment to change the general plan land use designation from Low Medium II Residential to Community Commercial and to amend Footnote No. 1 to the South Los Angeles Community Plan land use map to allow the property to be designated in Height District 2;
- (3) a Vesting Zone Change from RD1.5 to [Q]C2 and from C2 to [Q]C2 to allow the construction of a 70,000-square-foot office building with 320 parking spaces;
- (4) a Height District Change from Height District 1 to Height District 2D (with a 2:1 FAR Limitation) for the entire site; and
- (5) Lastly, Site Plan Review for the proposed development project that includes a roof-top mechanical screening (approximately 10 feet in height), 2 solar screens (approximately 18 feet in height) and one proposed future new sign (approximately 32 feet in height) located 20 feet above the proposed building's parapet wall.

April 10, 2012, the applicant submitted a revised project description dated March 28, 2012, as follows:

The Proposed Project is a two-story office building with approximately 56,960 square feet of floor area located over five levels of parking (two subterranean levels, one partially subterranean level, and two above ground levels) with 331 parking stalls, all on an approximately 70,763 square-foot site in the C2-1 Zone, in conjunction with a: (1) Haul Route, (2) General Plan Amendment, (3) Vesting Zone Change, (4) Height District Change, and (5) Site Plan Review, as follows:

- (1) a haul Route approval for proposed excavation of approximately 27,100 cubic yards of earth material to be exported for the proposed construction of an office building;
- (2) a General Plan Amendment to change the general plan land use designation from Low Medium II Residential to Community Commercial and to amend Footnote No.1 to the South Los Angeles Community Plan land use map to allow the property to be designated in Height District 2;
- (3) a Vesting Zone Change from RD1.5 to [QJC2 and from C2 to [QJC2 to allow the construction of a 56,960-square-foot office building with 331 parking spaces;
- (4) a Height District Change from Height District 1 to Height District 2D (with a 2:1 FAR Limitation) for the entire site; and
- (5) Lastly, Site Plan Review for the proposed development project.

The Proposed Project also includes a pedestrian plaza and ground level connection to the existing Golden State Building located at the northeast corner of Western Avenue and Adams Boulevard. The Proposed Project would remove an existing on-site surface parking lot to accommodate the proposed office building.

In summary, the applicant representative revised their project to decrease the excavation of dirt by 18,090 cubic yards, to reduce the building's floor area by 13,040 square feet, to eliminate one of the three stories, and to provide 11 more parking stalls.

The Department of City Planning considers that these technical edits, clarifies and amplifies the mitigated negative declaration and does not require recirculation pursuant to section 15073.5 (c).4. The Department of City Planning considers that the revision does not create any new substantial impacts beyond what has been previously analyzed in the original environmental clearance, as defined by the California Environmental Quality Act (CEQA Guidelines). Therefore, pursuant to section 15073.5 (b) and (c) of the CEQA Guidelines, recirculation of the MND is not required.

Sincerely,



LYNDA J. SMITH
City Planner

Prepared by:



HAYDEE URITA-LOPEZ
Planning Assistant



Division of Land / Environmental Review

City Hall • 200 N. Spring Street, Room 750 • Los Angeles, CA 90012



Initial Study

SOUTH CENTRAL LOS ANGELES COMMUNITY PLAN AREA

2500 S. Western Avenue Office Project

ENV-2010-328-MND

Council District No. 8

**THIS DOCUMENT COMPRISES OF THE INITIAL STUDY
REQUIRED UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT**

Project Address: 2500 South Western Avenue

Project Description: The Proposed Project is a three-story office building with approximately 70,000 square feet of interior leasable area atop a three-level subterranean parking structure. The parking structure would provide a minimum of 320 on-site parking spaces. The Proposed Project also includes a tiered pedestrian plaza and a covered pedestrian bridge that connects the Proposed Project with the existing on-site former Golden State Mutual Life Insurance Building located at the northeast corner of Western Avenue and Adams Boulevard. The Proposed Project would remove an existing on-site surface parking lot to accommodate the proposed office building.

**APPLICANT:
COMMUNITY IMPACT DEVELOPMENT II, LLC**

**PREPARED BY:
Environmental Review Section
Los Angeles City Planning Department**

October 2011

Section I. Introduction

The subject of this Initial Study is the proposed 2500 S. Western Avenue Office Project (Proposed Project), located on the northeast corner of the intersection of Western Avenue and Adams Boulevard in the South Central area of the City of Los Angeles. The Proposed Project consists of a three-story office building with approximately 70,000 square feet of leasable area atop a three-level subterranean parking structure. The Proposed Project would also include the construction of a tiered pedestrian plaza and a pedestrian walkway between the existing on-site five-story Golden State Mutual Life Insurance Building and the proposed office building. The three-level subterranean parking structure would provide approximately 320 parking spaces. Primary pedestrian access to the proposed office building would be along Western Avenue, while access to the Proposed Project's parking structure would be available via a driveway along Hobart Boulevard, in the same general location as the existing access driveway.

This Initial Study serves as an informational document and provides an analysis of the potential environmental effects of the Proposed Project, including construction activities and the operation of the proposed uses. This Initial Study shall be used in connection with all other permits and approvals necessary for the construction and operation of the Project. This Initial Study shall be used by the City of Los Angeles Department of Planning, Community Redevelopment Agency, Department of Building and Safety, Department of Transportation, Department of Public Works (including the Bureaus of Engineering and Sanitation), and all other responsible public agencies which must approve activities undertaken with respect to the Proposed Project.

A. Project Information

Project Title: 2500 S. Western Avenue Office Project

Project Location: The northeast corner of the intersection of Western Avenue and Adams Boulevard in the South Central area of the City of Los Angeles. The Project Site is bordered by a one-story commercial center to the southeast, a five-story apartment building on the east side of Hobart Boulevard, the three-story FAME Arms apartment building to the north, the St. John of God Retirement and Care Center located on the west side of

Western Avenue, and the FAME Assistance Corporation headquarters on the south side of Adams Boulevard.

Project Applicant: Community Impact Development II, LLC
968 W. Adams Boulevard, Suite 209
Los Angeles, California 90018

Lead Agency: City of Los Angeles
Department of City Planning
200 North Spring Street
Los Angeles, California 90012

B. Organization of the Initial Study

This Initial Study is organized into five sections as follows:

- **Introduction:** This section provides introductory information such as the Project title, the Project Applicant, and the lead agency for the Proposed Project.
- **Project Description:** This section provides a detailed description of the environmental setting and the Proposed Project, including project characteristics and environmental review requirements.
- **Initial Study Checklist:** This section contains the completed Initial Study Checklist.
- **Environmental Impact Analysis:** This section provides an assessment and discussion of the Project's potential environmental impacts for each environmental issue identified in the Initial Study Checklist. When the evaluation identifies potentially significant effects, as identified in the Initial Study Checklist, mitigation measures are provided to reduce such impacts to a less than significant level.
- **Preparers of the Initial Study and Persons Consulted:** This section provides a list of individuals from the City and other governmental agencies, as well as consultant team members, who participated in the preparation of the Initial Study.

Section II. Project Description

A. Environmental Setting

1. Project Location

The 2500 S. Western Avenue Office Project (the Proposed Project or Project) is situated on approximately 1.62 acres of land located generally at the northeast corner of the intersection of Western Avenue and Adams Boulevard (the Project Site) in the South Central Los Angeles Community Plan (the Community Plan) Area of the City of Los Angeles (the City). The Project Site is approximately three miles west of downtown Los Angeles and approximately 11 miles east of the Pacific Ocean. The Project Site is L-shaped and is bounded by: a retirement center to the north, Hobart Boulevard to the east, a commercial center to the southeast, Adams Boulevard to the south, and Western Avenue to the west.

The Project Site is located in a highly urbanized area and may be accessed via several regional freeways and local surface streets. The Project Site is located approximately 0.25 mile south of the Santa Monica (I-10) Freeway and approximately 2.1 miles west of the Harbor (I-110) Freeway. The Project Site is locally accessible via Western Avenue to the west, Hobart Boulevard to the east, and Adams Boulevard to the south, with driveway access on Hobart Boulevard. Western Avenue and Adams Boulevard are designated Major Highway Class II roadways by the City. Western Avenue provides direct access to the I-10 Freeway. Adams Boulevard provides access to the I-110 Freeway to the east of the Project Site. Additional nearby major arterials include Washington Boulevard to the north, Normandie Avenue to the east, Jefferson Boulevard to the south, and Arlington Avenue to the west.

Transit service to the Project Site is provided by the Los Angeles County Metropolitan Transportation Authority (Metro) and the City of Los Angeles Department of Transportation (LADOT) bus lines. Metro Local Line 37 runs east-west along Adams Boulevard. Metro Local Line 207, Metro Express Line 550, and Metro Rapid Line 757 all run north-south along Western Avenue. Metro Local Line 37 provides service from the Washington/Fairfax Transit Center to downtown Los Angeles. Metro Local Line 207 provides service from the Hollywood area to Imperial Highway. Metro Express Line 550

provides service from West Hollywood to the Los Angeles harbor area. Metro Rapid Line 757 provides service from West Hollywood to the Metro Crenshaw Station. LADOT Downtown Area Shuttle (DASH) Midtown is a local line that travels from Crenshaw Boulevard and Martin Luther King Jr. Boulevard to Redondo Boulevard and San Vicente Boulevard. Additional, nearby bus lines include the Metro Local 206, which travels along Normandie Avenue, and the Metro Local 209, which travels along Arlington Avenue.

Figure II-1 on page II-3 depicts the Project Site and surrounding area; and Figure II-2 on page II-4 provides an aerial photograph of the Project Site.

2. Existing Land Use and Zoning Designations

As mentioned above, the Project Site is located within the planning boundaries of the South Central Los Angeles Community Plan (the Community Plan Area), as well as within the planning boundaries of the Normandie 5 Redevelopment Plan (the Redevelopment Plan). The Department of City Planning is currently in the process of updating the Community Plan. The Community Plan currently designates the western portion of the Project Site as Community Commercial and designates the easternmost 150 feet of the Project Site as Low Medium II Multiple Family. In general, land use designations in the Redevelopment Plan conform to and are consistent with the land use provisions of the Community Plan. However, in the case of the Project Site, the existing land use designations are not consistent. While the Community Plan designates the Project Site for both commercial and residential uses, the Redevelopment Plan designates the entire Project Site for commercial uses.

The Project Site is also located within the planning boundaries of the City's Conditional Use Approval for Sale of Alcoholic Beverages Specific Plan (the Specific Plan) and is also within the borders of a Los Angeles Enterprise Zone. The Specific Plan regulates the sale of alcohol in this portion of the City, while the Los Angeles Enterprise Zone provides incentives to stimulate economic development to targeted areas of the City. The Project Site is located just east of the West Adams Historic Preservation Overlay Zone (HPOZ). Further, the Project Site is located within the boundaries of the Los Angeles Fast Food Interim Control Ordinance, which seeks to reduce the over-consumption of fast food in several areas of the City. Finally, the Project Site is located within the West Adams Targeted Neighborhood Initiative Area, which is designated by the non-profit organization Los Angeles Neighborhood Initiative (LANI) to stimulate community-driven neighborhood revitalization.

From a zoning perspective, the Project Site is divided into two distinct areas. The western portion of the Project Site is zoned C2-1 (Commercial Zone), while the easternmost 150 feet of the Project Site is zoned RD1.5-1 (Restricted Density Multiple Dwelling). The C2 zone allows for most commercial and retail uses. The RD1.5 zone



Source: Thomas Brothers Maps, 2010; Matrix Environmental, 2010.

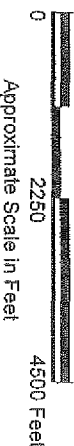


Figure II-1
Regional & Project Vicinity Map

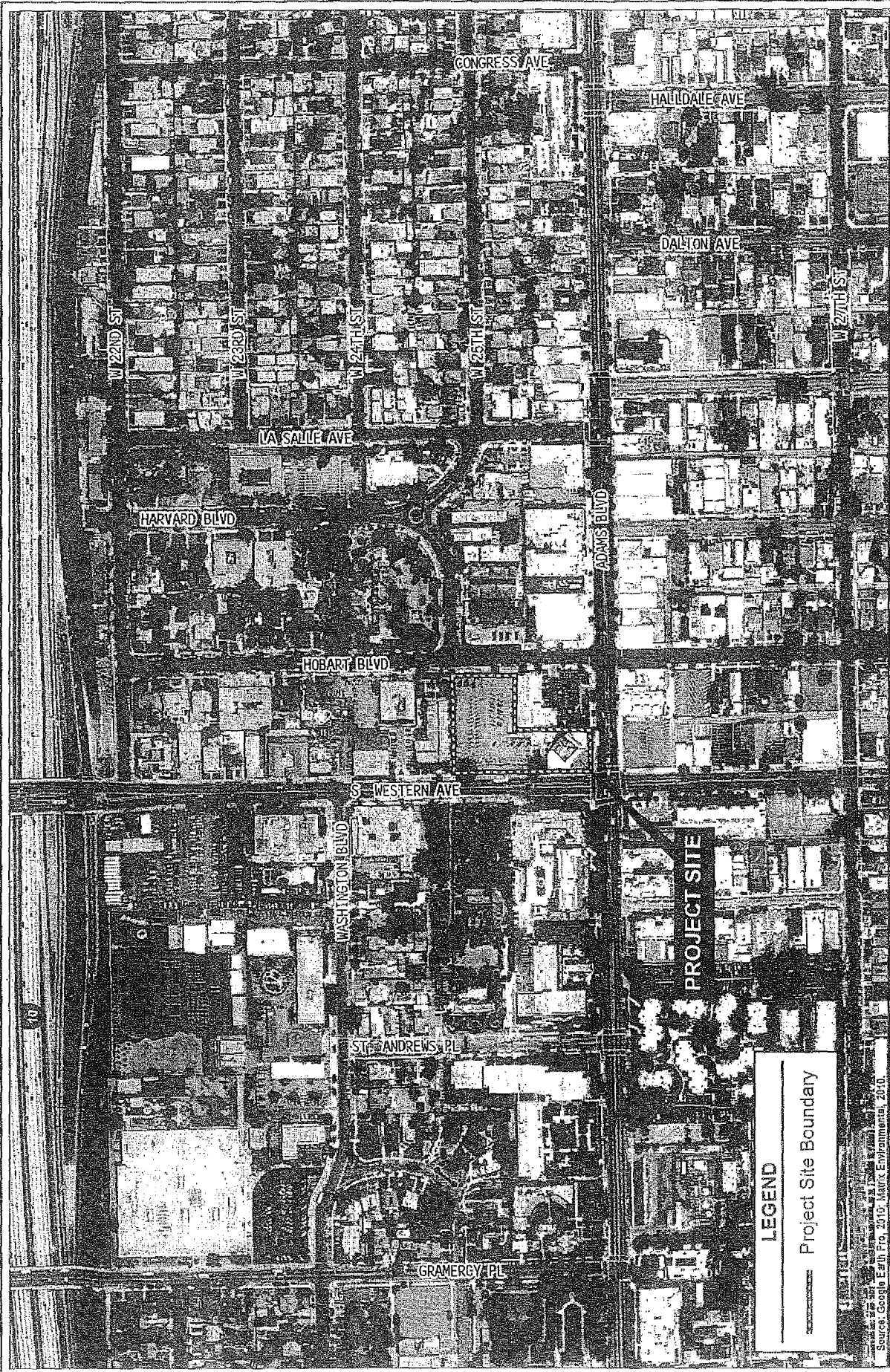
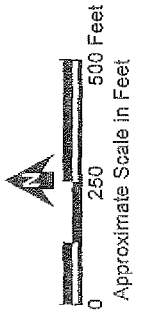


Figure II-2
Aerial Photograph



LEGEND
Project Site Boundary



Source: Google Earth Pro, 2019; Matrix Environmental, 2019.

allows for limited residential uses that comply with the height, front yard, rear yard, side yard, and lot area provisions of the RD1.5 zone. The "-1" component of each of these respective zones indicates that both portions of the Project Site are located in Height District 1, which, in the C2 zone permits a maximum floor area ratio (FAR) of 1.5:1, with no limitation on building height, , and in the RD1.5 zone, permits a maximum FAR of 3:1, with a 45 foot height limit.

3. Site History and Existing Conditions

The Project Site is approximately 1.62 acres in size and for over 50 years, has been developed with the former Golden State Mutual Life Insurance Building (Golden State Building) and a surface parking lot built to provide parking for employees of the Golden State Building. The Golden State Building is located on the southern portion of the Project Site, at the northeast corner of the intersection of Western Avenue and Adams Boulevard, while the surface parking lot is located on the northern portion of the Project Site. The five-story Golden State Building consists of approximately 39,800 square feet of floor area (58,820 gross sq ft.) and was dedicated in 1949. Other miscellaneous on-site structures include a guard shack along the Site's southern boundary, a swing gate at the existing driveway entrance along Hobart Boulevard, a service area at the rear of the Golden State Building, and pole-mounted flood lights along the perimeter of the surface parking lot. The service area at the rear of the Golden State Building is currently accessed by a driveway on Western Avenue.

On-site trees currently consist of a few isolated ornamental trees, including palm, cypress, Chinese elm, and magnolia. Four palm trees are located along the Project Site's eastern border while two tree clusters are located along the Project's western border. A lone cypress tree is located along the Project Site's southern border, adjacent to the north side of the adjacent, off-site commercial center. In addition to these trees, an approximately 20-foot-wide grass area is located along the Project Site's western boundary, adjacent to the Western Avenue sidewalk. This grass area also contains miscellaneous low-lying shrubs located against the surface parking lot's western fence. No other vegetation is located on the Project Site.

Existing signage on the Project Site includes the Golden State Mutual Life sign located above the main entrance of the Golden State Building and a currently unused billboard on the building's roof.

4. Surrounding Uses

The Community Plan Area consists of commercial uses located along major arterial roadways and single-family residential and low-rise, multi-family residential development along secondary roadways. Commercial uses in the Community Plan Area vary in age and type, ranging from multi-story brick structures constructed in the first two decades of the 20th Century to modern multi-story office buildings and commercial centers. Residential development within the Community Plan Area varies greatly in age and type, ranging from single-family homes built around the turn of the 20th Century to modern low-rise apartment buildings. Light industrial and public uses are scattered throughout the Community Plan Area, with industrial uses being clustered around the intersection of Slauson and Western Avenues and along Washington Boulevard. Public uses include public service facilities (e.g., police and fire stations), parks, schools, and religious institutions.

Within the Community Plan Area, the Project Site is located within the northern edge of the Redevelopment Plan Area. The Redevelopment Plan Area is approximately 210 acres in size and is located southwest of the City's Central Business District. The Redevelopment Plan Area includes land uses similar to those found in the overall Community Plan Area and as such, is generally characterized by a mixture of single-family residential and low-rise multi-family residential uses on secondary roadways, with commercial and retail uses located along major arterial roadways. The majority of the development within the Redevelopment Plan Area occurred prior to 1950, with much of the Area's housing stock dating back to the first two decades of the 20th Century. However, redevelopment efforts since the early 1960s have resulted in the construction of modern structures. The Redevelopment Plan Area contains a higher percentage of single-family homes than the Community Plan Area as a whole. Multi-family residential buildings are concentrated at the northern portion of the Redevelopment Plan Area, in the vicinity of the Project Site. The Redevelopment Plan Area contains also notable community facilities such as the Loren Miller Park and Community Center, the Adams Western Senior Housing Mixed-Use Project, and the Los Angeles Fire Department (LAFD) Engine House No. 18.

The properties surrounding the Project Site are generally consistent with the character of the Community Plan Area and the northern portion of the Redevelopment Plan Area (although only properties south of the Project Site are within the Redevelopment Plan Area). The Project Site is surrounded by a variety of commercial, institutional, and residential uses in buildings up to five stories in height. Photographs depicting the Project Site and its immediate surroundings are shown in Figures II-3 through II-6 on pages II-7 through II-10.

The property north of and adjacent to the Project Site consists of the three-story FAME Arms Apartments, an assisted-living retirement community for persons with



View 1: FAME Arms Apartments directly north of the Project Site, Western Avenue frontage.



View 2: Land uses north of the FAME Arms Apartments along Western Avenue.



View 3: Land uses north of the FAME Arms Apartments along Hobart Boulevard (decorative fence is the rear of the FAME Arms Apartments).

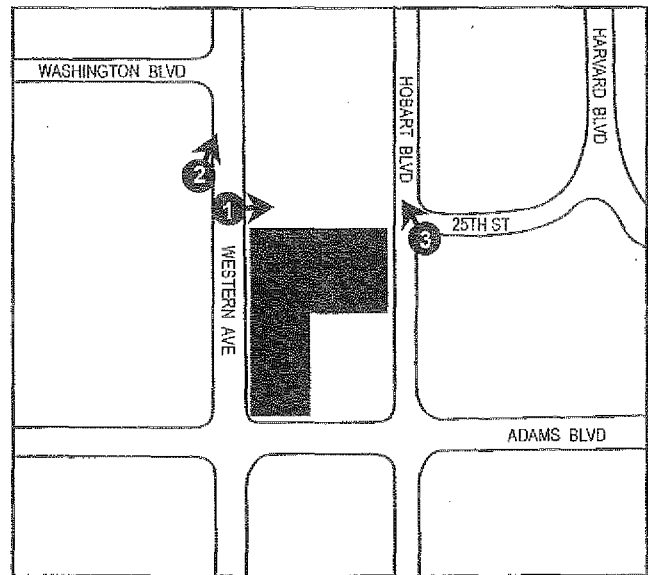
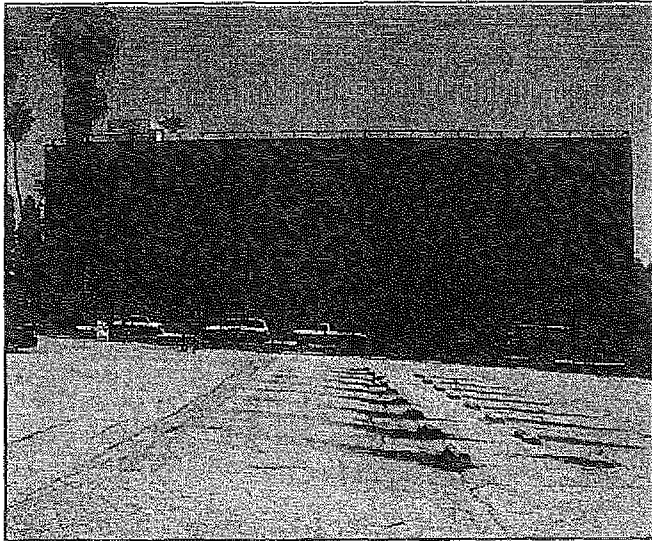


Photo Location Map

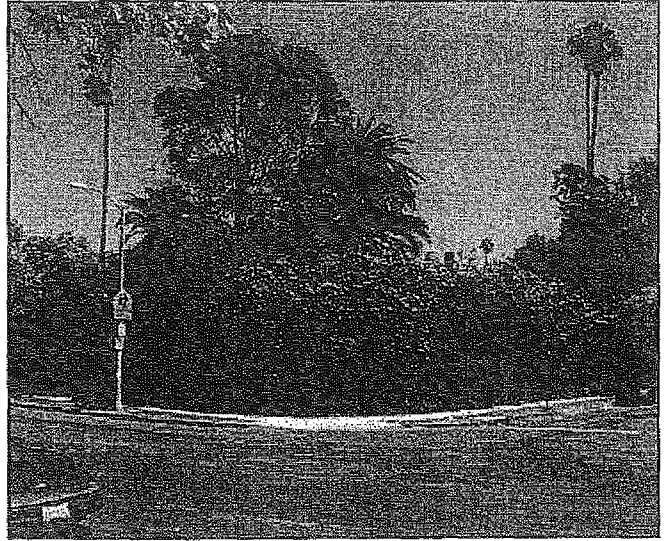
Source: Matrix Environmental, 2010.



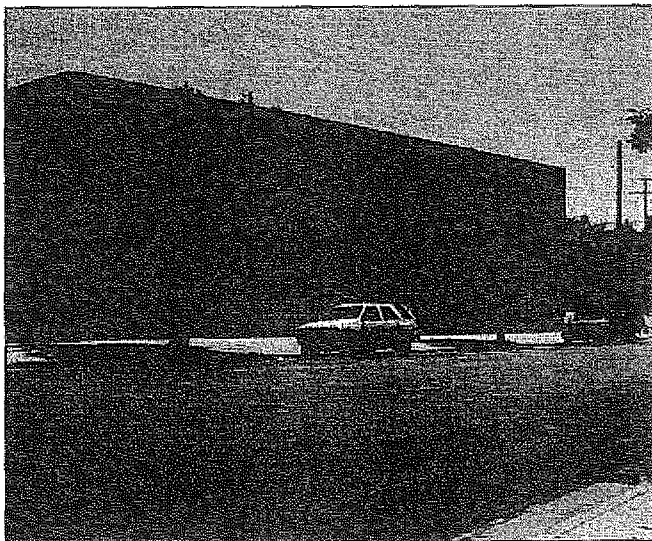
Figure II-3
Views of the Project Site and Surrounding Uses,
Views 1, 2, and 3.



View 4: Five-story apartment building across Hobart Boulevard from Project Site (on-site surface parking lot is in foreground).



View 5: Fredrick Hastings Rindge House, north of the five-story apartment building.



View 6: One-story commercial building south of the five-story apartment building.

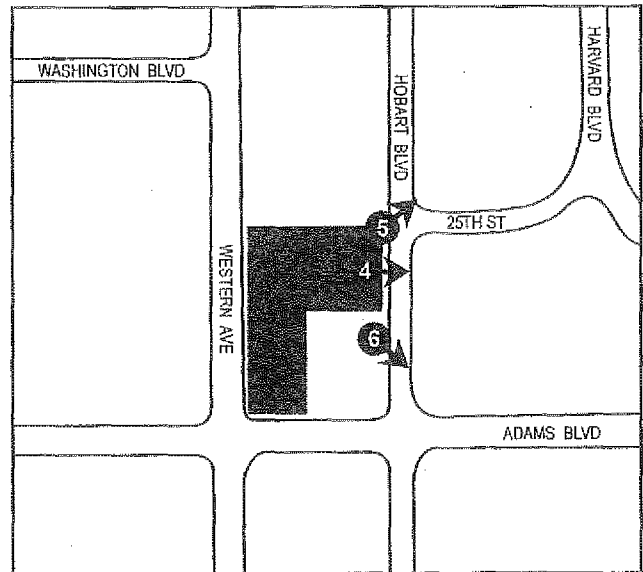
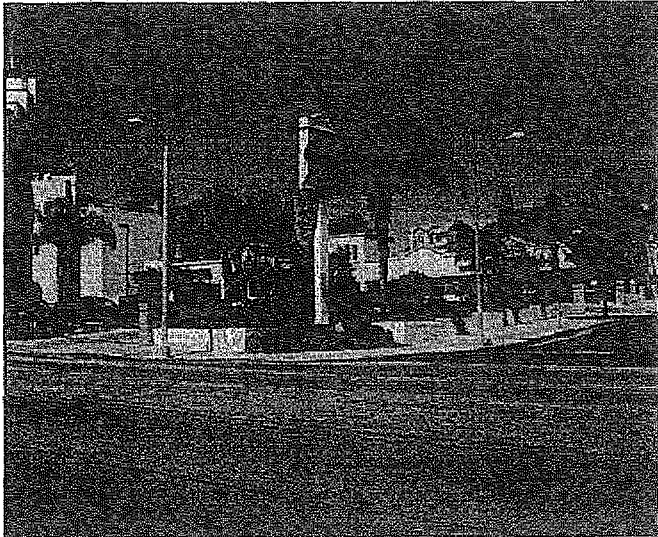


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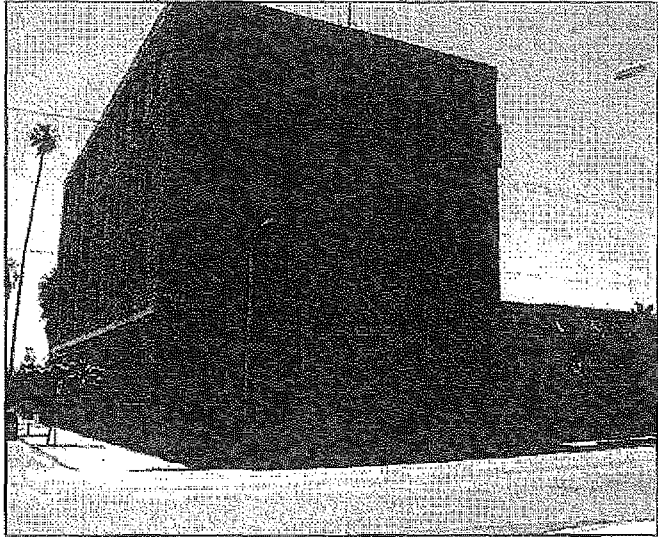
Source: Matrix Environmental, 2010.



Figure II-4
Views of the Project Site and Surrounding Uses,
Views 4, 5, and 6



View 7: One-story commercial center bordering the southeast corner of the Project Site.



View 8: Five-story FAC headquarters across West Adams Boulevard from the off-site one-story commercial center.



View 9: Surface parking lot across Adams Boulevard from the on-site Golden State Building.

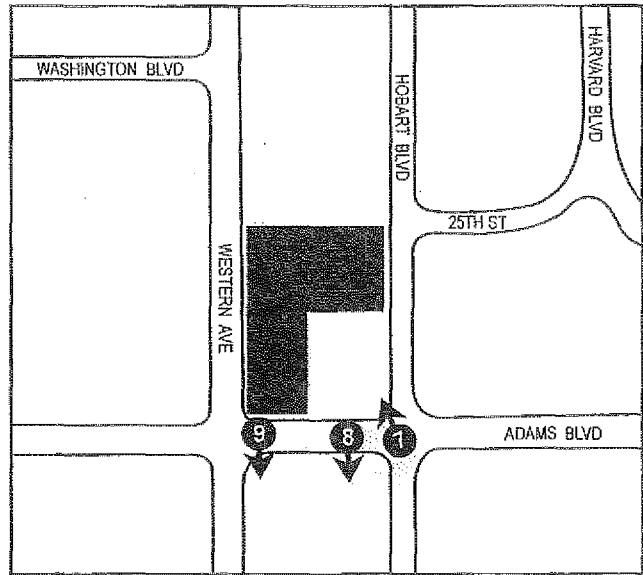


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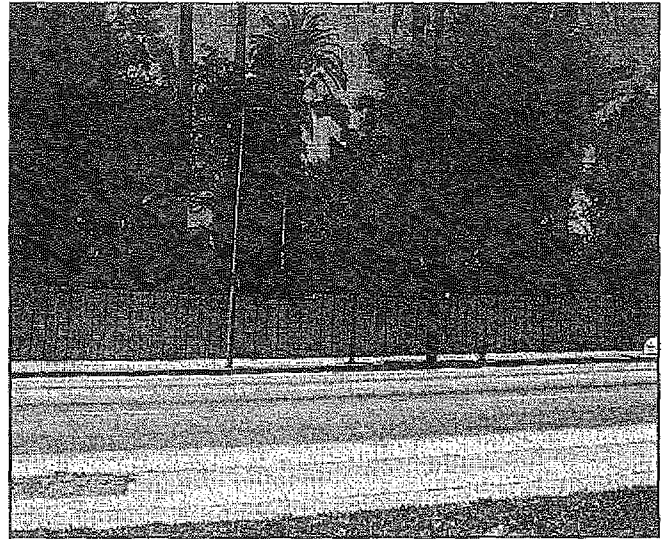
Source: Matrix Environmental, 2010.



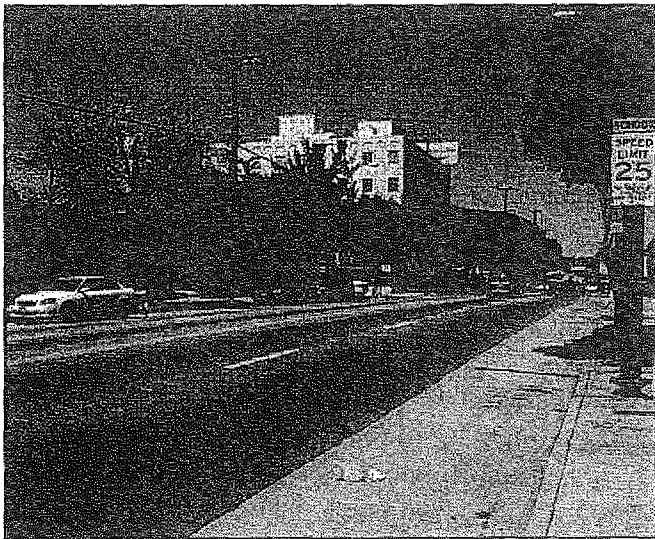
Figure II-5
Views of the Project Site and Surrounding Uses,
Views 7, 8, and 9



View 10: St. John of God Retirement Center west across Western Avenue from the on-site Golden State Building.



View 11: Ramsey - Durfee Estate across Western Avenue from on-site surface parking lot.



View 12: Land uses on Western Avenue north of the Ramsey - Durfee Estate.

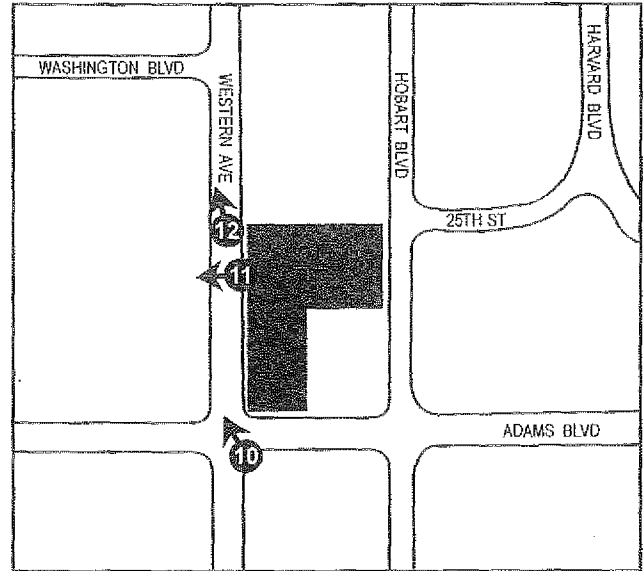


Photo Location Map

Source: Matrix Environmental, 2010.



Figure II-6
Views of the Project Site and Surrounding Uses,
Views 10, 11, and 12

disabilities and seniors 62 years and older. North of the FAME Arms Apartments, land uses include multi-story apartment buildings located along the west side of Hobart Boulevard and a mixture of apartment, commercial, educational, and medical-related land uses along the east side of Western Avenue. For a visual depiction of land uses north of the Project Site, see Figure II-3 on page II-7.

The property east of the Project Site, across Hobart Boulevard, consists of a five-story, brick apartment building. North of this brick apartment building, along the east side of Hobart Boulevard, is the Frederick Hastings Rindge House, a Renaissance Revival-style mansion which was listed on the National Register of Historic Places (NRHP) in 1986. The property south of the brick apartment building consists of a single-story, concrete commercial building that fronts Adams Boulevard. A mixture of commercial and residential land uses continue east of these structures, with commercial uses being primarily located along Adams Boulevard. For a visual depiction of land uses east of the Project Site, see Figure II-4 on page II-8.

Properties southeast and south of the Project Site consist of a single-story commercial center and the FAME Assistance Corporation (FAC) headquarters (and associated parking lot) south across Adams Boulevard. The commercial center, named Adams Plaza, located at the northwest corner of Adams and Hobart Boulevards, contains community-serving retail uses, including a WIC location, a laundromat, and a discount store. The FAC headquarters is housed in a four-story brick building with modern renovations. A parking lot serving the FAC headquarters is located immediately adjacent to the west side of the building, across Adams Boulevard from the Golden State Building. Land uses in the vicinity of these structures include commercial and community-serving retail uses located along the south side of Adams Boulevard in both directions. Land uses south of the FAC headquarters include single-family residential homes, with single-story community-retail/restaurant uses located along Hobart Avenue. For a visual depiction of land uses south of the Project Site, see Figure II-5 on page II-9.

The properties west of the Project Site, across Western Avenue, consist of the four-story St. John of God Retirement and Care Center (the St. John of God Retirement Center) and the Ramsey-Durfee Estate. The St. John of God Retirement Center, located directly across Western Avenue from the Golden State Building, is a modern, four-story residential apartment building with a stucco exterior. Balconies and landscaping are located along Western Avenue; with landscaping that consists of palm trees and low-lying vines and shrubs. Directly north of the St. John of God Retirement Center, across Western Avenue from the on-site surface parking lot, is the Ramsey-Durfee Estate (the Estate). The Estate consists of a large, Tudor-Revival style mansion (named Villa Maria) set back on the property and buffered from Western Avenue by extensive landscaping. The Estate is now used as the headquarters for the Brothers of St. John of God. The Estate is listed as a

Historic Cultural Monument by the Los Angeles Cultural Heritage Commission and is also listed on the NRHP. North of the Estate is a mixture of single-family homes, apartment buildings, and commercial and retail uses. For a visual depiction of land uses west of the Project Site, see Figure II-6 on page II-10.

B. Project Goals and Objectives

The primary goal of the Proposed Project is to replace an existing vacant parking lot, with a new office building which is anticipated, though not required, to serve as the headquarters of the South Central Los Angeles Regional Center. Additional objectives of the Proposed Project include the following:

- To provide a well-designed, high-quality office building that complements and enhances the Community Plan Area and Redevelopment Plan Area and implements good planning principles by constructing an office project along a major arterial and transit corridor;
- To locate a new office use on an underutilized commercial parcel, returning the Project Site to an economic use;
- To reduce blight and revitalize an existing urban site that lays virtually vacant as a paved surface parking lot;
- To promote the economic, social, and cultural and physical well-being of the Project area by contributing to the revitalization of the Community Plan Area;
- To provide job opportunities for the surrounding community;
- To improve the landscaping and streetscape appearance along Western Avenue and Hobart Boulevard; and
- To bring the Community Plan land use designation, zoning, and permitted FAR into consistency with the Redevelopment Plan and existing uses, as it pertains to the Project Site.

C. Proposed Project Description

The Proposed Project would transform an underutilized surface parking lot which extends from Western Avenue to Hobart into a modern office building. While the new building is anticipated to serve as the headquarters for the South Central Los Angeles Regional Center, a non-profit organization serving individuals with developmental disabilities and their families, it could serve other office tenants as well.

The Proposed Project would replace the existing on-site surface parking lot with a three-story office building with three-levels of subterranean parking in its place. The proposed office building would be approximately 75,000 gross square feet (sf) in size (providing approximately 70,000 sf of interior leasable floor area), while the subterranean parking garage would provide a minimum of 320 parking spaces. Along with the existing Golden State Building, approximately 109,800 sf of floor area would be provided on the site. This equates to an on-site floor-to-area ratio (FAR) of 1.66:1.

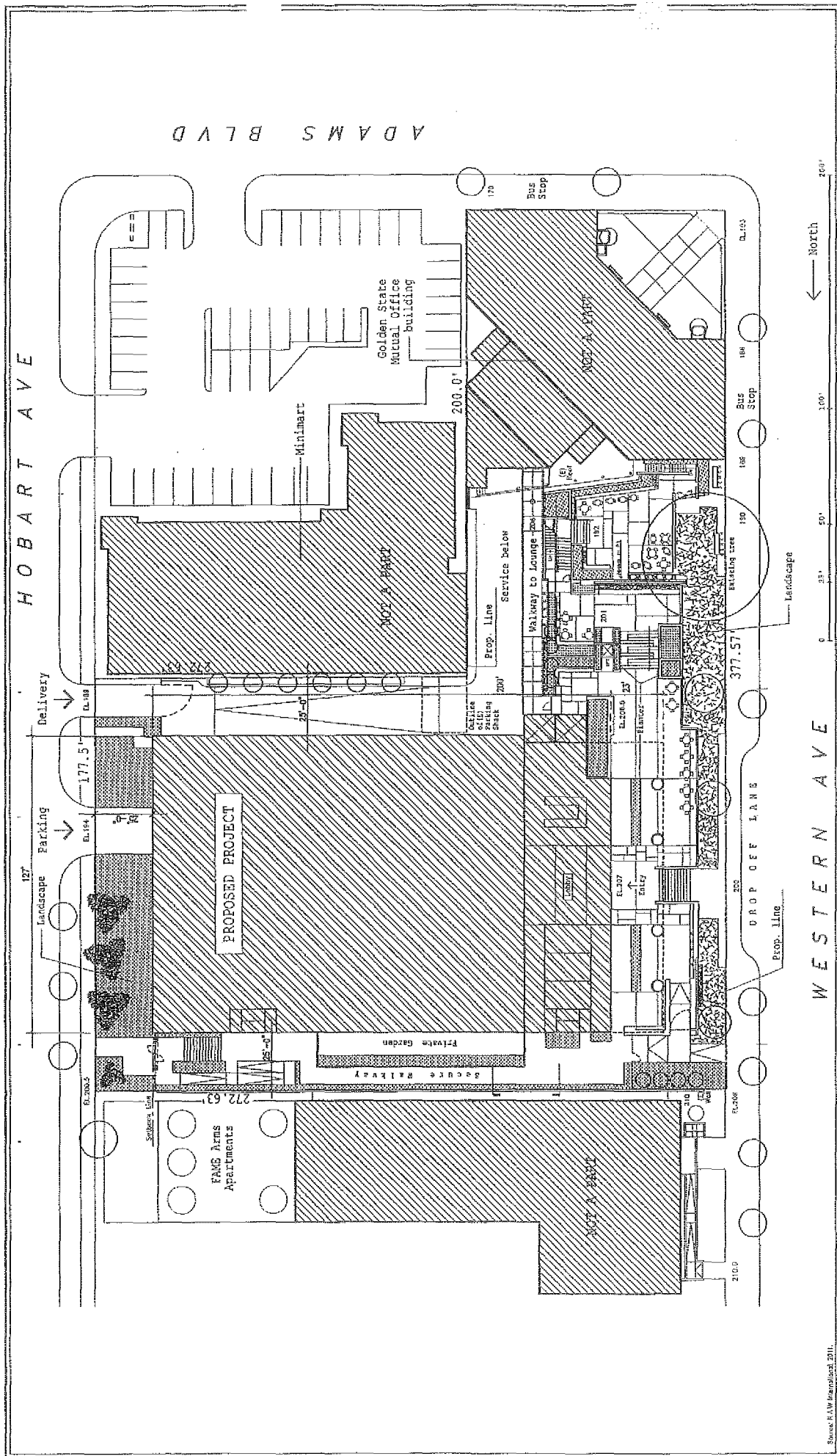
As the Project Site slopes downward to the south, the height of the building would vary depending on the measurement location. However, the Proposed Project would rise approximately 75 feet above grade at the southern edge of the proposed building, along the service drive. The northern edge of the proposed building would rise approximately 45 feet above grade. The slope of the Project Site would expose the southern edge of the top level of the subterranean parking garage. In addition, the Project includes a roof-top mechanical screen (approximately 10 feet in height above the parapet wall), two solar screens (approximately 18 feet in height) at the southwest corner of the building, and one proposed sign (approximately 32 feet in height) located 20 feet above the Project's parapet wall.

The Project would also locate pedestrian linkages between the proposed office building and the existing Golden State Building. The project would also provide open space, as discussed in the open space and landscaping discussion below.

In addition to the above features, a secure walkway and private garden would be provided adjacent to the north side of the proposed office building. Signage identifying the building's tenant would be placed on top of the proposed building. Further, a service area would be provided in the center of the Project Site, between the proposed office building and the existing Golden State Building.

Primary pedestrian access to the proposed office building would be from Western Avenue via stairs located at the building's main entrance. To accommodate employee and visitor drop-off at the Western Avenue entrance, as well as to ensure adequate traffic flow along Western Avenue, a drop off area would be installed adjacent to the east side of Western Avenue. This vehicle drop off zone would allow motorists to pull out of the main travel lanes of Western Avenue to drop-off/pick-up employees and visitors. Primary vehicle access for workers and visitors would be provided along Hobart Boulevard, at the general location of the existing driveway.

A site plan, building elevations, and building floor plans illustrating the Proposed Project are provided in Figures II-7 through II-11 on pages II-14 through II-18.



ADAMS BLVD

HOBART AVE

WESTERN AVE

North

Figure II-7
Site Plan

Source: A.W. Marshall & Co., 2011.



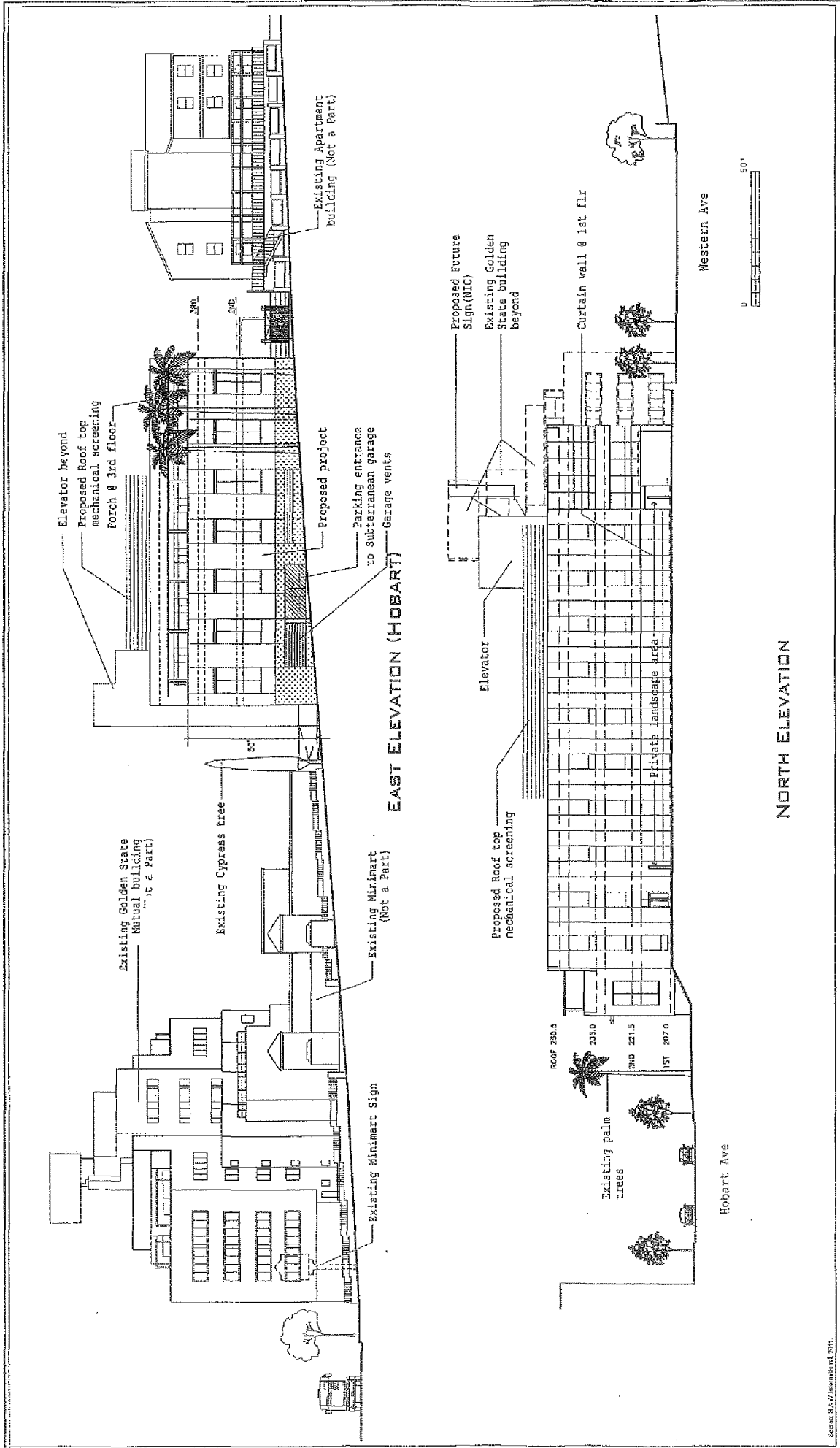


Figure II-8
East and North Elevations

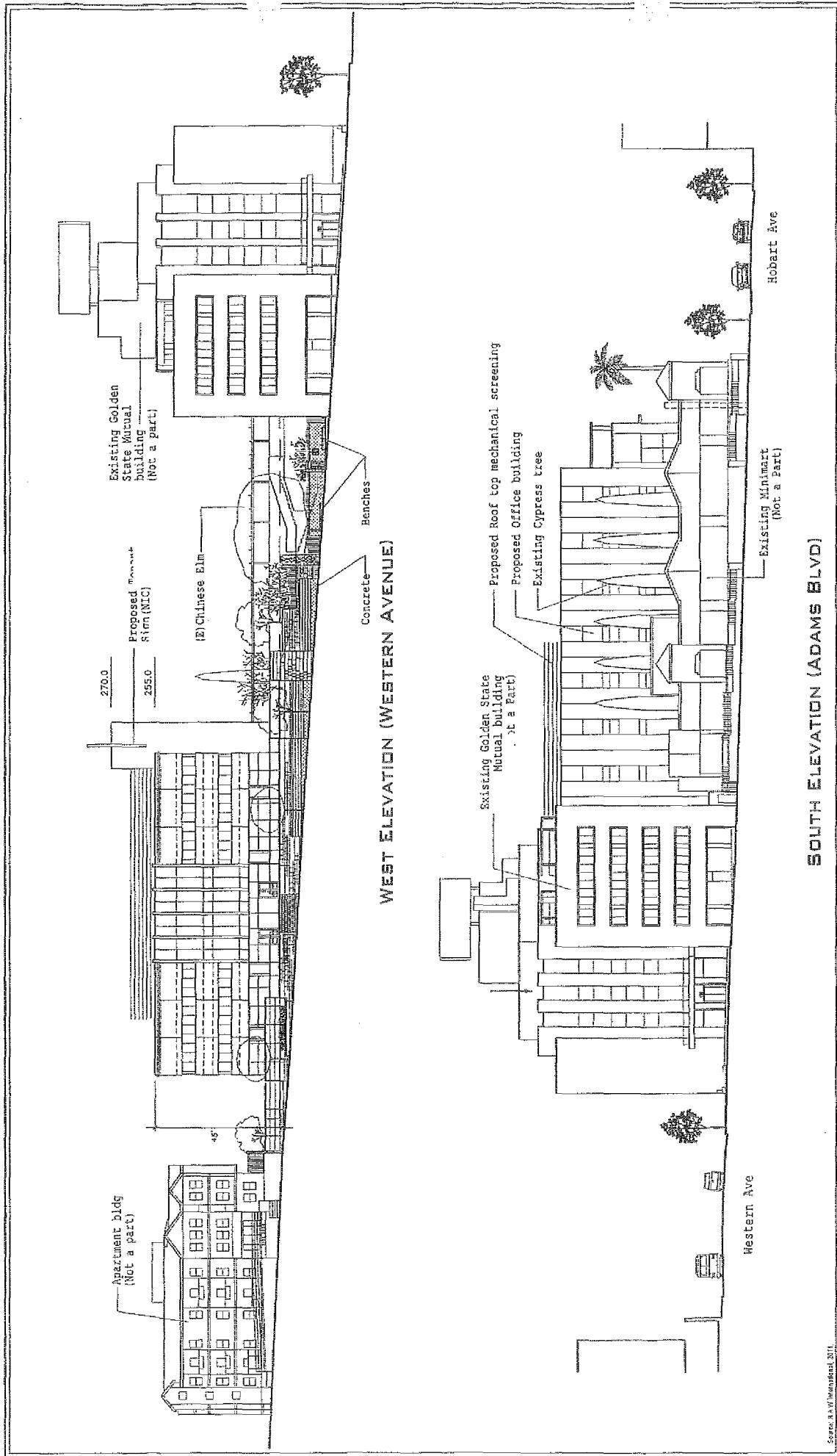
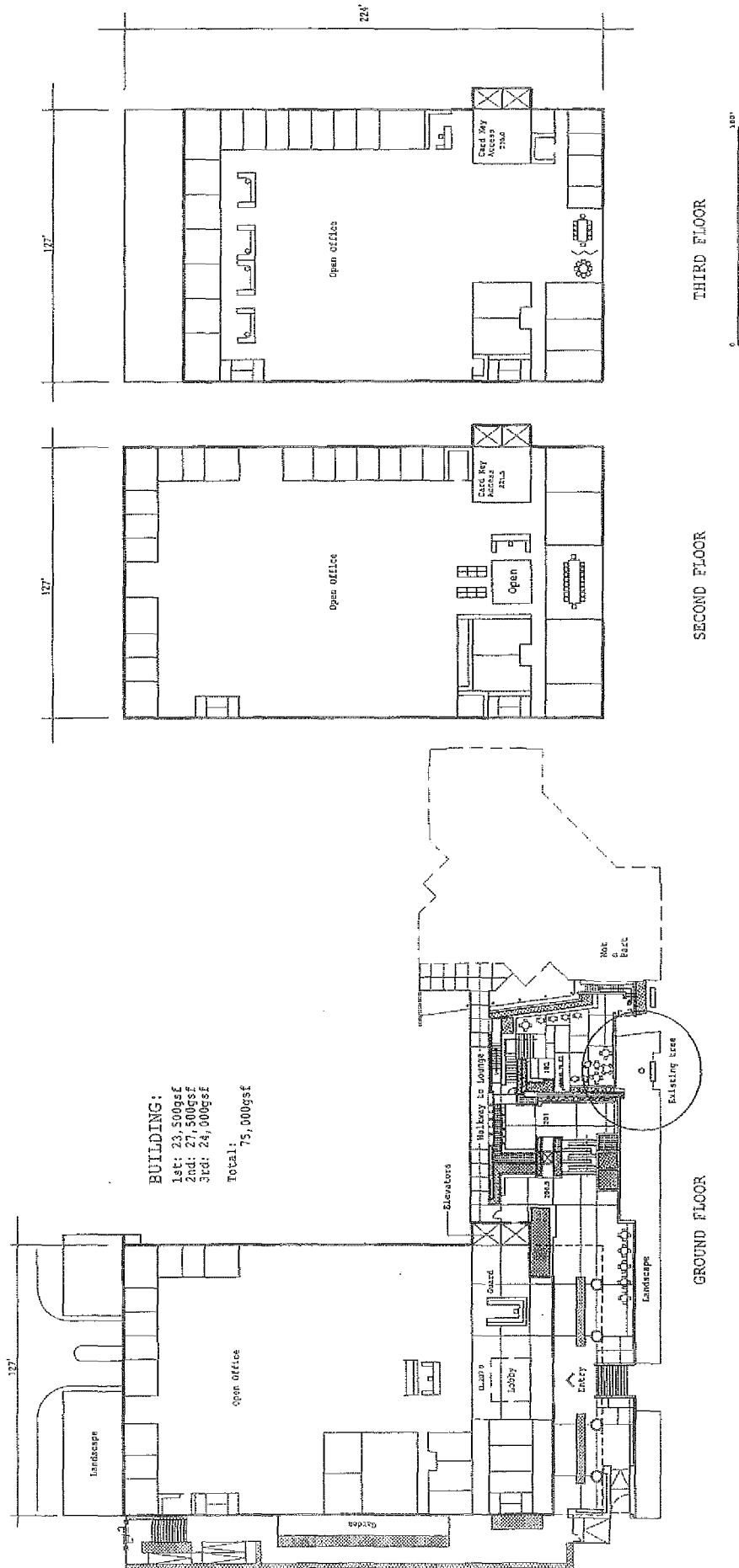


Figure II-9
West and South Elevations



BUILDING:
 1st: 23,500gsf
 2nd: 27,500gsf
 3rd: 24,000gsf
Total: 75,000gsf

THIRD FLOOR

SECOND FLOOR

GROUND FLOOR



Figure II-10
Floor Plans

Sheet C.A.M. Supplemental 2011



1. Project Design and Key Architectural Features

The Proposed Project would provide a well-designed new office building along Western Avenue and Hobart Boulevard, intended to enhance the urban fabric along these roadways while respecting the existing Golden State Building. The Project would be designed incorporating applicable design guidelines as set forth in the Community Plan in order to achieve a high level of quality and distinctive character in its design, and to ensure that it would be compatible with existing uses and development within the vicinity. Further, the proposed building would be arranged and designed to be consistent with its surrounding uses both in use and scale. The proposed three-story office building would be designed to be compatible with the mass and scale of the several multi-story buildings in the area, including the five-story, brick apartment building across Hobart Boulevard, the three-story FAME Arms Apartments to the north, the four-story St. John of God Retirement Center across Western Avenue, the four-story FAC headquarters across Adams Boulevard, and the existing on-site five-story Golden State Building. The Proposed Project would be designed to visually improve the aesthetics of the Project Site by replacing an underutilized surface parking lot with an office building that is consistent with the design guidelines of the Community Plan, including locating parking away from public view, locating commercial uses along the frontage of commercial properties, and providing a pedestrian entrance for developments fronting on main commercial drives.

The proposed office building would also be designed to respect sensitive uses in the Project vicinity, including the FAME Arms Apartments, located adjacent to the north side of the Project Site. The proposed office building would respect this residential use by concentrating the tallest portion of the building (i.e., the portion of the building where the rooftop equipment enclosures and elevator shaft are located) at the southern side of the building. Additionally, the proposed building would be set back from the Project site's northern boundary and a secure walkway and private garden would be located along the northern side of the building. Moreover, all on-site parking would be provided in the subterranean parking structure and vehicle noise and headlights associated with the existing surface parking lot would no longer occur adjacent to this use. Further, the on-site service area would be placed in the center of the Project Site to move loading, delivery, and trash collection activities away from nearby uses. Lastly, as mentioned above, a drop off area would be installed adjacent to the east side of Western Avenue which would address any potential vehicles stopping on the main travel lanes of Western Avenue.

The proposed building would be constructed using materials that would respect nearby uses. The Project would utilize native and unique landscaping, as well as hardscape features, along Western Avenue. To respect the existing uses along Hobart Boulevard, distinctive materials, setbacks, landscape buffering, and security controls would be provided. The two public façades on Western Avenue and Hobart Boulevard would be

comprised of glass curtain walls with either plaster or metal cladding. The northern and southern façades would mostly be concealed from public view. As a result, these façades would have simpler finishing treatments. As mentioned above, due to the Project Site's gentle southern downward slope, the top level of the southern portion of the parking garage would be visible along Hobart Boulevard. However, this portion of the parking garage would receive the same decorative architectural treatments as the Western and Hobart façades, and would be buffered from the Hobart Boulevard streetscape through the use of landscaping. The building's palette of materials would extend to the tiered pedestrian plaza and covered pedestrian bridge.

The Proposed Project would include security features, which are not present at the existing surface parking lot. The Project would promote safety and security by providing adequate security lighting and other measures, including but not limited to, a video surveillance system, on-site security personnel, a monitored alarm system, and controlled access entryways. Also of note is that all on-site security lighting would be shielded and directional so as to prevent spillover onto adjacent properties. Pedestrian lighting would be provided by means of low-level lighting fixtures.

2. Parking and Vehicular Access

Access-controlled parking for the Proposed Project would be provided in accordance with City requirements. Parking would be provided at a rate that exceeds the City's requirement of 2 spaces per 1,000 square feet. Parking for the office building would provide a minimum of 320 on-site parking spaces in a subterranean parking garage.

Vehicular access to the Project Site and subterranean garage would occur via Hobart Boulevard, as is the case for the existing surface parking lot. No vehicular access would for visitors or workers would be provided along Western Avenue or Adams Boulevard, designated major highways. However, as mentioned above, a drop-off area would be added to northbound Western Avenue. This drop-off area would allow motorists to pull out of the main traffic lanes on Western Boulevard to drop-off/pick-up employees and visitors.

3. Open Space and Landscaping

The Proposed Project would transform a currently underutilized surface parking lot into an office building with complementary landscaping and vegetation to enhance the Western Avenue and Hobart Boulevard streetscape. Within the Proposed Project, landscaping elements would include trees and planters along street frontages, as well as landscaping along Western Avenue to provide a pleasant pedestrian atmosphere. A large Chinese elm tree located along the Project Site's western boundary, just north of the

Golden State Building, and the single cypress tree along the Project Site's southern edge by the existing off-site mini-mart, would be retained. All other existing on-site landscaping would be removed.

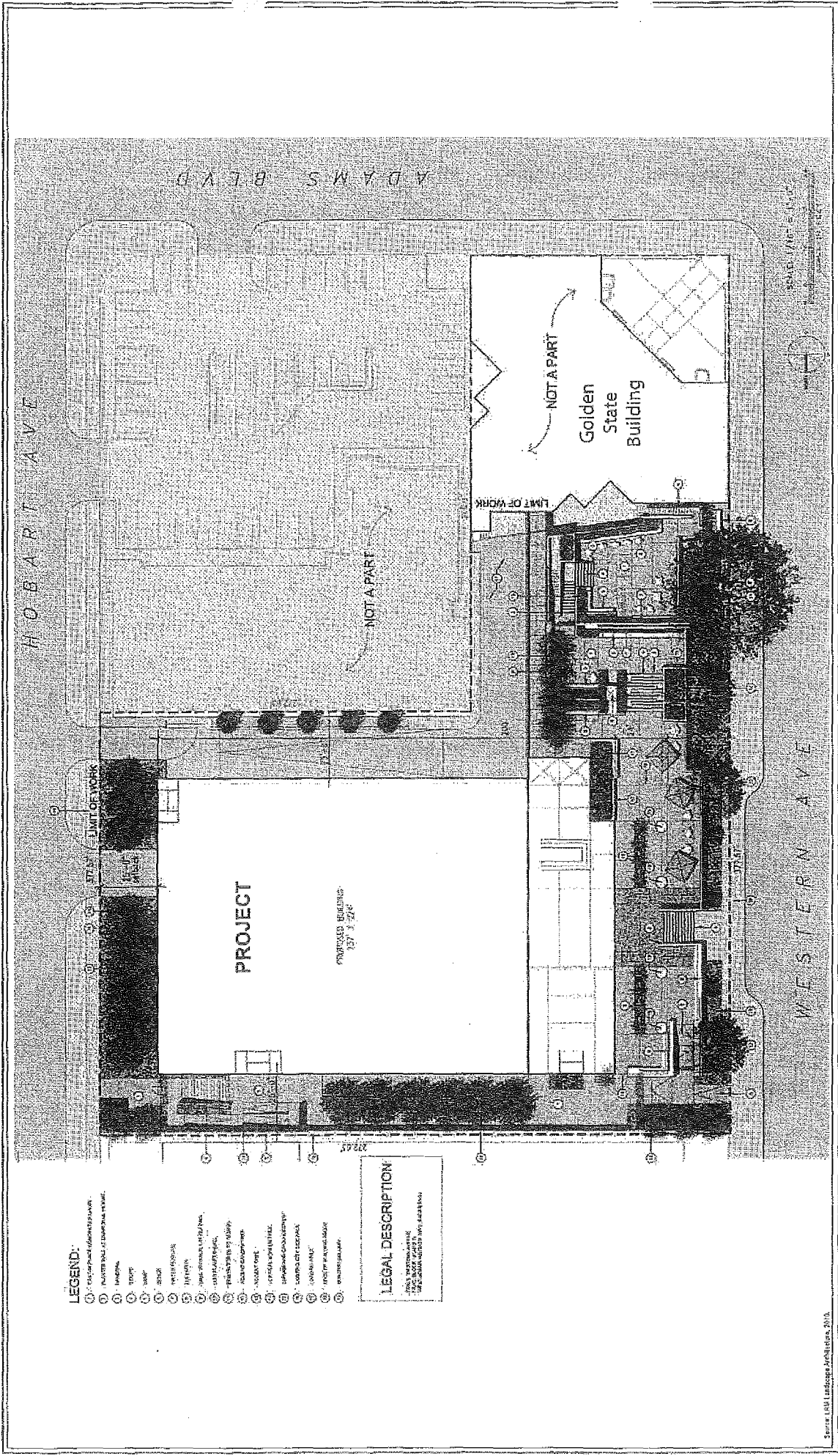
Landscaping improvements along Western Avenue would include a landscaped buffer between the existing sidewalk and the proposed building that would provide shrubs and ground cover, as well as accent canopy trees. Landscaping along the southern portion of this area would conceal the top level of the subterranean parking garage. Landscaping improvements along Hobart Boulevard would be similar to that along Western Avenue, and would include an approximately 25-foot wide landscaped area with shrubs, accent canopy trees, and vertical screen trees. Additional private open space includes a private, secured walkway and garden located on the north side of the proposed office building.

As mentioned above, the Project includes a landscaped plaza area between the proposed office building and the existing Golden State Building, adjacent to Western Avenue.

All landscaping would be installed in accordance with the City of Los Angeles Landscape Ordinance, No. 170,978, as amended. Water-wise irrigation practices would be employed and monitored to deliver required amounts of water only when needed. Seasonal adjustments would be made to the watering schedule as needed. The Project's preliminary conceptual landscaping plan is depicted in Figure II-12 on page II-22.

4. Community Plan Land Use Designations and Zoning

The existing Community Plan and zoning designations, which were originally established under Case No. ZA 9646, have been in place for more than 50 years, and are inconsistent with the current Redevelopment Plan. To implement the Project as proposed, as well as to make the entire Project Site consistent with the Redevelopment Plan, a General Plan Amendment for the easternmost 150 feet of the Project Site is required. The proposed General Plan Amendment seeks to change the land-use designation for the easternmost 150 feet of the Project Site from Low Medium II Residential to Community Commercial, and to amend Footnote 1 of the Community Plan Land Use map to allow a redesignation of the Project site to Height District 2. In connection with this General Plan amendment, the Project is requesting a Vesting Zone Change for this portion of the Project Site from RD1.5 (Residential) to C2 (Commercial). Lastly, the Proposed Project is requesting a change to the Project Site's height district to accommodate the proposed office building. Specifically, the Proposed Project would change the existing height designation from Height District 1 to Height District 2. These proposed changes to the existing Community Plan and zoning designations would make the Project Site's existing land use designations consistent with those set forth in the current Redevelopment Plan.



- LEGEND:**
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LEGAL DESCRIPTION

1/2 ACRES MORE OR LESS
 1/2 ACRES MORE OR LESS
 1/2 ACRES MORE OR LESS

Figure II-12
 Conceptual Landscape Plan

Source: LEI Landscape Architecture, 2013.

D. Construction/Phasing

The Proposed Project would be constructed over an approximately 14 month time period. The Project would be built in one phase and demolition is anticipated to begin as soon as project approvals are obtained.

E. Discretionary Actions

Implementing the Project as proposed requires various discretionary actions by the City of Los Angeles. Required discretionary actions to be implement the Project as proposed include the following:

- General Plan Amendment to change the General Plan land use designation from Low Medium II Residential to Community Commercial and to amend Footnote No. 1 to the South Los Angeles Community Plan land use map to allow the property to be designated in Height District 2;
- Vesting Zone Change from RD1.5 to [Q]C2 and from C2 to [Q]C2 to allow the construction of a 70,000-square-foot office building with 320 parking spaces;
- Height District Change from Height District 1 to Height District 2D (with a FAR of 2.0:1) for the entire site;
- Site Plan Review for the proposed development project that includes a roof-top mechanical screening (approximately 10 feet in height), two solar screens (approximately 18 feet in height) and one proposed future new sign (approximately 32 feet in height) located 20 feet above the proposed building's parapet wall;
- Demolition, grading, foundation, and building permits;
- Haul route approval for proposed excavation of approximately 45, 200 cubic yards of earth material to be exported for the proposed construction of an office building; and
- Any additional actions as may be determined necessary.

F. Related Projects

Section 15063(b) of the State CEQA Guidelines provides that an Initial Study consider the environmental effects of a Proposed Project individually as well as

cumulatively. Section 15355 of the State CEQA Guidelines defines cumulative impacts as two or more individual effects which, when considered together, are considerable or which compound or increase other environmental impacts. These include those projects which are proposed, recently approved, under construction, or reasonably foreseeable and which could produce a cumulative impact on the environment when considered in combination with the Proposed Project. As shown in Table II-1 on page II-25, a total of 14 related projects were identified through coordination with the Los Angeles Department of Transportation.

II. Project Description

Table II-1
List of Related Projects

Project No.	Project Name	Location	Size
1	Day Care	1140 Crenshaw Boulevard	5,000 square feet (sf)
2	Private Elementary School	1932 10th Avenue	90 students
3	Shopping Center	1144 South Western Avenue	21,684 sf
4	Starbuck's Coffee with Drive-Thru	4177 West Washington Boulevard	1,600 sf
5	LAUSD—Central Region Elementary School #5	Budlong Avenue & Washington Boulevard	575 students
6	USC Parkside II Residential Tower (College Dormitory)	920 West 37th Street	160,000 sf
7	LAUSD—Central Region Elementary School #15	3rd Avenue & Washington Boulevard	875 students
8	Shopping Center	3060 South Crenshaw Boulevard	109,006 sf
9	Retail, Office, & Bank	3060 South Crenshaw Boulevard	Retail (13,969 sf) Office (25,015 sf) Bank (6,000 sf)
10	Washington Square Mixed-Use Development	4040 West Washington Boulevard	Condominiums (219 units) Apartments (200 units) Live/Work Units (128 units) Retail (82,500 sf) Restaurant (18,800 sf) High-Turnover Restaurant (5,500 sf)
11	Condominiums	3001 South Western Avenue	66 units
12	Restaurant	3267 West Olympic Boulevard	17,033 sf
13	Mixed-Use Development	2401 West Jefferson Boulevard	Apartments (52 units) Retail (20,877 sf)
14	ICEF Adams School Expansion (Charter School)	3200 West Adams Boulevard	150 students

Source: Traffic Study for the 2500 S. Western Office Project, prepared by Gibson Transportation Consulting, Inc. March 2011. Related projects derived from coordination with the Los Angeles Department of Transportation.

Section III. Initial Study Checklist

LEAD CITY AGENCY Department of City Planning		COUNCIL DISTRICT 8	DATE October 4, 2011
RESPONSIBLE AGENCIES			
PROJECT TITLE/NO. 2500 South Western Avenue Office Project		CASE NO. ENV-2010-328-MND	
PREVIOUS ACTIONS CASE NO.	<input type="checkbox"/> DOES have significant changes from previous actions. <input checked="" type="checkbox"/> DOES NOT have significant changes from previous actions.		

PROJECT DESCRIPTION:

The Proposed Project is a three-story office building with approximately 70,000 square feet of floor area, that includes roof-top mechanical screening (approximately 10 feet in height) , 2 solar screens (approximately 18 feet in height), and one proposed future new sign (approximately 32 feet in height) located 20 feet above the proposed building's parapet wall, a pedestrian bridge on the 2nd floor that connects to the Golden State Mutual Office Building, and 3 levels of subterranean parking with 320 parking stalls, all on an approximately 70,763 square-foot site in the C2-1 Zone, in conjunction with a: (1) haul route, (2) general plan amendment, (3) vesting zone change, (4) height district change, and (5) site plan review, as follows:

- (1) a Haul Route approval for proposed excavation of approximately 45,200 cubic yards of earth material to be exported for the proposed construction of an office building;
- (2) a General Plan Amendment to change the general plan land use designation from Low Medium II Residential to Community Commercial and to amend Footnote No. 1 to the South Los Angeles Community Plan land use map to allow the property to be designated in Height District 2;
- (3) a Vesting Zone Change from RD1.5 to [Q]C2 and from C2 to [Q]C2 to allow the construction of a 70,000-square-foot office building with 320 parking spaces;
- (4) a Height District Change from Height District 1 to Height District 2D (with a 2:1 FAR Limitation) for the entire site; and
- (5) Lastly, Site Plan Review for the proposed development project that includes roof-top mechanical screening (approximately 10 feet in height), 2 solar screens (approximately 18 feet in height) and one proposed future new sign (approximately 32 feet in height) located 20 feet above the proposed building's parapet wall.

The Proposed Project also includes a tiered pedestrian plaza and a covered pedestrian bridge that connects the Proposed Project with the existing Golden State Building located at the northeast corner of Western Avenue and Adams Boulevard. The Proposed Project would remove an existing on-site surface parking lot to accommodate the proposed office building.

ENVIRONMENTAL SETTING:

The Proposed Project is situated on approximately 1.62 acres of land located generally at the northeast corner of the intersection of Western Avenue and Adams Boulevard in the South Central Los Angeles Community Plan Area of the City of Los Angeles. The Project Site is approximately three miles west of Downtown Los Angeles and approximately 11 miles east of the Pacific Ocean. The Project Site is bound by a retirement center to the north, Hobart Boulevard and a commercial center to the east, Adams Boulevard to the south, and Western Avenue to the west. Properties surrounding the site are mostly occupied by multi-story commercial and residential buildings, and a one-story commercial center.

Section III. Initial Study Checklist

PROJECT LOCATION

The northeast corner of the intersection of Western Avenue and Adams Boulevard in the City of Los Angeles.

PLANNING DISTRICT

South Los Angeles

STATUS:

- PRELIMINARY
- PROPOSED
- ADOPTED March 22, 2000
date

EXISTING ZONING C2-1 (Commercial) and RD1.5 (Residential)	MAX. DENSITY ZONING N/A	<input type="checkbox"/> DOES CONFORM TO PLAN <input checked="" type="checkbox"/> DOES NOT CONFORM TO PLAN <input type="checkbox"/> NO DISTRICT PLAN
PLANNED LAND USE & ZONE C2-2	MAX. DENSITY PLAN N/A	
SURROUNDING LAND USES Retail, Commercial, and Multi-family Residential	PROJECT DENSITY N/A	



DETERMINATION (To be completed by Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions on the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

SIGNATURE

TITLE

EVALUATION OF ENVIRONMENTAL IMPACTS:

- 1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants based on a project-specific screening analysis).
- 2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4) "Negative Declaration: Less Than Significant With Mitigation Incorporated" applies where the incorporation of a mitigation measure has reduced an effect from "Potentially Significant Impact" to "Less Than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section XVII, "Earlier Analysis," cross referenced).
- 5) Earlier analysis must be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR, or negative declaration. Section 15063 (c)(3)(D). In this case, a brief discussion should identify the following:
 - 1) Earlier Analysis Used. Identify and state where they are available for review.
 - 2) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - 3) Mitigation Measures. For effects that are "Less Than Significant With Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
- 6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated
- 7) Supporting Information Sources: A sources list should be attached, and other sources used or individuals contacted should be cited in the discussion.
- 8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whichever format is selected.
- 9) The explanation of each issue should identify:
 - 1) The significance criteria or threshold, if any, used to evaluate each question; and
 - 2) The mitigation measure identified, if any, to reduce the impact to less than significance.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|--|---|--|
| <input checked="" type="checkbox"/> Aesthetics | <input type="checkbox"/> Green House Gas Emissions | <input type="checkbox"/> Population/Housing |
| <input type="checkbox"/> Agricultural Resources | <input checked="" type="checkbox"/> Hazards & Hazardous Materials | <input checked="" type="checkbox"/> Public Services |
| <input checked="" type="checkbox"/> Air Quality | <input checked="" type="checkbox"/> Hydrology/Water Quality | <input type="checkbox"/> Recreation |
| <input checked="" type="checkbox"/> Biological Resources | <input checked="" type="checkbox"/> Land Use/Planning | <input checked="" type="checkbox"/> Transportation/Traffic |
| <input checked="" type="checkbox"/> Cultural Resources | <input type="checkbox"/> Mineral Resources | <input checked="" type="checkbox"/> Utilities/Service Systems |
| <input checked="" type="checkbox"/> Geology/Soils | <input checked="" type="checkbox"/> Noise | <input checked="" type="checkbox"/> Mandatory Findings of Significance |

INITIAL STUDY CHECKLIST (To be completed by the Lead City Agency)

 **BACKGROUND**

PROPONENT NAME Community Impact Development II, LLC Attn: James D. Howard, Jr.	PHONE NUMBER (323) 766-0700
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PROPONENT ADDRESS 1968 W. Adams Blvd., Suite 209 Los Angeles, CA 90018

AGENCY REQUIRING CHECKLIST Department of City Planning	DATE SUBMITTED October 2011
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PROPOSAL NAME (If Applicable) 2500 S. Western Avenue Office Project

ENVIRONMENTAL IMPACTS

(Explanations of all potentially and less than significant impacts are required to be attached on separate sheets)

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
I. AESTHETICS. Would the project:				
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a city-designated scenic highway?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
II. AGRICULTURAL RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:				
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with existing zoning for agricultural use, or a Williamson Act Contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
III. AIR QUALITY. Where available, the significance criteria established by the South Coast Air Quality Management District (SCAQMD) may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IV. BIOLOGICAL RESOURCES. Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as tree preservation policy or ordinance?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
V. CULTURAL RESOURCES. Would the project:				
a. Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
VI. GEOLOGY AND SOILS. Would the project:				
a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving: Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving : Strong seismic ground shaking?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving : Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving : Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potential result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
VII. GREEN HOUSE GAS EMISSIONS. Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
VIII. HAZARDS AND HAZARDOUS MATERIALS. Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
h. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
IX. HYDROLOGY AND WATER QUALITY. Would the project result in:				
a. Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Section III. Initial Study Checklist

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of preexisting nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Place housing within a 100-year flood hazard area as mapped on federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j. Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
X. LAND USE AND PLANNING. Would the project:				
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with applicable land use plan, policy or regulation of an agency with jurisdiction over the project (including but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Conflict with any applicable habitat conservation plan or natural community conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
XI. MINERAL RESOURCES. Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
XII. NOISE. Would the project result in:				
a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
XIII. POPULATION AND HOUSING. Would the project:				
a. Induce substantial population growth in an area either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
XIV. PUBLIC SERVICES.				
a. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Schools?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
XV. RECREATION.				
a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Section III. Initial Study Checklist

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

XVI. TRANSPORTATION/CIRCULATION. Would the project:

a. Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Substantially increase hazards to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Result in inadequate emergency access?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities supporting alternative transportation (e.g., bus turnouts, bicycle racks)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

XVII. UTILITIES. Would the project:

a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

XVIII. MANDATORY FINDINGS OF SIGNIFICANCE

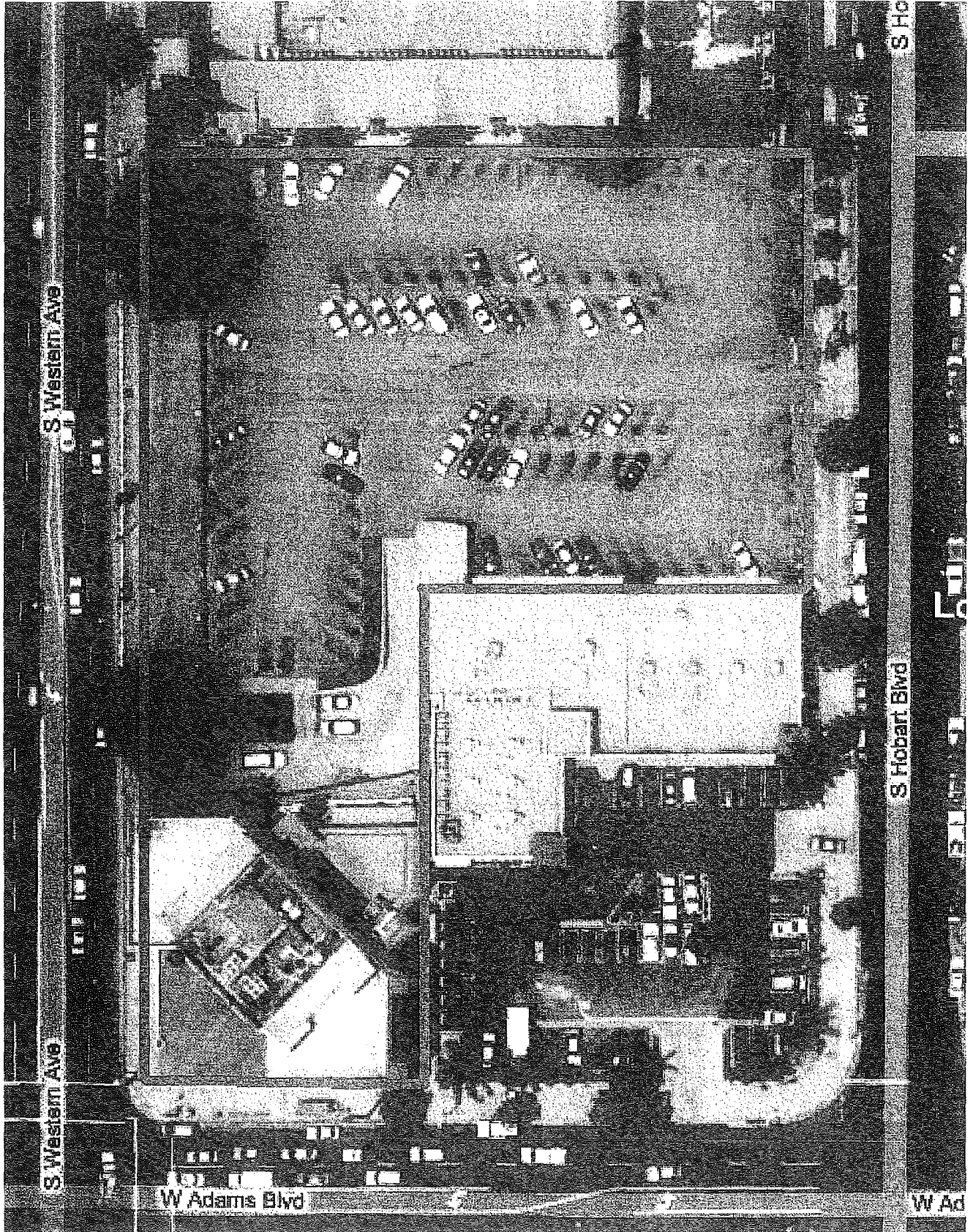
a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Does the project have impacts which are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects).	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Does the project have environmental effects which cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>



DISCUSSION OF THE ENVIRONMENTAL EVALUATION (Attach additional sheets if necessary)

PREPARED BY	TITLE	TELEPHONE #	DATE
Bruce Lackow Matrix Environmental 6701 Center Drive, Suite 900 Los Angeles, CA 90045	President	(424) 207-5333	October 4, 2011

CPC 2010-760

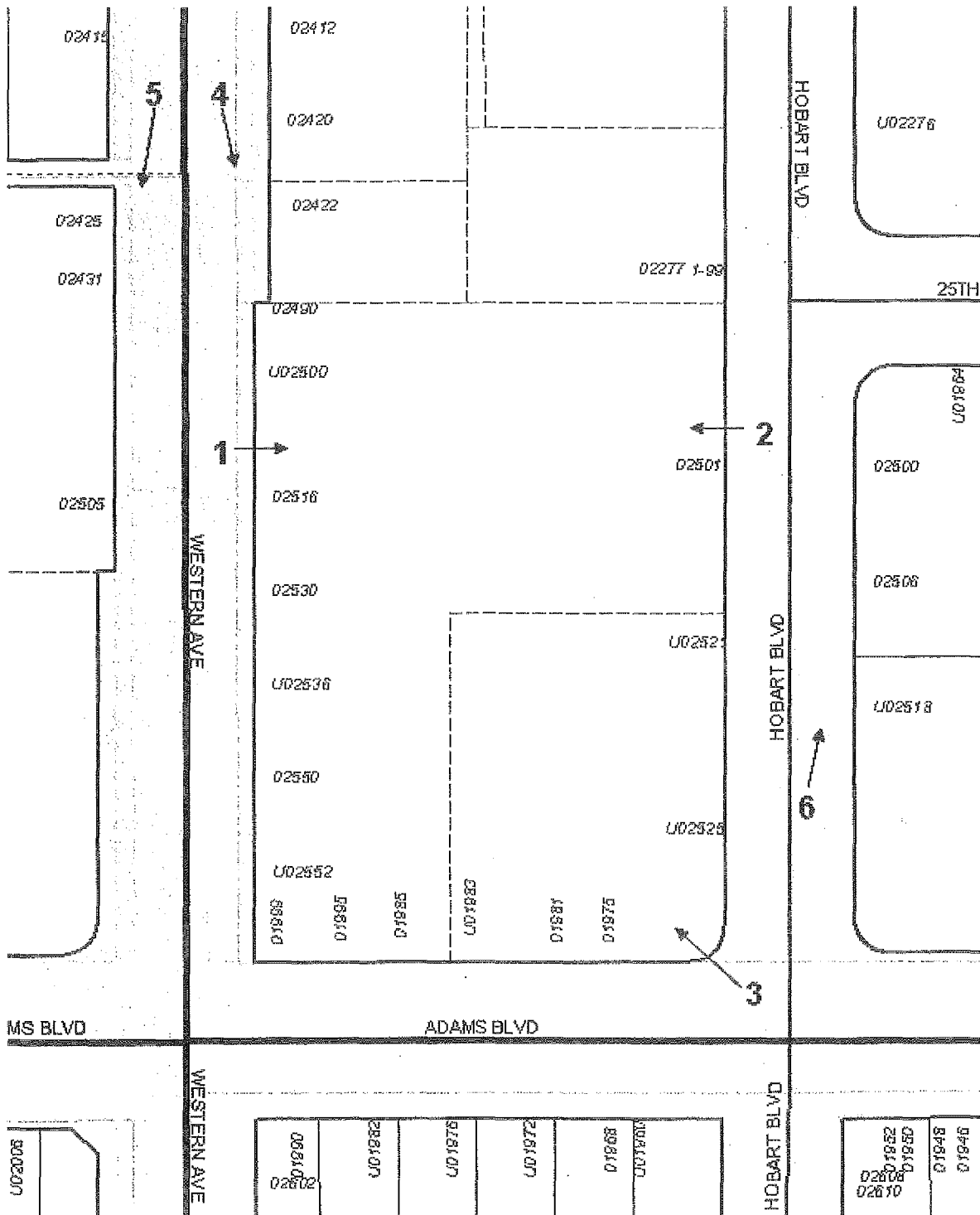


AERIAL PHOTOGRAPH OF SITE

2500 S. WESTERN AVENUE

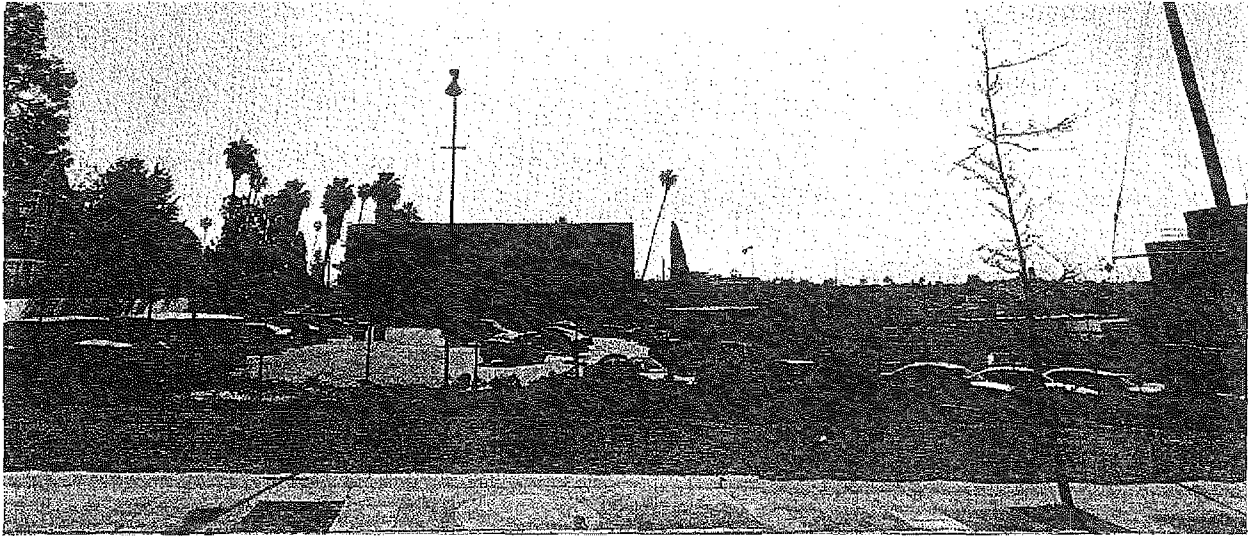
EXHIBIT D
PHOTOS

CPC-2010-0760-GPA-VZC-HD-SPR



PHOTOGRAPH INDEX MAP

2500 S. WESTERN AVENUE

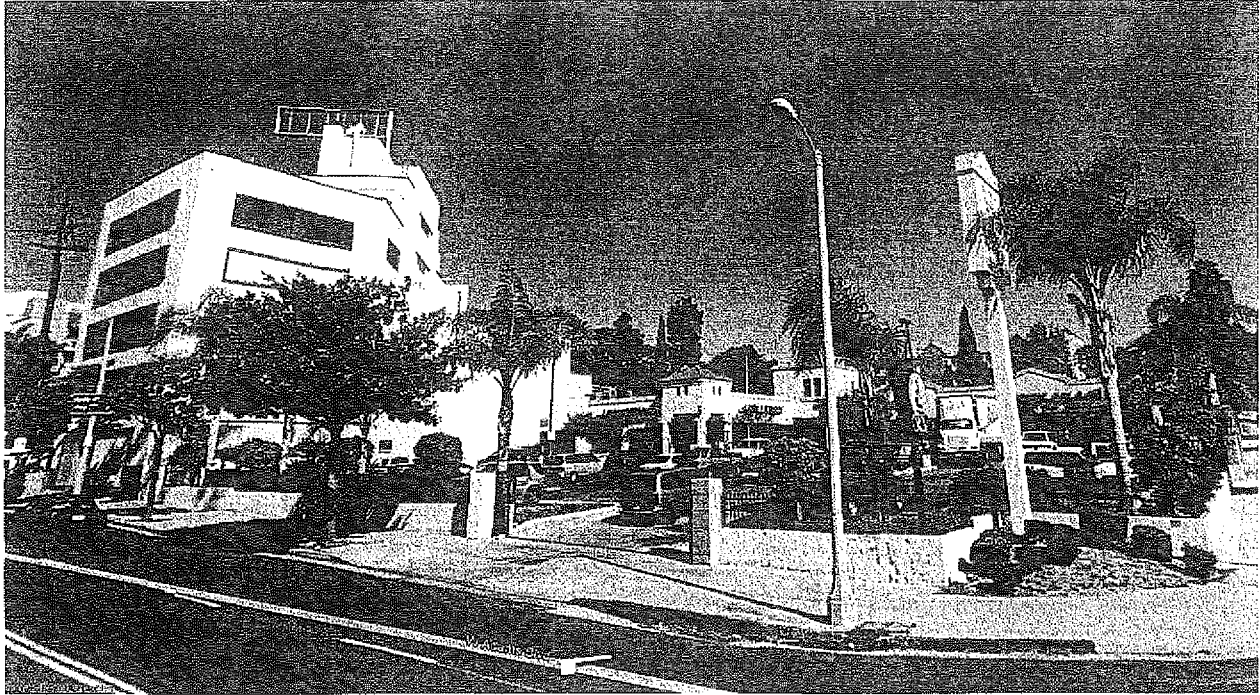


1. VIEW OF SITE LOOKING EAST FROM WESTERN AVENUE

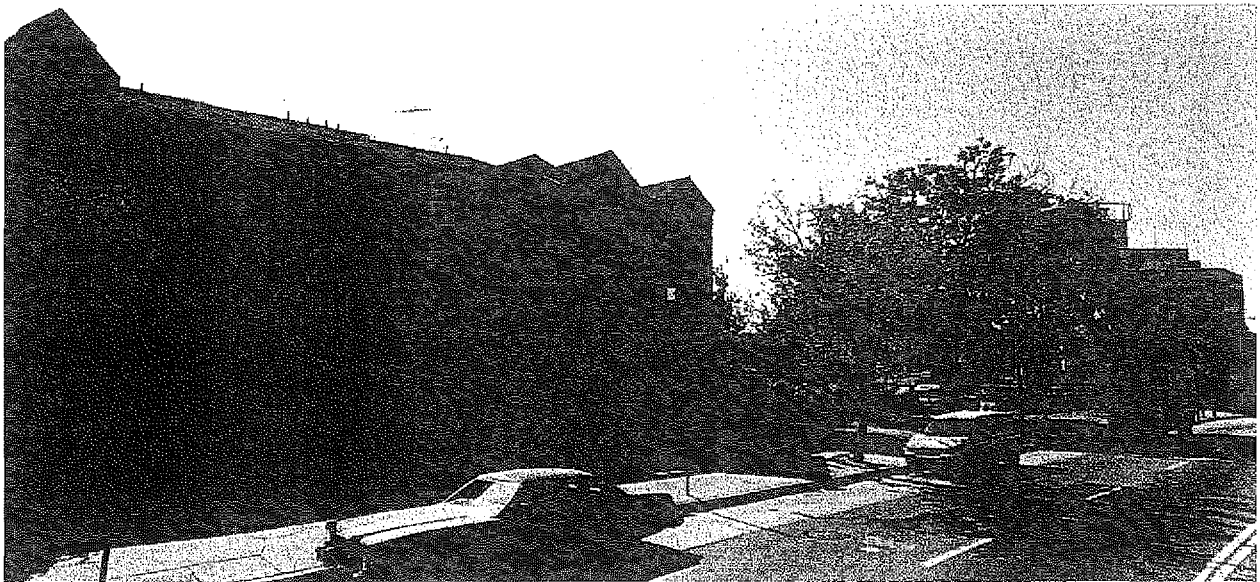


2. VIEW OF SITE LOOKING WEST FROM HOBART BOULEVARD

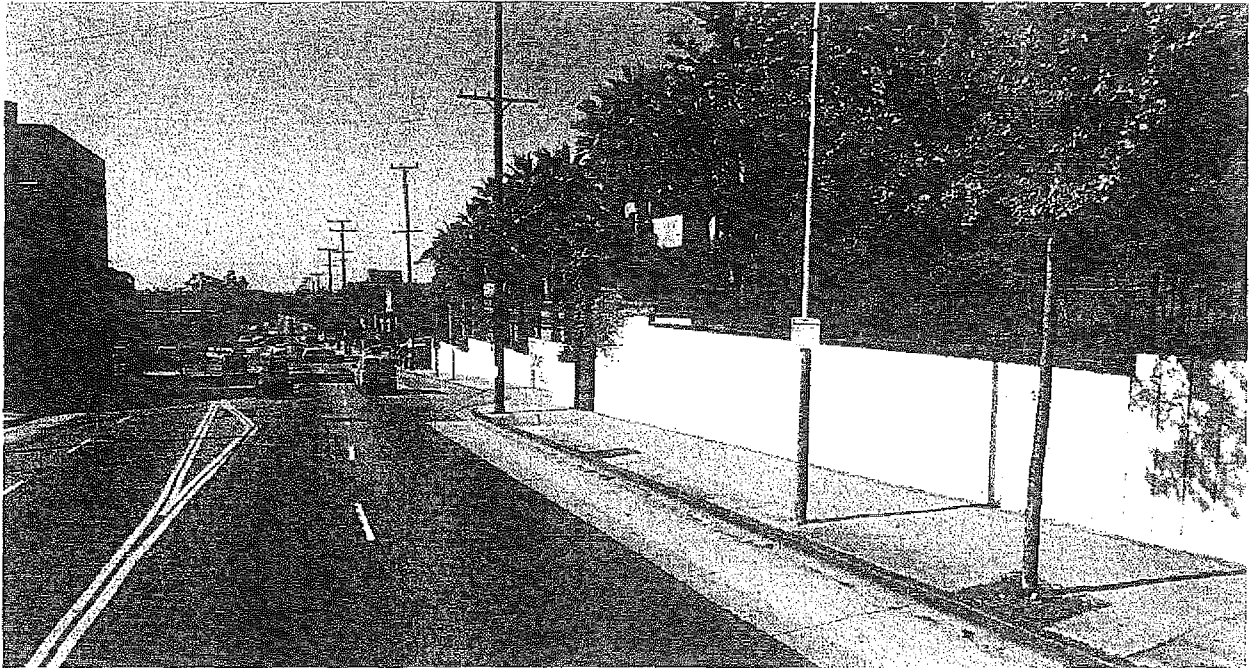
2500 S. WESTERN AVENUE



3. VIEW OF DEVELOPMENT SOUTH OF PROJECT SITE ALONG ADAMS AVENUE



4. VIEW OF DEVELOPMENT NORTH OF PROJECT SITE ALONG WESTERN AVENUE



5. VIEW OF WEST SIDE OF WESTERN AVENUE OPPOSITE PROJECT SITE



6. VIEW OF EAST SIDE OF HOBART BOULEVARD OPPOSITE PROJECT SITE

2500 S. WESTERN AVENUE

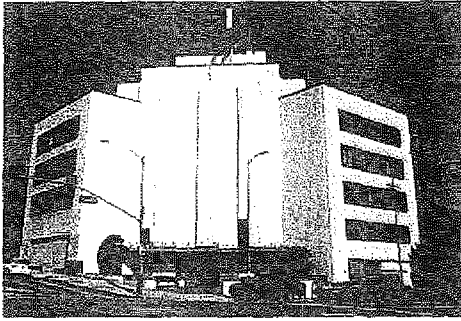


Figure 4: Golden State Mutual Life, South & East Elevations.

4.0 IDENTIFICATION OF HISTORIC RESOURCES

4.1 Historic Resources on the Parcel

Golden State Mutual Life

There is a significant historic building located on the same parcel as the identified Project site.¹⁴ Golden State Mutual Life headquarters is located at 1999 West Adams Boulevard. It was completed in 1949 as the second home to the Golden State Mutual Life Insurance Company, and was designed by architect Paul R. Williams in the Late Moderne style.

Architectural Description

The five-story (plus penthouse) building is an excellent example of the Late Moderne architectural style, which emerged in the immediate post World War II period as a modern adaptation of pre-war architectural styles. Golden

¹⁴ See Los Angeles County Tax Assessor Map in Appendix A.

State Mutual Life is prominently located at the corner of Western Avenue and West Adams Boulevard, and has a strong iconic presence in the neighborhood.

Examples of the Late Moderne style in Los Angeles typically date from 1945 through the early 1950s, and represent a stylistic melding of the pre-war Streamline Moderne and International styles.

Golden State Mutual Life is composed of multiple volumes that give a sculptural quality and a strong visual scheme to the building. There are three main volumes, which consist of a center portion with a distinctly vertical emphasis, flanked by two horizontal wings set at angles from the main volume. The center portion rises one and one-half stories above the wings. The three volumes create a symmetrical composition on the front elevation. The first floor of each of the flanking wings was originally devoted to retail spaces that fronted Western Boulevard and Adams Avenue.

The construction method is concrete encased steel frame with poured-in-place concrete walls. The central volume has three vertical bands of louvered windows running from above the entrance canopy to just below the cornice line of the fifth floor. The windows on the front elevation of the two wings consist of bezeled window bands, which lend to the horizontal

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Golden State Mutual Life Historic Resources Analysis

HISTORIC RESOURCES GROUP

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emphasis of those portions of the building.

The windows on the rear of the central portion are flush with the building surface and maintain the horizontal band configuration, but without the decorative trim. All windows are aluminum sash. While some windows may have been replaced, the fenestration pattern and trim details are original.

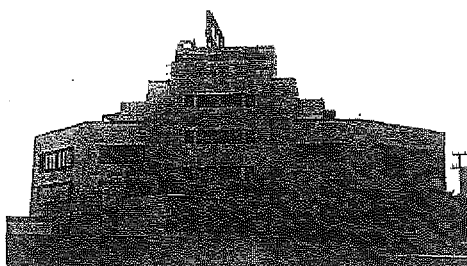


Figure 5: Rear elevation, showing window

The main entry is set at an angle facing the corner of Western Avenue and Adams Boulevard and is clad in stone. There is a double set of aluminum frame doors in the center, and openings on either side that may have originally been glazed. All three openings are surmounted by metal grilles. The building is set back from the street, and there is a prominent painted concrete forecourt with raised planters and small walls along the side to define the space.

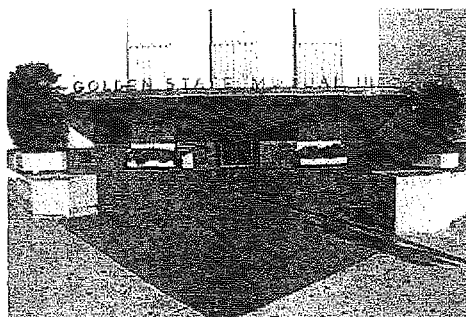


Figure 6: Detail of Front Entrance.

There is a metal cantilevered canopy over the front entrance. Above the canopy are the words Golden State Mutual Life and metal address numbers are above the front door. This signage is consistent with the period in both material and typography.

The exterior of the Golden State Mutual Life building is largely intact. There have been minor changes, including some window replacement, alterations to the front entry (possible replacement of doors; closing window openings), and closing the retail spaces on the first floor, but these do not substantially alter the building's original appearance. Photographs of the existing condition of the exterior of the building are included in Appendix B.

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Golden State Mutual Life Historic Resources Analysis

HISTORIC RESOURCES GROUP

Corporate History

The Golden State Mutual Life Insurance Company was one of the largest African-American owned insurance companies in the United States. It was established by William Nickerson, Jr., Norman O. Houston, and George A. Beavers, prominent Los Angeles African-American entrepreneurs of the 1920s. The Golden State Guarantee Fund Insurance Company was founded in 1925 to address the critical lack of insurance available to African Americans in Los Angeles at the time. The name was changed to Golden State Mutual in 1931.

The company began operation in a second story office at 1435 Central Avenue, which was the center for Los Angeles' African-American community. In 1931 the company built its first permanent home office at 4621 Central Avenue.

By 1931 Golden State Mutual was ranked 19th among the 165 leading insurance companies in the state of California, and it was the largest African-American owned insurance company west of the Mississippi. The company made insurance available and affordable for African Americans, and hired and trained people from the community in the insurance business. The company had a continued tradition of service and support for the African-American community.

In 1949 Golden State Mutual moved into new headquarters at 1999 West Adams Boulevard. Paul R. Williams served as the architect for new headquarters, erected on the corner of Western and Adams Boulevards in 1949.

Architect Paul R. Williams

Paul R. Williams was one of the most successful architects to practice in Los Angeles during the twentieth century. Certified as an architect in 1915, Williams was the only licensed African American architect working on the west coast during the 1920s.¹⁵ Much of his work was residential and he was renowned for designing palatial homes for important figures in the entertainment industry including Lon Chaney, Lucille Ball, and Tyrone Power. He also designed a number of important buildings associated with the African American community including the Second Baptist Church (1926), the Hudson-Liddeli Building (1928; demolished), the Golden State Lodge for the Order of the Elks (1930, demolished), and the Angelus Funeral Home (1934).¹⁶

West Adams History

The Project site is located in the West Adams neighborhood of Los Angeles.

¹⁵ See selected chronology of the life of Paul R. Williams in Appendix C.

¹⁶ Grimes, Teresa. *Historic Resources Associated with African-Americans in Los Angeles*. Multiple Property Submission, 2009.

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Golden State Mutual Life Historic Resources Analysis

HISTORIC RESOURCES GROUP

West Adams is significant within the context of early twentieth century development, and the history of African-Americans in Los Angeles. West Adams encompasses the area bounded roughly by Figueroa Street to the east, Western Boulevard to the west, Pico Boulevard to the north, and Jefferson Boulevard on the south. It was largely developed between 1900 and 1925, and was originally settled by wealthy families who built elaborate homes in popular styles of the period, including Queen Anne, Mission Revival, Craftsman, and Period Revival styles.

In the late 1940s the first African-American family settled in the neighborhood, and the area quickly developed into an enclave for African-American families and businesses.

African American History in West Adams¹⁷

During the 1930s, nearly 25,000 blacks arrived in Los Angeles, hailing mostly from Dallas, Houston, and New Orleans. There were initial clashes with the entrenched middle class of the community, but they soon found common cause with fellow newcomers, and they established their own churches and social networks. By this point, the size of Los Angeles' black community

eclipsed all other cities in the West, reaching 63,744 in 1940, compared to barely 5,000 in San Francisco; 8,500 in Oakland; about 8,000 in Denver; and 4,000 in Seattle. Los Angeles had emerged as the most important center of African American settlement in the West.

In the 1940s, initially spurred by World War II and the burgeoning defense industry, African American migration reached unprecedented levels. Over 140,000 blacks arrived in Los Angeles County in the 1940s alone. Blacks were now a much more visible presence in the city, and constituted a diverse group in terms of class, culture, politics, and religion. With its growing magnitude and complexity, the black community would ultimately redirect the course of Los Angeles history.

A few notable early black settlements also took root in the western part of the city. Among these, the West Jefferson/West Adams section was most significant.

Black settlement in West Adams started in 1923 and 1924, when a batch of race restrictive covenants on white-owned homes expired. The influx of black buyers raised fears among white homeowners, who reacted by tightening race restrictions in the area. These efforts were not completely effective, although they did result in driving the prices up on black housing in the area,

¹⁷ African American history is derived from the *History of African Americans in Los Angeles* Multiple Property Submission.

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Golden State Mutual Life Historic Resources Analysis

HISTORIC RESOURCES GROUP

thus limiting access only to wealthier blacks.

The most exclusive area within West Adams was "Sugar Hill," at the northeast corner. A wealthy neighborhood of spacious Craftsman, Mission Revival, and Italian Renaissance Revival residences from the turn of the century, this sector was originally a haven of affluent whites. In 1938, blacks finally broke the color line there when businessman Norman Houston purchased a home in Sugar Hill. He waited three years to move in, fearing a backlash from his white neighbors. Once he did, other members of the black elite soon followed including film stars Louise Beavers and Hattie McDaniels, J.A. Somerville, businessman Horace Clark, and activist Betty Hill.

In comparison to Central Avenue, the primary historical center of African Americans in Los Angeles, West Adams did not have a clear hub. However, a number of notable businesses and community institutions did move to the area, some of them migrating away from Central Avenue. These included the new Golden State Mutual Life Insurance building and the new First African Methodist Episcopal Church (FAME). The migration of these important institutions drew more black home seekers to the area.

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Golden State Mutual Life Historic Resources Analysis

HISTORIC RESOURCES GROUP

4.2 Application of Criteria

Evaluation for the National Register

Golden State Mutual Life is significant at the local level under National Register Criteria A and C. It is eligible under Criterion A for its association with the history of African-Americans in Los Angeles, and the significance of West Adams in the 1940s as a predominantly black neighborhood. It is eligible under Criterion C as an excellent example of the Late Moderne architectural style in Los Angeles, and as the representative work of master architect Paul R. Williams. The period of significance is 1949, the year that the building was completed.

While there have been minor alterations to Golden State Mutual Life, the building overall retains its ability to convey its historic significance. It retains all seven aspects of integrity -- location, design, materials, workmanship, feeling, and association. The exterior appears largely as it did when construction was completed in 1949.

Evaluation for the California Register

The evaluation of Golden State Mutual Life as eligible for listing in the National Register also qualifies it for listing in the California Register under Criteria 1 and 3: criterion 1 for its association with the history of African-Americans in Los Angeles, and criterion 3 as an excellent example of the Late Moderne architectural style and as the work of master architect Paul R. Williams.

Local Evaluation

Golden State Mutual Life is eligible for designation as a City of Los Angeles Historic-Cultural Monument. It meets three of the local criteria for designation as an HCM, in that it is a building:

- in which the broad cultural, political, economic, or social history of the nation, state, or community is reflected or exemplified; or
- which embody the distinguishing characteristics of an architectural-type specimen, inherently valuable for a study of a period, style, or method of construction; or
- which are a notable work of a master builder, designer, or architect whose individual genius influenced his or her age.

Golden State Mutual Life is significant for its association with African-American history in Los Angeles, and particularly for its role within the historic West Adams area. It is an excellent example of the Late Moderne architectural style, and is the work of master architect Paul R. Williams.

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Golden State Mutual Life Historic Resources Analysis

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4.3 Historic Resources Adjacent to the Project Site

The Project site is located in the West Adams area of Los Angeles. This is an expansive geographic area with a rich history and a large number of historic resources. In addition, West Adams contains a number of designated historic districts (HPOZs) and potential districts.

For the purposes of this report, a study area was identified in immediate proximity to the Project site that contains buildings and neighborhoods that may be directly or indirectly

impacted by the Project. The general area of study is identified in Figure 5; photographs of all buildings in the study area are included in Appendix D. The neighborhood is primarily residential, with some associated commercial and retail development.

These properties were examined for the purposes of identifying potential historic resources adjacent to the Project site. Properties that were previously evaluated as an historic resource or district, meet the fifty-year age threshold, or exhibit characteristics or associations known to be significant have been identified.

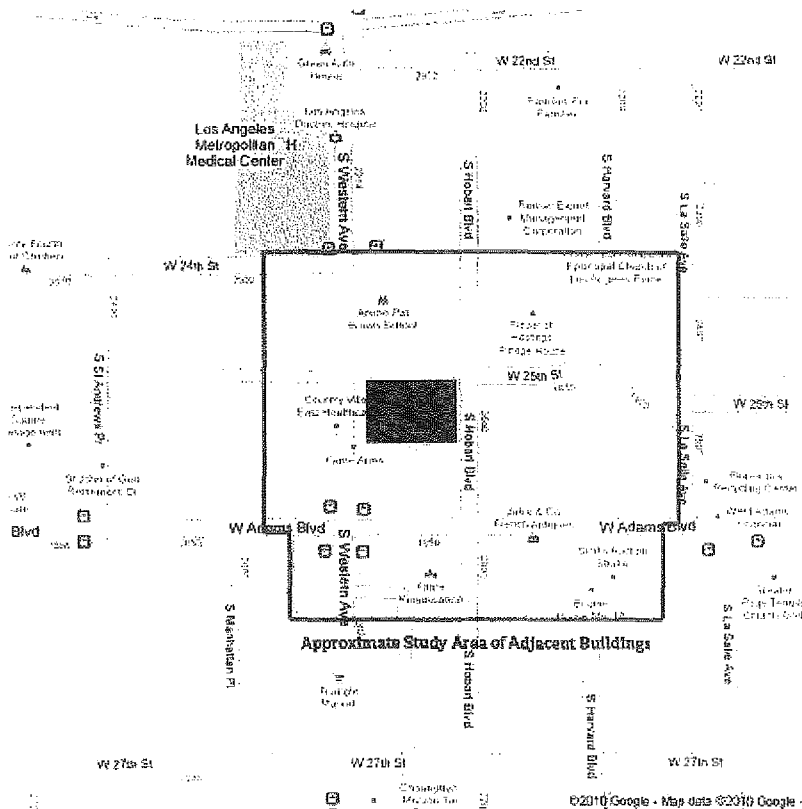


Figure 7: Approximate Study Area of Buildings Adjacent to the Project Site

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Golden State Mutual Life Historic Resources Analysis

HISTORIC RESOURCES GROUP

Designated Adjacent Resources

There are five significant historic buildings within the vicinity of the Project site that have been designated as City of Los Angeles Historic-Cultural Monuments (HCM):

- Engine House #18 at 2616 South Hobart. The Mission Revival building was designed in 1904 by noted local architect John C. Parkinson. It is City of Los Angeles HCM #349, and was listed in the National Register of Historic Places in 1982.
- The Rindge House at 2263 S. Harvard Boulevard. It was designated by noted architect Frederick Louis Roehrig in 1902. It is City of Los Angeles HCM #95, and was listed in the National Register of Historic Places in 1986.
- The Ramsay-Durfee Estate at 2425 S. Western Avenue. This Tudor Revival residence was designed in 1907 by Frederick Louis Roehrig. It is City of Los Angeles HCM #130, and was listed in the National Register of Historic Places in 1989.
- The Thomas W. Phillips residence at 2215 S. Harvard Boulevard. It is City of Los Angeles HCM #551.
- The Wesley W. Beckett House, 2218 S. Harvard Boulevard. It is City of Los Angeles HCM #117.

Potential Historic Resources

In addition to those resources within the vicinity of the Project site that have been designated, other buildings in the study area have been evaluated for their potential historic significance through Federal review processes.

- 2058 West 24th Street has been determined eligible for listing in the National Register of Historic Places. It is a Mission Revival style residence constructed in 1899.

In addition, there are fifteen properties within the study area that were identified during previous surveys, but need to be evaluated for eligibility for listing the National Register of Historic Places. These are indicated by a California status code of "7" in Appendix E.



Figure 8: Engine House #18.

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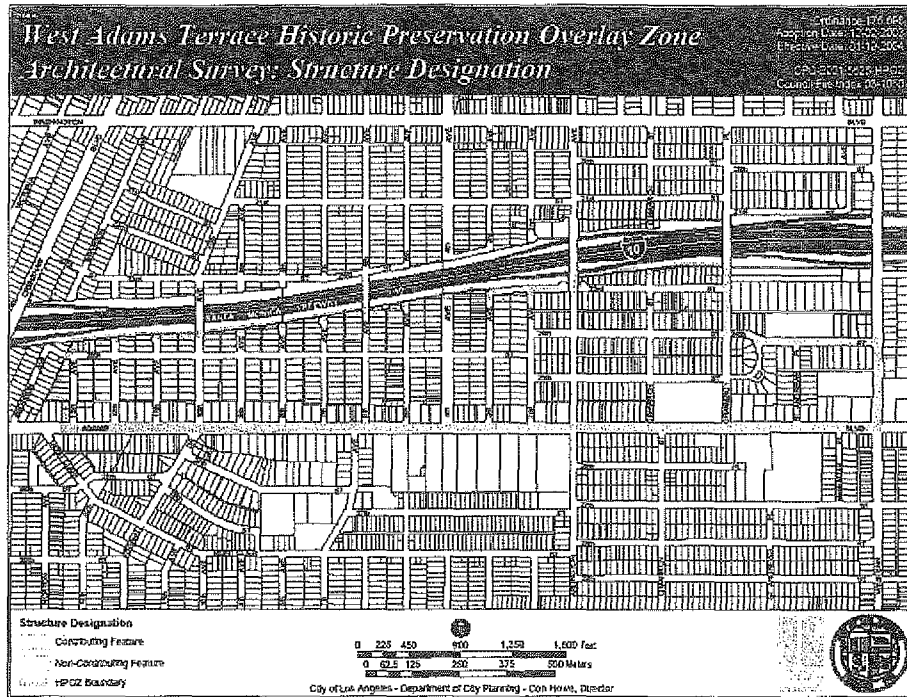


Figure 9: West Adams Terrace HPOZ. Source: City of Los Angeles.

Designated Adjacent Historic Districts (HPOZ)

While the proposed Project is not located within a designated Historic Preservation Overlay Zone (HPOZ), there are multiple HPOZs in the adjacent area, including Adams-Normandie, Harvard Heights, Lafayette Square, Western Heights, University Park, and West Adams Terrace.

West Adams Terrace

Of the West Adams HPOZs, West Adams Terrace is immediately adjacent to the Project site. There is a contributing structure directly across the

street from Project's proposed new construction.

West Adams Terrace includes subdivisions with large and lavish homes originally designed for wealthy middle-class families as well as other tracts developed for the working class. Single family homes in the HPOZ range in size and style from modest Victorian-era cottages, to early 20th century Craftsman and Mission Revival bungalows, to larger Period Revival and Classical styles. Many of the houses were designed by recognized architects and builders including Frank Tyler,

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Hunt and Burns, Frank Meline, Paul R. Williams, and E.L. Petitfils.

Potential Historic Districts

In addition to the designated HPOZs, there is a collection of early twentieth century residential development immediately adjacent to the Project site on the east. This neighborhood is characterized by a combination of modest and more elaborate residences designed by well-known architects of the period. This neighborhood is generally bounded by West 22nd Street to the north, West Adams Boulevard to the South, Western Avenue to the West, and South LaSalle Avenue to the east.¹⁸

¹⁸ This area may be part of a larger potential residential district. The area of study for this report only included the area with the stated boundaries.

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5.0 ANALYSIS OF POTENTIAL IMPACTS

5.1 Significance Threshold

The purpose of the California Environmental Quality Act (CEQA) is to evaluate whether a proposed project may have an adverse effect on the environment and, if so, if that effect can be reduced or eliminated by pursuing an alternative course of action or through mitigation measures.

The City of Los Angeles CEQA Thresholds Guide states that a project would normally have a significant impact on historic resources if it would result in a substantial adverse change in the significance of a historic resource. A substantial adverse change in significance occurs if the project involves:

- Demolition of a significant resource;
- Relocation that does not maintain the integrity and (historical/architectural) significance of a significant resource;
- Conversion, rehabilitation, or alteration of a significant resource which does not conform to the Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings; or
- Construction that reduces the integrity or significance of important resources on the site or in the vicinity.

In addition to guidance provided by the City of Los Angeles, the State Legislature, in enacting the California Register, amended CEQA to clarify which properties are significant, as well as which project impacts are considered to be significantly adverse.

A project with an effect that may cause a substantial adverse change in the significance of a historic resource is a project that may have a significant effect on the environment.¹⁹ A substantial adverse change in the significance of a historic resource means demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of a historical resource would be materially impaired.²⁰

The State CEQA Guidelines also include a slightly different definition of "substantial adverse change":

Substantial adverse change in the significance of an historical resource means physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of an historical resource would be materially impaired.²¹

The Guidelines go on to state that "[t]he significance of an historic resource is

¹⁹ CEQA Guidelines, section 15064.5(b).

²⁰ CEQA Guidelines, section 15064.5(b) (1).

²¹ CEQA Guidelines, section 15064.5(b)(1).

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materially impaired when a project... [d]emolishes or materially alters in an adverse manner those physical characteristics of an historical resource that convey its historical significance and that justify its inclusion in, or eligibility for, inclusion in the California Register of Historical Resources... local register of historic resources... or its identification in a historic resources survey."²²

According to National Register Bulletin 15, to be eligible for listing in the National Register, a property must not only be shown to be significant under National Register criteria, but it must also have integrity. Integrity is defined as the ability of a property to convey its significance.

As such, the test for determining whether or not the project will have a significant impact on identified historic resources is whether it will materially impair physical integrity of the historic resource such that it would no longer be listed in the National or California Registers or other landmark programs such as the City's list of Historic-Cultural Monuments.

The significance thresholds established by the City of Los Angeles include, and go beyond, the provisions set forth in

²² *CEQA Guidelines*, section 15064.5(b)(2).

the CEQA Guidelines and as such, the significance thresholds set forth in the City's CEQA Thresholds Guide are used in the following analysis.

Potential impacts to historic resources are discussed below.

5.2 Impacts from Demolition on the Project Site

Golden State Mutual Life Vehicular Court, Retaining Wall, and Parking Lot Accessory Building.

There is a small vehicular court with some landscaping and a loading dock at the western side of the north (rear) yard of the existing Golden State Mutual Life building. A rear entrance for employees opens to the first floor of the existing building. Pedestrians enter from steps leading down from the property's surface parking lot to the north. The surface parking lot is at a higher grade than the ground level of the existing building. There is a tall retaining wall between the court and the parking lot to accommodate the grade break.

There is one accessory building on the site. The accessory building is a small structure on the south edge of the parking lot, adjacent to but higher than the vehicular court. This small building is part of the original site development and was primarily for attendant use in the parking lot. The building has painted concrete walls and what appears to be a wood-framed roof with a wood fascia and built-up membrane

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roof. The windows are painted steel sash.

The proposed project will remove the existing parking attendant's building, the concrete retaining wall, the driveway, and will alter the grades, paving, and landscaping of the vehicular court.

Standard 2 states that the historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided. The demolition consists of the removal of a very small utilitarian structure and a utilitarian retaining wall, and some paving, stair, and landscape features of the vehicular court. These features are proportionately very small compared to the historic building, are located at the rear of the building, and do not contribute significantly to the architectural character and primary uses of the historic building.

5.3 Impacts from Additions

Retaining Wall and Rear Pedestrian Court

A new retaining wall will be constructed of similar height, length, and location as part of the regarding and construction of the new office building to the north on the site of the parking lot.

New planters and stairways will furnish the exterior rear employee entrance. The vehicular and pedestrian rear court will become pedestrian only. The

features of the historic building at the rear entrance will be retained. The loading dock will continue to function, but access will be from a new driveway from Hobart Avenue that will serve both the existing and new office buildings. The new features maintain an open space and grade break of similar character as the existing features.

Standard 9 requires that new additions, exterior alterations, or related new construction not destroy historic materials, features, and spatial relationships that characterize the property. The new work must be differentiated from the old while being compatible with the historic materials, features, size, scale and proportion, and massing. The new pedestrian rear court does not alter the adjacent existing building. The court's dimensions in plan and retaining walls are similar in scale to the existing rear yard features. The new design is simple, but does not replicate the materials and configuration of the original features.

Standard 10 requires that new additions and adjacent or related new construction be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment will be unimpaired. The new exterior rear court retains the essential form of and relationship between the original space and the historic building. The proposed retaining wall is not attached to the existing building and therefore

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can be removed, leaving the existing building unimpaired.

Pedestrian Bridge

The proposed pedestrian bridge is very simple in design and finish, making it compatible with the simple materials and forms of the existing building. The scale of the bridge in width and height is compatible with the scale of the terraces, walkways, walls, and spandrels of the existing building (see the sections looking west and east at the plaza level in Figure 10). The proposed bridge is not visible from Adams Boulevard when viewing the existing building. The bridge is set back from Western Avenue and Hobart Street such that it can be seen from the side streets when one passes by the existing building, but the setback retains the visual prominence of the existing building. The simplicity and location at the rear of the existing building subordinates the addition to the existing building.

The proposed work on the exterior of the existing Golden State Mutual Building is one opening in the exterior wall on rear (north) elevation for a proposed new pedestrian bridge that connects the main plaza level and pedestrian court of the new building to the historic building at the second floor. The alterations required are cutting out a portion of a low terrace wall to the width of the bridge, and installing new door frame and doors in the existing glazed wall that opens to the raised

exterior terrace. Existing security bars on the windows will be removed.

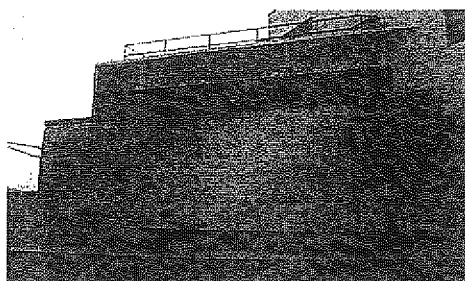


Figure 11: Rear (north) elevation at location of opening for proposed bridge

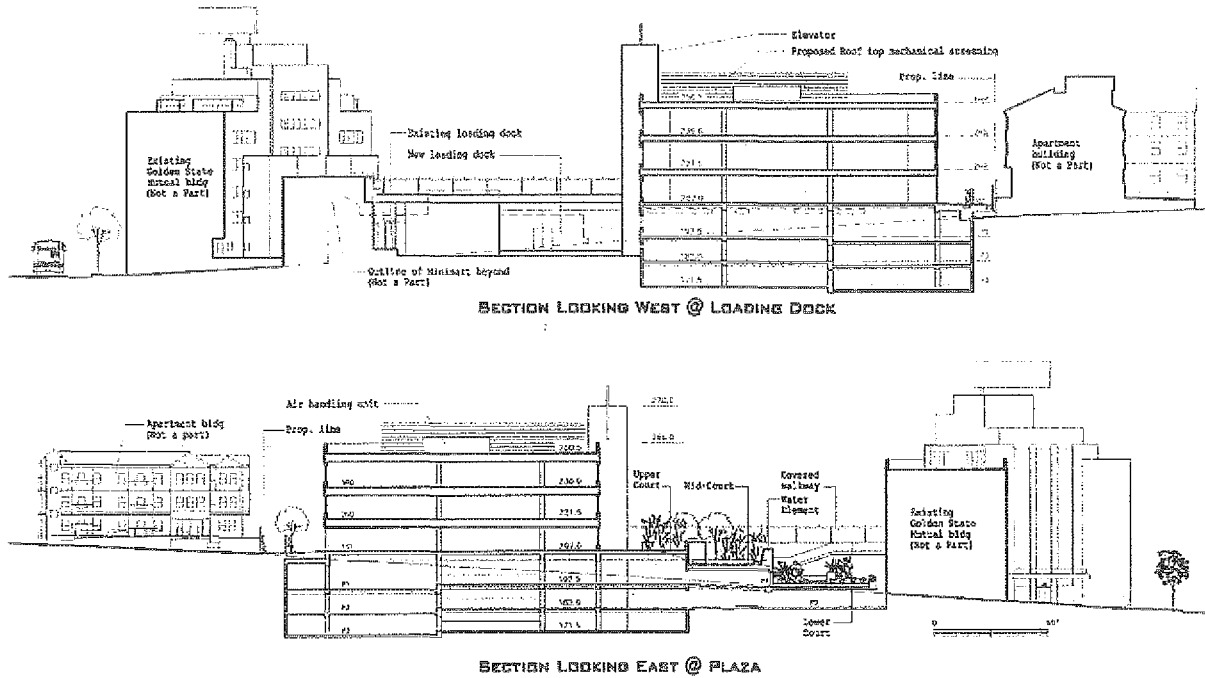
The proposed work on the interior of the existing Golden State Mutual Life Building resulting from the addition of the pedestrian bridge is one opening in the exterior wall on rear (north) that connects the new parking structure to the historic building. The opening is located on a rear wall that faces in a northeast direction due to the geometry of the building plan.

The interior space at the proposed opening is occupied by the employee café. The café space has been altered since original construction. There are no significant interior features, materials, or finishes. The exterior alteration does not change the configuration of any interior space.

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OFFICE BUILDING @ 2500 SOUTH WESTERN AVENUE

E A W International

Figure 10: Sections looking West and East at the Plaza Level.

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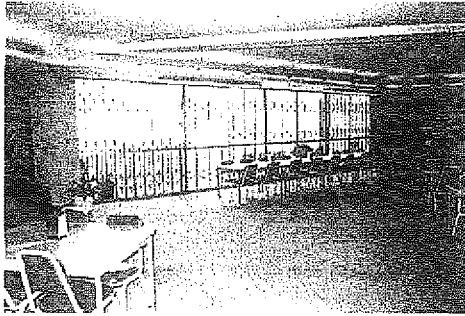


Figure 12: Employee cafe windows at location of opening for proposed bridge

The exterior alteration changes the view from the interior space, since that view will now include the pedestrian bridge. The existing views to the exterior are of a surface parking lot, an apartment building to the north, an apartment building to the east, and street trees along Western Avenue and Hobart Street. The scale and shape of the proposed bridge does not cause a major change in the existing views. However, views will be blocked by adjacent new construction (see section 5.4).

Standard 2 requires that the historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided. The quantity of exterior building fabric that is removed by the proposed alteration is proportionately very small at the low terrace wall and the glazed wall. The proposed alteration

is on the rear face of the building on a secondary façade.

Standard 9 requires that new additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing.

Standard 10 requires that new additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment will be unimpaired. After the proposed addition, the essential form and visual character of the terrace and wall are still visible. While the proposed additions are not 100% "reversible" in that the alterations "subtract" a small quantity of existing material from the historic building, the existing configuration could be restored by reconstructing the missing length of terrace wall and glazed wall to match the adjacent existing features after removing the proposed bridge connection.

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5.4 Impacts from the Adjacent New Building on the Site

A new three story office building is proposed on the existing surface parking lot to the north of the historic building. The proposed building is larger in footprint than the historic building, and taller above adjacent existing finished grades than the existing building (at some parts of the proposed building).

The new building has three stories of office space above three levels of subterranean parking, though the top level of parking "daylights" to form a plaza on the south side of the new building because the existing site and adjacent streets slope down from north to south. The south wall of the new office building is more than 100 feet north of the north wall of the existing historic building. The height of the top of the parapet of the new building is less than 50 feet above adjacent grades and plazas; however, there is a relatively small elevator tower that rises about one story higher. There is a mechanical equipment screen on the roof which is set well back from the exterior parapet wall.

The parapet height of the new building is not as high above adjacent grades as the main wings of the historic building. However, due to the rising grades to the north, the new building's parapets are about one-half story higher than the historic building. The central wing of the historic building is higher than the

main body of the proposed new building. The top of the proposed elevator tower on the new building is not taller than the highest features of the historic building.

The west wall of the proposed new building is set back more than 40 feet from the right-of-way (rear of sidewalk) on Western Avenue. The west wall of the historic building is at the sidewalk line, which means that the new building is set back more than 40 feet from the façade of the existing office building. The east wall of the proposed new building is set back 25 feet from Hobart Avenue.

The new building will not be visible from Adams Boulevard when looking toward the historic building in a due north direction. The new building will be visible if approaching from the east, or traveling east, above the one-story retail strip mall that is on adjacent property east of the historic building.

It appears that the new building will not be visible when standing at the corner of Adams Boulevard and Western Avenue in front of the historic building, looking to the north on Western Avenue. The new building will become visible moving to the north as one passes by the west façade of the historic building. When approaching the historic building from the north on Western Avenue, the existing office building will be visible before the proposed building is seen. The new building will come into

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view on the left as one travels south on Western Avenue.

The historic building facades are designed with fenestration that creates a vertical emphasis in the central portion at the main entrance, and fenestration that creates a horizontal emphasis in the two wings that flank the center. The rear-facing fenestration is characterized by horizontal bands. The proposed building's front façade on Western Avenue is divided into three bays with the entrance doors at the center bay; all fenestration is horizontally proportioned.

The south (side) elevation of the new building faces the rear elevation of the historic building; it is articulated into nine bays and has vertically proportioned fenestration. The east façade on Hobart Boulevard steps down one story in the eastern-most bay to form a terrace and trellis; its fenestration is also vertically proportioned. While the architecture of the facades is not delineated in detail, the new building has a relatively flat wall design (i.e., not "punched" openings and deep shadow lines). In this respect, the new building is similar to the well articulated but shallow profiles on the historic building.

The new building will change the views from the existing building to the north. The existing views look over an asphalt-paved parking lot to some street trees along Western and Hobart Avenues, and a recently constructed multi-family

residential building along the north property line. There are trees visible in adjacent institutional and residential properties to the northeast and northwest. The new building blocks views to the direct north, and partially blocks views to the northeast and northwest along Western and Hobart Avenues.

Standard 9 requires that related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.

The proposed new building does not destroy historic materials and features.

The new building occupies the ground that was originally and is currently used as a surface parking lot, therefore changing a spatial relationship. However, this parking lot does not contribute to the character and significance of the existing building and its setting. The new building is set back far enough from Western Avenue that it is visually subordinated to the historic building. The new building does not alter the existing building's dominance as the principal visual feature at the northeast corner of Adams Boulevard and Western Avenue.

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The new building is different in its façade design than the original building.

The proposed exterior materials of the new building are primarily cement plaster, glazed curtain walls, and metal trim. With the exception of cement plaster in lieu of poured-in-place concrete, the palettes, but not the styles, are similar. The flatness of the proposed facades are compatible with the flatness of the modernist existing building.

The footprint of the proposed new building is much larger than the existing building. However, the location, distance between the buildings, and setbacks (particularly on Western Avenue) reduce the potential for the impact of scale. The new building has fewer stories than the existing building. The new building's parapets are higher than the lower side wings of the existing building due to the change in grade, but are not higher relative to adjacent grades. The location and resulting sight lines reduce the visibility of the new building when viewed from the exterior of the existing building. Therefore, the scale is compatible. The tripartite organization and proportions of the three bays on the front, west façade of the new building recall the massing of the existing building: a central volume with two wings.

5.5 Impacts from Construction

Construction methods have the potential to directly affect the historic materials of the existing building.

Standard 5 of the Secretary of the Interior's Standards for Rehabilitation (hereafter, the "Standards") states that "distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved." If there were damage to such character caused by adjacent new construction then the work would not conform to this Standard, resulting in an adverse impact. In order to meet the Secretary of the Interior's Standards, methods used for the new construction should not damage the historical resource.

The existing retaining wall and small structure are more than forty feet north of the existing building. The demolition process has the potential to cause vibration. The location and scale of the demolition is such that there is no risk of undermining of the buildings walls and footings. The use of large construction equipment has the potential to damage the existing building. Movement or falling of demolished material has the potential to damage the existing building.

Excavation more than eighty feet north of the existing building is required for the subterranean parking below the proposed new office building. This work has the potential to cause vibration, undermining of walls and footings, and increase in seismic risk to the existing building during the period of construction.

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In addition, construction of the foundation and subterranean parking levels has the potential to disturb any archaeological resources that may be present on the site. Any archaeological resources need to be identified during construction, protected and preserved in place, or documented, in accordance with a mitigation program.

The potential for construction methods to damage historic and cultural resources can be avoided by adopting mitigation measures as outlined in this report. Without mitigation, adjacent construction may result in significant adverse impacts.

5.6 Impacts to Adjacent Historic Resources

An analysis of the impacts of the Project on adjacent historic resources includes a review of:

- Location, topography, or other factors that may impact the likelihood of the Project having a physical or visual effect on the adjacent resources and overall character of the neighborhood;
- Visual changes, including changes to the appearance of buildings, structures, sites, or districts;
- Changes in view sheds;
- Compatibility of the scale, massing, and density of the new construction with the existing neighborhood; and
- Compatibility of the design elements of the proposed new

construction with adjacent and nearby historic resources.

Although the Project site is not located in a designated Historic Preservation Overlay Zone (HPOZ), as identified in Section 4.3, it is located adjacent to the West Adams Terrace HPOZ, whose eastern boundary is along Western Avenue between Adams Boulevard and 24th Street. There are both contributors and non-contributors to the HPOZ along Western Avenue. The Ramsay-Durfee Estate, which is both a contributor to the HPOZ and an individually designated Historic-Cultural Monument, is located directly across from the Project site. The Ramsay-Durfee Estate is located between the St. John of God Retirement Home and the Country Villa East Healthcare Center. Its setting will not be further compromised by the Project.

Based on an analysis of the topography, potential changes to the appearance of the block, potential changes in view sheds, and compatibility of the scale and density of the proposed Project, there do not appear to be any significant adverse impacts on the adjacent HPOZ. The proposed Project is compatible with existing commercial and multi-family buildings in the area, and therefore will not detract from the setting of the HPOZ or its eligibility as a designated historic resource.

There are five individually significant Historic Cultural-Monuments (HCMs)

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within the vicinity of the Project site. In addition to the Ramsay-Durfee Estate, the HCM with the most immediate adjacency to the Project site is the Rindge House on Harvard Boulevard. The Rindge House is located to the northeast of the proposed new construction on the Project site. Because of the topography and existing landscaping surrounding the Rindge House, it is not anticipated that the new construction will have a significant impact on the view sheds from the house. In addition, the setting of the Rindge House has been compromised by surrounding later construction. The proposed Project will not further compromise the setting or impact the eligibility of the designated historic resource.

Along Hobart Avenue, to the north of the Project site, is a collection of single-family residences that appear to be eligible for designation as an HPOZ. There is a four-story apartment building between the site of the proposed new construction and the single-family residences which will act as a buffer between the new office building and the single-family residential neighborhood. The proposed new construction will be compatible in size, scale, and massing with the existing apartment building, and therefore there will not be a significant adverse impact on the residential neighborhood once the project is implemented.

5.6 Summary of Impacts

Standard 1: A property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationships.

The existing property will be used as it was historically for offices.

Standard 2: The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided.

The physical character of the existing building is preserved. There are changes to a non-contributing parking lot to the north, insignificant changes to views to the north, and removal of a retaining wall, parking lot gate building, and reconstruction of a vehicular entrance and service yard on the northwest.

Impacts of construction without mitigation could damage existing character.

Standard 3: Each property will be recognized as a physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.

There are no additions to the existing building and no proposed features on the new building that could be

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confused with the original features of the existing building.

Standard 4: Changes to a property that have acquired historic significance in their own right will be retained and preserved.

There are no changes to the property that have acquired historic significance.

Standard 5: Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved.

All extant characteristics of the existing building are being retained.

Standard 6: Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.

No repairs or replacements are proposed.

Standard 7: Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.

No physical treatments are proposed.

Standard 8: Archeological resources will be protected and preserved in

place. If such resources must be disturbed, mitigation measures will be undertaken.

The proposed project will conform to Standard 8 if archaeological resources are protected and preserved in place. If any resources are to be disturbed, mitigation measures must be undertaken.

Standard 9: New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.

The proposed additions of a bridge and pedestrian court at the rear, and adjacent new construction on the site do not have significant impacts on the materials and character of the existing building and its property.

Standard 10: New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment will be unimpaired.

The additions and adjacent new construction can be removed with physical impacts on the existing building.

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Impacts to Adjacent Historic Resources

The proposed new construction will not have a significant impact on the designated or potential historic resources located adjacent to the Project site. The setting and eligibility of these resources will not be compromised after Project implementation, due to the topography, existing infill construction, and the compatible size, scale, and massing of the proposed new building.

Summary

In summary, with the exception of potential impacts during the construction phase, after recommended mitigation there are no potential significant impacts that do not meet the Secretary of the Interior's Standards for Rehabilitation. The Project maintains sufficient visual distance from the Golden State Mutual building so that the integrity of the resource and its ability to convey its historic association are retained.

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Implementation of the mitigation measures listed here will reduce impacts to historic resources in the Project vicinity to a less-than-significant level.

1. The Golden State Mutual Building, site features, and parking lot guard building shall be documented according to HABS Level II standards prior to any demolition, abatement or rehabilitation work. The negatives and archival quality prints should be donated to the Los Angeles Public Library.

HABS Level II documentation shall consist of the following:

- short form architectural and historical narrative;
 - archival drawings;
 - if adequate archival drawings are not available, measured drawings will be produced; and
 - large format photography with views of all exterior elevations for each building, important features, key spatial relationships among buildings, and exterior hardscape features.
2. A structural engineer with qualifications in completed historic preservation projects that conform to the Secretary of the Interior's Standards for Rehabilitation will be consulted and provide monitoring and written review of the engineering and construction of work that is on site and contiguous with historic resources that are to remain. There are potential impacts that are mitigated to a less than significant level by monitoring by a qualified professional. These impacts are demolition of buildings and landscaping, shoring, excavation, new buildings below and above grade near and attached to historic resources on the site, and temporary shoring to mitigate weaknesses of interior removals and additional seismic risk that occur only during the construction phase.
 3. A professional with who has completed historic preservation projects that conform to the Secretary of the Interior's Standards for Rehabilitation and that meets the Secretary of the Interior's Professional Qualifications Standards for Historic Architect (36 CFR 61) will be consulted and provide monitoring and written review of the work that is related to historic preservation.
 4. In the event potentially significant archaeological materials are encountered during Project construction, work shall be stopped immediately or redirected

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until the significance of the find can be evaluated by a qualified archaeologist. The archaeological monitor shall have the authority to halt any activities impacting potentially significant archaeological resources and the monitor must be permitted to adequately evaluate the find in accordance with CEQA criteria. If materials are found to be significant, measures must be taken to preserve such materials in place or relocate the material off site for further study.

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7.0 Summary of Impacts after Mitigation

The proposed Project, with the recommended mitigation measures listed above, will not result in adverse affects to the Golden State Mutual Life Building and other adjacent historic resources such that they will no longer convey their historic significance.

Implementation of the recommended mitigation measures listed above will reduce impacts to historic resources to a less-than-significant level and all adjacent resources will continue to remain eligible for local designation, the California Register of Historical Resources, and/or the National Register of Historic Places.

Note that this Standards compliant summary is based on the current Project description, and may be subject to change as plans develop.

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**DETERMINATION LETTER
CPC-2010-760-GPA-VZC-HD-
SPR
MAILING DATE: 10/03/12**

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