



## DEPARTMENT OF CITY PLANNING RECOMMENDATION REPORT

### City Planning Commission

**Date:** March 26, 2026  
**Time:** After 8:30 a.m.  
**Place:** Van Nuys City Hall,  
Council Chamber  
14410 Sylvan Street, 2nd Floor  
Van Nuys, CA 91401

The meeting's telephonic number and access code number will be provided no later than 72 hours before the meeting on the meeting agenda published at <https://planning.lacity.gov/about/commissions-boards-hearings> and/or by contacting [cpc@lacity.org](mailto:cpc@lacity.org).

**Public Hearing:** March 26, 2026  
**Appeal Status:** Not Appealable  
**Expiration Date:** April 1, 2026

**Multiple Approval:** No

**PROJECT LOCATION:** One World Way (150-275 West Center Way; 6351 West Century Boulevard; 9601-9851 South Coast Guard Road; 7001-7117 West Imperial Highway; 10200-10601 South Lincoln Boulevard; 10285-10415 South Post Way; 9801-10601 South Sepulveda Boulevard; 10300-11300 South Service Road; 6700-7601 West Service Road; 9610 Sky Way; 100-800 World Way; 6900-7450 World Way West)

**PROPOSED PROJECT:** An Amendment to the LAX Sign District for modifications to: allow new digital display signage on the 1961 Airport Traffic Control Tower (ATCT), in compliance with Secretary of Interior Standards; clarify allowable locations for certain off-site signs; clarify permitted sign types; modify Controlled Refresh Rate standards and frequencies for digital displays; remove the maximum permitted sign area provision for the Landside Subarea of 60,542 square feet of signage activated at any one time, while maintaining the maximum permitted sign area for off-site signs at a total of 80,722 square feet within the Landside Subarea and 289,600 square feet within the Airside Subarea; and other updates and technical corrections.

**Case No.:** CPC-2011-1964-SN-AMDT1  
**CEQA No.:** ENV-2011-1965-EIR-ADD1  
**Council No.:** 11 - Park  
**Plan Area:** Los Angeles International Airport - LAX Plan  
**Plan Overlay:** LAX Specific Plan, Coastal Transportation Corridor Specific Plan  
**Certified NC:** Westchester/Playa NC  
**General Plan Land Use:** Airport Landside; Airport Airside  
**Applicant:** Lisette Covarrubias, Los Angeles World Airports (LAWA)  
**Representative:** Lisette Covarrubias, LAWA

**REQUESTED ACTIONS:**

1. Pursuant to CEQA Guidelines Sections 15162 and 15164, in consideration of the whole of the administrative record, that the project was assessed in the previously certified Environmental Impact Report No. ENV-2011-1965-EIR, certified on June 17, 2015, and as supported by the Addendum dated February 2026, no major revisions are required to the EIR and no subsequent EIR is required for approval of the project; and
2. Pursuant to LAMC Section 13.11 of Chapter 1, an Amendment to the LAX Sign District (Ordinance No. 183,737).

**RECOMMENDED ACTIONS:**

1. **Find**, pursuant to CEQA Guidelines Sections 15162 and 15164, based on the independent judgement of the decision-maker, in consideration of the whole of the administrative record, that the project was assessed in the previously certified Environmental Impact Report No. ENV-2011-1965-EIR, certified on June 17, 2015; and as supported by the Addendum dated February 2026, that no major revisions are required to the EIR and no subsequent EIR is required for approval of the project;
2. **Recommend that the City Council approve** an Ordinance for the Amendment of the LAX Sign District (Ordinance No. 183,737); and
3. **Adopt** the attached Findings.

VINCENT P. BERTONI, AICP  
Director of Planning



Milena Zasadzien  
Principal City Planner



Esther Ahn  
City Planner



Michelle Carter  
City Planner

**ADVICE TO PUBLIC:** \*The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Written communications may be mailed to the *Commission Secretariat, Room 272, City Hall, 200 North Spring Street, Los Angeles, CA 90012* (Phone No. 213-978-1300). While all written communications are given to the Commission for consideration, the initial packets are sent to the week prior to the Commission's meeting date. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters, assistive listening devices, or other auxiliary aids and/or other services may be provided upon request. To ensure availability of services, please make your request not later than three working days (72 hours) prior to the meeting by calling the Commission Secretariat at (213) 978-1299.

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<b>Exhibits:</b>	
A – Sign District Map	
B – Draft Sign District Ordinance	
C – Draft Sign District Design Standards and Guidelines	

Link to – Final Environmental Impact Report (ENV-2011-1965-EIR):  
<https://planning.lacity.gov/development-services/eir/los-angeles-international-airport-lax-sign-district-0>

Link to EIR Addendum:  
<https://www.lawa.org/lawa-our-lax/environmental-documents/current-projects>

## PROJECT ANALYSIS

### Project Summary

The proposed ordinance seeks to amend the Los Angeles International Airport (LAX) Sign District (LAX Sign District) to modernize and expand regulations governing off-site signage within Los Angeles International Airport (LAX). The amendment would continue to allow the installation of new off-site signage, including digital displays, supergraphics, wall signs and other sign types, that may display commercial messaging for businesses, services, and events not located at the airport, consistent with practices at major international airports. Building upon the framework established by the LAX Specific Plan (Ordinance 185,164) and the original LAX Sign District adopted by the Los Angeles City Council in 2015, the amended ordinance would maintain overall limits on signage in the Landside and Airside Subareas while providing greater flexibility in how signage is activated and integrated into the airport environment, including the introduction of digital display signage on the façades of the historic 1961 Airport Traffic Control Tower (ATCT).

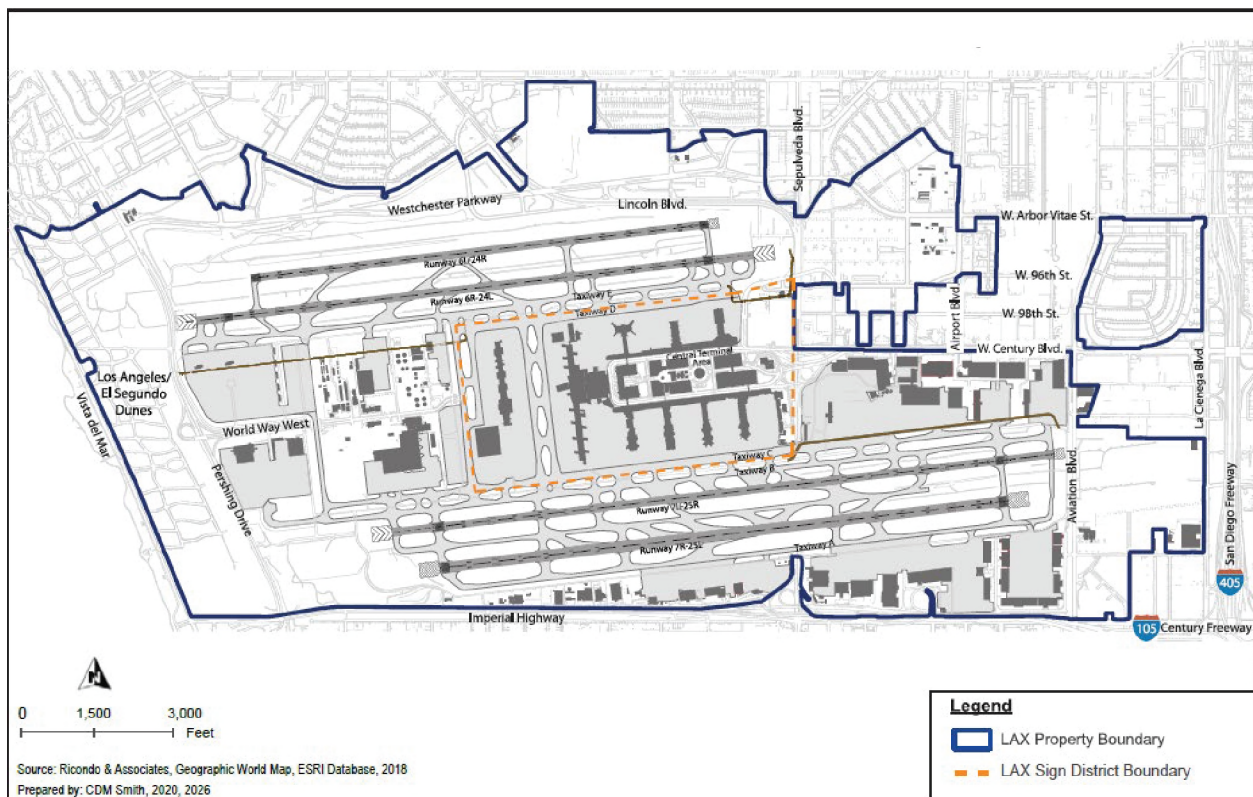
The amendment would establish updated design standards and review procedures, to be administered by Los Angeles World Airports (LAWA), to ensure that signage enhances wayfinding, complements airport architecture, and minimizes potential visual impacts. It would also continue funding commitments to visual blight reduction and gateway improvements in surrounding communities. By supporting a coordinated and high-quality signage program, the proposed amendment would enhance the visual identity and visitor experience at LAX, promote tourism and cultural events in the City of Los Angeles, and support the airport's continued modernization as a premier international gateway in advance of major global events and increasing passenger activity.

### Background and Context

With over 73.71 million annual passengers in 2025, LAX is the eleventh busiest airport in the world, and the primary gateway to the City of Los Angeles and the greater Los Angeles area, making LAX a regional destination that serves as a center of commerce and a gateway to Asia and the Pacific region and international transport. LAX is owned and operated by LAWA, a department of the City of Los Angeles. It is located adjacent to the communities of Westchester, El Segundo, Lennox, and Inglewood, as shown in **Figure 1** below. Planning for LAX is governed by a number of policy documents, including the LAX Plan, a component of the Land Use Element of the City's General Plan, and the LAX Specific Plan (last amended by Ordinance 185,164), which is the implementing ordinance for airport-owned properties in the LAX Plan area. The LAX Specific Plan was adopted in December 2004 and last amended in September 2017 and is implemented by LAWA. The LAX Specific Plan establishes zoning and land use regulations and procedures to ensure consistency with the LAX Plan and to ensure adequate environmental review and documentation of future airport projects. These planning documents designate land within the airport for Airport Airside and Airport Landside uses. The Airside Subarea includes terminal concourses, gates, passenger boarding bridges, airport access ways and equipment which allow for the safe and efficient operation of airport airfield activities. The Landside Subarea

functions as the interface between airside operations and the regional ground transportation network, which includes the arrival and departure roadways associated with the Central Terminal Area of LAX, as well as portions of the terminals facing the interior Central Terminal Area roadway, parking structures, roadway columns, the ground access areas and property generally located between Sepulveda Boulevard and Terminal 1, and the area along Sepulveda Boulevard immediately adjacent to the Central Terminal Area. Powered aircrafts are not permitted in the Landside area. The LAX Specific Plan allows and regulates on-site signage and refers to the accompanying LAX Sign District for the regulation of off-site signage. The Amendment to the ordinance would update the signage regulations to be consistent with LAX modernization efforts, including the repurposing of the former 1961 Air Traffic Control Tower as a placemaking and signage element, as well as include updates for consistency with other City signage regulations and Chapter 1A of the LAMC.

**Figure 1.** Location of Sign District boundary and overall project site.



**Project Goals**

The proposed amendment to the LAX Sign District will continue to support and contribute to the modernization of LAX and allow for a more flexible use of existing signage opportunities at the airport. The project, as amended, will continue to be an integral part of the visual landscape at LAX, taking into account the unique characteristics of this major international gateway. The goal of the amendment to the LAX Sign District is to create an engaging pedestrian and tourist environment, as well as enhance the means of promoting business, cultural, entertainment, and

visitor-serving activities and events in the City of Los Angeles. The proposed amended ordinance is intended to: 1) promote and enhance LAX as an international gateway to the Pacific Rim and as an important public amenity, and maintain an image as one of the nation's premier airports by encouraging creative, well-designed signs that contribute in a positive way to LAX's visual environment; 2) ensure that new off-site signs are responsive to and integrated with the aesthetic character of the structures on which they are located, and are positioned in a manner that is compatible with the surrounding architecture and other signage at the airport; and 3) promote compatibility between off-site signs and adjacent residential communities by encouraging design strategies that minimize visual impacts such as clutter, excessive scale, illumination, and motion or animation. This approach supports the preservation of neighborhood character and enhances the overall visual environment. The proposed amended ordinance is also aimed at coordinating the location and display signs to enhance the public pedestrian realm, minimize potential traffic hazards, protect public safety, and maintain compatibility with surrounding uses.

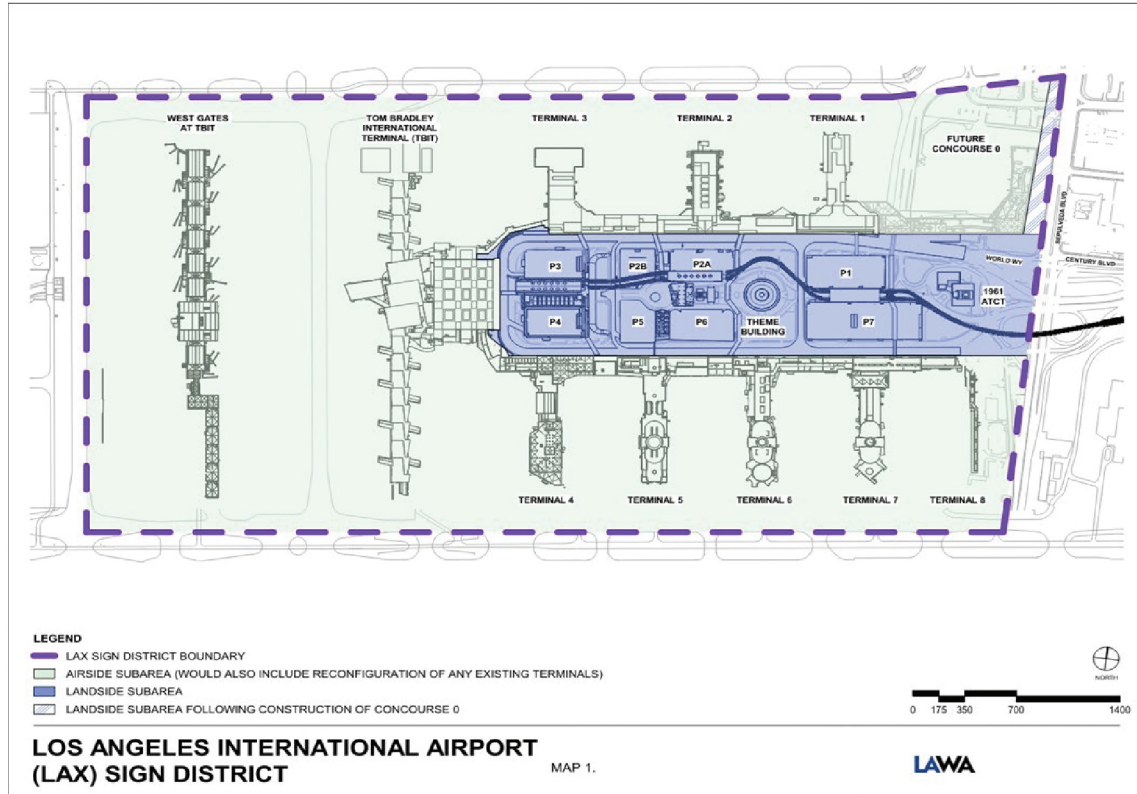
### Project Details

The proposed amendment to LAX Sign District would update and expand regulations governing off-site signs within LAX. The regulations of the amended Sign District would continue to allow for the display of commercial messaging at the airport for businesses, uses, facilities, services, or products not found at LAX (non-airport-related signage), similar to what is found at many other airports across the United States. The regulations are intended to enhance the visual environment and promote visitor-serving activities and events in the City of Los Angeles in a manner that adapts to the evolving needs of the LAX airport.

### *Boundaries and Subareas*

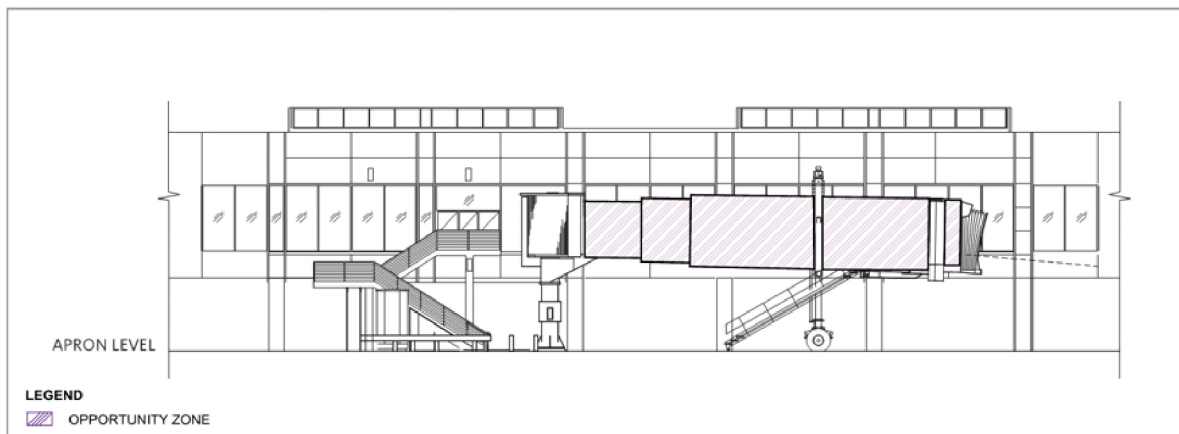
The original LAX Sign District was approved by the City Council in June 2015 and limited new off-site signage to the interior of the LAX campus, within portions of the Landside and Airside Subareas identified in the LAX Specific Plan. The project site encompasses a 469-acre area within the interior portion of LAX and the proposed signage as amended would continue to be limited to the previously approved area (approximately 12.5 percent of the airport's 3,650-acre property), as shown in **Figure 2**.

**Figure 2.** Sign District boundary and subarea map, including the Landside Subarea and Airside Subarea.



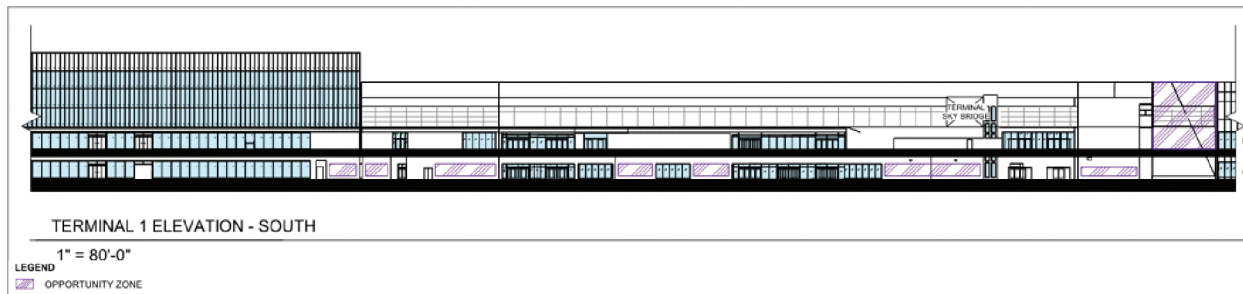
The boundary of the LAX Sign District remains unchanged, which encompasses portions of both the Landside and Airside Subareas, with the Airside portion including approximately 368 acres where signs would be allowed on the exterior of passenger boarding bridges which extend from terminal gates to load and unload passengers from aircraft. Signage within the Airside Subarea would be limited to static supergraphic signs or wall signs on passenger boarding bridges, as shown in **Figure 3** below. Illumination of the signs would be prohibited.

**Figure 3.** Airside Subarea Supergraphic Sign or Wall Sign on a passenger loading bridge.

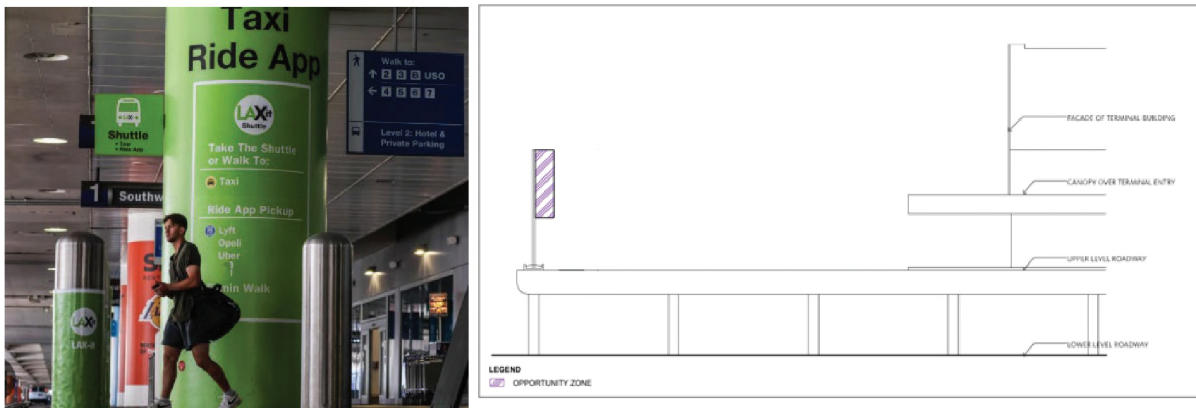


The Landside portion includes approximately 101 acres of the CTA, which is arranged with one-way, U-shaped roadways (both upper and lower), parking structures at the interior, and terminal buildings to the north, west, and south of the roadways. Signs in this area would be located on parking structures, terminal buildings, sky bridges (which connect parking structures and Automated People Mover (APM) stations to terminals), columns supporting the upper-level roadway, and light poles, and would include digital displays, supergraphic signs, wall signs, column wrap signs, and pole signs. Digital displays would be allowed on parking structure facades and sky bridges but would generally not be allowed on terminal facades (with the exception of the east facing façade of Terminal 1, and the south and east facing facades of future Concourse 0). An example of potential supergraphic or wall signage areas on the Terminal 1 building façade is shown below in **Figure 4**. Instead of prescriptive locations for sign locations and size, the proposed ordinance and LAX Design Standards and Guidelines establish opportunity areas on parking structures, terminals, and sky bridges to allow for flexibility in the siting of signs subject to the overall maximum sign area allowed in the landside Subarea. Digital displays would continue to be limited to a luminance level of 300 candelas per square meter ( $\text{cd}/\text{m}^2$ ) during nighttime hours and would allow a luminance level of 6,000  $\text{cd}/\text{m}^2$  during daytime hours and would have no limit on hours of operation. Animation and moving images would be prohibited, and digital display images would have a coordinated refresh rate of either once every 8 seconds, 16 seconds, or one hour, depending on the sign location.

**Figure 4.** Landside Subarea - Digital Display, Supergraphic Sign, or Wall Sign opportunities



In addition, column wrap signs and pole signs, as shown in **Figure 5**, would be coordinated and spaced accordingly along the roadways, with prioritization for wayfinding signage. These would not include any digital elements.

**Figure 5.** Landside Subarea – Column Wrap Sign and Pole Sign examples

### *Size Limitations*

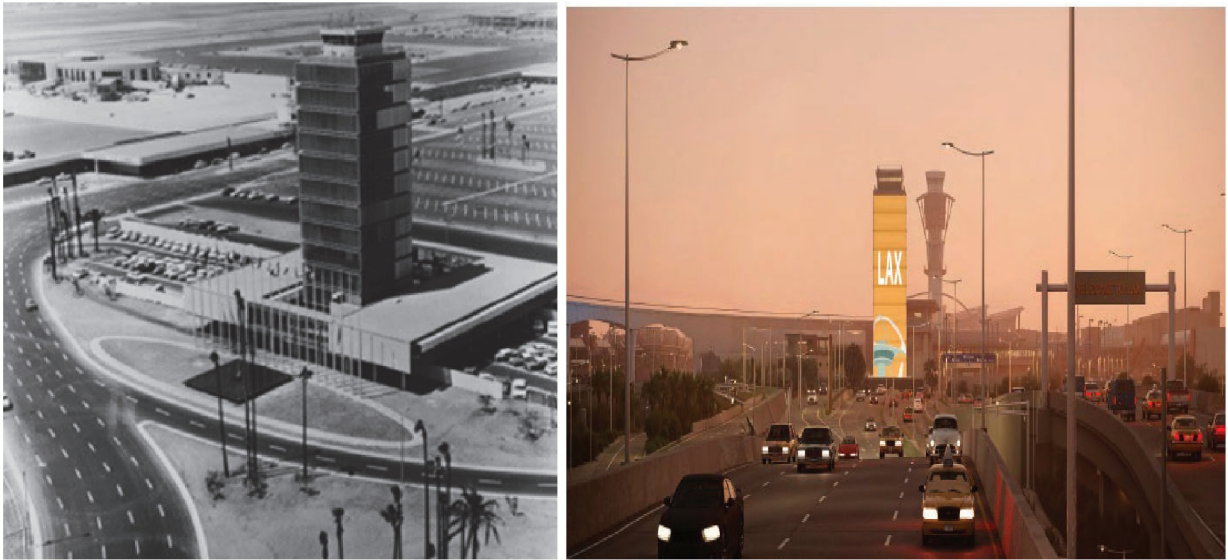
As proposed, the amended LAX Sign District would continue to permit a maximum of 80,722 square feet of proposed new off-site signage within the Landside Subarea and a maximum of approximately 289,600 square feet of proposed new off-site signage within the Airside Subarea, but would remove the maximum permitted sign area provision that limited the amount of signage permitted to be activated at any one time within the Landside Subarea to no more than a 75 percent maximum, or 60,542 square feet. Instead, the total maximum of 80,722 square feet could be activated at any one time. This total would include the area of signage on the 1961 Airport Traffic Control Tower.

### *New Digital Displays on 1961 Airport Traffic Control Tower*

The proposed amended LAX Sign District Ordinance would also allow new digital display signage on the facade of the 1961 Airport Traffic Control Tower (ATCT) to promote welcome messages and imagery, LAX-related travel and tourism messaging, artistic displays, as well as content celebrating commemorations, City-hosted events, and special occasions, as shown in **Figure 6** below. The digital display would be subject to the same luminance standards and would maintain a consistent image with a refresh rate of the image of not more than once every 16 seconds. Consistent with the LAX Preservation Plan, the proposed signage on the 1961 ATCT would respect the existing historic fabric and preservation measures proposed to maintain the tower's integrity while accommodating contemporary airport needs. These measures include removing non-contributing elements while preserving original structural features, installing a visually permeable LED display system mounted in floor-by-floor horizontal bands that recall the tower's original architectural profile, incorporating interpretive digital content about the tower's history, coordinating design and installation with preservation professionals in accordance with the Secretary of the Interior's Standards for Rehabilitation, and preparing a Historic Structure Report to document and guide the long-term treatment and maintenance of the historic resource. The amount of new signage permitted on the facade of the ATCT, which is located within the Landside

Subarea, would total up to approximately 25,725 square feet. This ATCT signage would count toward the maximum permitted signage in the Landside Subarea.

**Figure 6.** Historic 1961 ATCT Tower and proposed Digital Displays



### *Design Standards and Guidelines*

In general, the ordinance permits several types of signage, including supergraphics, wall signs, digital display signs, pole signs, and column wrap signs. The amended Sign District also includes controls on the number of signs, sign dimensions, sign placement, sign illumination, and use of digital technology through the establishment of the new LAX Sign District Design Standards and Guidelines. The LAX Sign District Design Standards and Guidelines are subject to the approval and modification by the Executive Director of LAWA and serve to implement parameters for a sign program that responds to the complex and fluid nature of the airport environment by: 1) prioritizing wayfinding signage, 2) structuring the approach to size and placement of off-site signs to minimize distractions to drivers and pedestrians, and 3) providing signage opportunities within a visual field of a consistent size and type. The amended LAX Sign District, together with the newly established LAX Sign District Design Standards and Guidelines, serve to clarify the permitted sign types allowed, clarify allowable locations for certain off-site signage within the CTA, modify digital signage Controlled Refresh Rate standards and frequencies, and generally align the ordinance with methodologies of recently adopted Sign Districts throughout the City of Los Angeles and with the evolving needs of the airport and its users.

### *Procedures and LAWA Review*

The amended ordinance includes the existing Los Angeles Department of Building and Safety (LADBS) sign review process and revises the LAWA Executive Director Sign-Off review procedure, which applies to off-site signs within the Sign District that fall into the following sign

types: Column Wrap Signs, Digital Displays, Pole Signs, Supergraphic Signs, and Wall Signs. These ministerial review procedures ensure that new signs within the Sign District are well-designed and integrated into the architecture of supporting structures by complying with the LAX Sign District Ordinance, LAX Sign District Design Standards and Guidelines, the Los Angeles Municipal Code (LAMC), and State and federal laws, as applicable. Any request for an adjustment, exception, modification, or interpretation of these regulations, including amendments to the LAX Sign District Design Standards and Guidelines, would continue to follow a discretionary process, subject to the approval by the Executive Director and/or Board of Airport Commissioners for Los Angeles World Airports (BOAC). These review processes would be fully administered by LAWA Staff.

### *Benefits Programs*

The proposed amended ordinance also continues the existing Sign Reduction Program and a Visual Blight Reduction Program to reduce the amount of non-conforming signage and to improve aesthetics of the surrounding area. As part of the amended LAX Sign District Ordinance, the Sign Reduction Program would continue a requirement to remove legally permitted off-site signage outside of the CTA, funded through a profit-sharing agreement. The Visual Blight Reduction Program would continue to require a contribution to be deposited into a LAWA Visual Blight Reduction (VBR) Trust Fund which will be used to improve visual aesthetics in gateway corridors at, or adjacent to, LAX with high percentages of airport visitor traffic, to offset the ongoing effects of new off-site signage at the airport. Therefore, the proposed amended ordinance maintains LAWA's previous commitments to sign reduction and financial contributions for the establishment and implementation of a Visual Blight Reduction program.

The proposed amendment to the LAX Sign District, as recommended by Planning Staff, would meet several City goals including contributing to the ongoing modernization of LAX and providing improvements to the visual environment of the airport and surrounding communities.

### **Recommendation**

In 2025, LAX was the world's eleventh busiest passenger airport, moving approximately 73.71 million annual passengers. As a world-class airport and international gateway for local and visiting travelers, signage is a vital component of the local, regional, and state economy and contributes to an image of quality and excellence for the City and promotes Los Angeles as a destination of regional importance. Signage is a common and important feature at world-class airports and plays a role in defining the image of the airport by enhancing travelers' visual experience by fostering a dynamic and engaging pedestrian, tourist, and work environment, and by promoting business, cultural, entertainment, and visitor-serving activities and events in the City of Los Angeles.

In anticipation of hosting the 2026 Soccer World Cup and 2028 Olympic and Paralympic Games, the City has invested billions into modernizing LAX, including, but not limited to, the renovation of terminals and construction of an APM to transport travelers to and from the LAX facilities and the regional transportation system. An amendment to the LAX Sign District would support the LAX

Plan and LAX Specific Plan and is necessary to update the signage regulations to be consistent with LAX modernization efforts, including the repurposing of the former 1961 Air Traffic Control Tower as a placemaking and signage element, as well as include updates for consistency with other City signage regulations and Chapter 1A of the LAMC. Planning Staff recommends approval of the amended LAX Sign District.

## FINDINGS

### A. ENTITLEMENT FINDINGS

#### SIGN DISTRICT FINDINGS

1. **The Sign District Amendment substantially conforms with the purposes, intent and provisions of the General Plan.**

The City-initiated Sign District Amendment is in substantial conformance with the purposes, intent, and provisions of the General Plan, as explained below:

The proposal is consistent with and implements policies contained in portions of the General Plan, including the General Plan Framework and the LAX Plan. Specifically, the General Plan Framework states the following:

*Economic Development Chapter, Introduction.*

*"...the City must take advantage of the critical role of the Port of Los Angeles and the Los Angeles International Airport in supporting the local economy. "*

*Economic Development Policy 7.2.13.*

*Facilitate environmentally sound operations and expansion of the Port of Los Angeles and the Los Angeles International Airport as major drivers of the local and regional economy.*

*Economic Development Policy 7.3.4.*

*Recognize the crucial role that the Port of Los Angeles and the Los Angeles International Airport play in future employment growth by supporting planned Port and Airport expansion and modernization that mitigates its negative impacts.*

These policies recognize the crucial role that LAX plays as a major driver of the local and regional economy. Signage is a common feature at airports that plays a role in defining the image of the airport, and the proposed amended LAX Sign District is a part of the overall modernization effort to provide an improved image at LAX with state-of-the-art facilities for travelers. The proposed amended ordinance ensures that signage will be well designed and integrated into the architecture in a way that enhances the visual environment. In addition, the amended LAX Sign District is intended to allow for the promotion of the unique character of Los Angeles through positive imagery, illustrations, and sponsorships of familiar local attractions, industries, and landmarks.

Further, the LAX Plan states:

*Goal 3: Optimize LAX's critical role in supporting the economy as a major generator of economic activity.*

*Policy 3.5, Program P2. Modernize, upgrade, and improve LAX in order to sustain the airport's economic benefits.*

Consistent with the economic policies cited from the Framework Element, the LAX Plan also calls for airport improvements that will have beneficial economic impacts beyond the airport itself. As mentioned above, the proposed amended LAX Sign District will improve the image of the airport, facilitate modernization projects, and allow for the promotion of key components of the Los Angeles economy to visitors. In addition, the sign removal and aesthetic enhancement components of the amended ordinance will continue to remove visual blight in the surrounding communities and support beautification efforts that support increased economic activity.

*Policy 3.9, Program P4 and Policy P4, Program 3.9. Develop and incorporate signage guidelines that provide guidance and establish controls for signage that are appropriate to an airport.*

The LAX Plan calls for the development of sign guidelines and controls that are consistent with the property's use as a major international airport and primary point of entry to the City of Los Angeles. The proposed amended LAX Sign District ordinance, along with the LAX Sign District Design Standards and Guidelines, is consistent with and implements this program of the LAX Plan by specifying controls on the number of signs, sign type, sign dimensions, sign placement, sign illumination, and the use of digital technology. In addition, the proposed amended ordinance includes review processes that provide LAWA with the authority and flexibility to ensure that new off-site signs are consistent with a uniform standard of quality, similar to the existing LAX Airport Tenant Signage Standards for on-site signs. The signage review procedures and processes contained in the proposed amended ordinance would be administered by LAWA directly, including the establishment and development of the LAX Sign District Design Standards and Guidelines which would complement the regulations set forth by the amended LAX Sign District ordinance.

*Goal 5: Acknowledge neighborhood context and promote compatibility between LAX and the surrounding neighborhoods.*

*Policy P1, Program 3.2.2. Ensure that the scale and activity level of airport facilities appropriately relates to any abutting neighborhood edges.*

*Policy P6, Program 3.2.2. Locate airport uses and activities with the potential to adversely affect nearby land uses through noise, light spill-over, odor, vibration, and other consequences of airport operations and development as far from, or oriented away from adjacent residential neighborhoods as feasible.*

*Policy P1, Program 3.9. Appropriately relate those airport facilities that are adjacent to community land uses to the scale and level of activity of those uses.*

Consistent with the above-referenced policies and programs, the proposed amended ordinance includes features to ensure that the airport is as compatible as possible with surrounding properties and neighborhoods. The proposed amended LAX Sign District ordinance includes standards which prohibit any electronic or light enhanced signage to be installed within the Airside Subarea and prohibit any off-site signs on a number of buildings within the project site, including the Theme Building, the current Airport Traffic Control Tower, and the Clifton A. Moore Administration Building (except the former 1961 Airport Traffic Control Tower). The proposed amended ordinance includes standards related to sign illumination (either internal or external), reflective materials, digital display illumination, and refresh rates, which regulate the use and operation of digital displays to ensure there is minimal potential for light- and glare-related impacts onto surrounding properties. As specified in the standards, the illuminance contribution of all proposed signage would be limited to 0.3 footcandle (fc) at the property line of residentially zoned properties.

*Policy P2, Program 3.9.*

*Relate Airport Landside facilities to the existing airport infrastructure in a clear, well-organized, functional, and compatible manner.*

The proposed amended Sign District is a part of the overall modernization effort underway at LAX to improve the visual environment by unifying disparate components in the Central Terminal Area through artful integration of lighting, graphics, and architecture. Signage is required to be well-designed and integrated into the architecture in a way that draws upon and is complementary to key existing airport elements as well as future planned improvements. The proposed project also accomplishes this policy goal through the establishment of the LAX Sign District Design Standards and Guidelines, which are intended to impose parameters for a sign program that respond to the complex and fluid nature of the airport environment by: prioritizing wayfinding signage; structure the approach to size and placement of off-site signs to minimize distractions to drivers and pedestrians; and providing signage opportunities within a visual field of a consistent size and type .

**2. The proposed Sign District Ordinance is in conformity with public necessity, convenience, general welfare, and good zoning practice.**

The amended ordinance is in conformity with the public necessity, convenience, general welfare, and good zoning practice. The proposed amended LAX Sign District would create unique signage regulations for the Central Terminal Area of LAX and connecting passenger boarding bridges. The provision of specially tailored dynamic signage regulations would advance the LAX Plan's goals for revitalization of the area by creating an engaging visual environment for travelers and reducing visual blight in surrounding communities. The amended LAX Sign District would enhance the means of promoting business, cultural, entertainment, and visitor-serving activities and events in the City of Los Angeles. The amended LAX Sign District would create a uniform aesthetic appearance in the messaging, theming, and branding occurring throughout LAX, and help contribute to Los Angeles' image as a world-class entertainment and tourist destination. As such,

the amended LAX Sign District conforms to the public necessity, convenience, and general welfare of the city.

The signage regulations reflect good zoning practice in that they include controls on the type, size, height, hours of operation, illumination level, and other physical characteristics of new, off-site signs at the airport. Appropriate and balanced sign regulations are necessary to maintain compatibility and sensitivity to surrounding uses. The amended LAX Sign District would establish regulations that minimize potential traffic hazards and protect public safety, including minimizing any potential traffic hazards to the surrounding roadways. Such standards include a reduction of existing off-site signs in surrounding communities, no increase in off-site signage along surrounding public streets, including Sepulveda Boulevard, except for signage associated with future terminals, and regulation of refresh rates on digital display signs.

The proposed amendment to the LAX Sign District also conforms to good zoning practice in that it clarifies and establishes certain signage design criteria, standards, location, and types of permitted and prohibited signs. The amended LAX Sign District would not create visual impacts on the surrounding communities, and no signs would be placed on the following significant architectural features or buildings within LAX, including the Theme Building, the current Airport Traffic Control Tower, and the Clifton A. Moore Administration Building (except the former 1961 Airport Traffic Control Tower).

The amended LAX Sign District allows for the erection, installation, or construction of new off-site signs or the alteration, redesign, or replacement of existing off-site signs within the Airside and Landside Subareas, pursuant to the amendment of a Sign District as set forth in LAMC Section 13.11 of Chapter 1 and Section 13B.1.4 of Chapter 1A. While the LAX Specific Plan supersedes the sign regulations contained in the LAMC, the process of permitting off-site signs through the supplemental use district application process is specifically provided for by the LAX Sign District and represents good zoning practices consistent with the City's standardized process for review of off-site sign requests.

Further, the proposed amended ordinance is in support of the General Plan Framework and LAX Plan as discussed above.

## **B. CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) FINDINGS**

### **Background**

Pursuant to the California Environmental Quality Act, Public Resources Code Sections 21000 et seq. (CEQA) and the State CEQA Guidelines, Title 14, California Code of Regulations, Sections 15000 et seq. (CEQA Guidelines), on June 17, 2015, the City of Los Angeles (City), acting as lead agency, certified an Environmental Impact Report (EIR) for the LAX Sign District Project (Case No. ENV-2011-1965-EIR, State Clearinghouse No. 2012031055) (Certified EIR). The Certified EIR analyzed the development and implementation of new off-site signage within LAX. The City found that the Certified EIR was completed in compliance with CEQA and State

and City CEQA Guidelines, and adopted findings. The Certified EIR concluded that all of the LAX Sign District Project's environmental impacts would be less than significant.

Subsequently, an Addendum, dated February 2026, was prepared to document and analyze the environmental impacts from revisions to the project that have been made (Modified Project).

Revisions under the Modified Project include:

- To allow new digital display signage on the 1961 Airport Traffic Control Tower (ATCT), in compliance with Secretary of Interior Standards;
- Clarify allowable locations for certain off-site signs;
- Clarify permitted sign types;
- Modify Controlled Refresh Rate standards and frequencies for digital displays;
- Remove the maximum permitted sign area provision for the Landside Subarea of 60,542 square feet of signage activated at any one time, while maintaining the maximum permitted sign area for off-site signs at a total of 80,722 square feet within the Landside Subarea and 289,600 square feet within the Airside Subarea; and
- Other updates and technical corrections.

#### CEQA Authority for an Addendum

CEQA establishes the type of environmental documentation required when changes to a project occur after an EIR is certified. Specifically, Section 15164(a) of the CEQA Guidelines states that: *"The lead agency or responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred."*

Sections 15162 and 15163 of the CEQA Guidelines respectively require preparation of a Subsequent or Supplemental EIR when an EIR has been certified and one or more of the following circumstances exist:

1. Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
2. Substantial changes occur with respect to the circumstances under which the project is undertaken, which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
3. New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
  - a. The project will have one or more significant effects not discussed in the previous EIR or negative declaration;

- b. Significant effects previously examined will be substantially more severe than shown in the previous EIR;
- c. Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
- d. Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Likewise, California Public Resources Code Section 21166 states that no subsequent or supplemental environmental impact report shall be required by the lead agency or by any responsible agency, unless one or more of the following events occur:

1. Substantial changes are proposed in the project which will require major revisions of the environmental impact report;
2. Substantial changes occur with respect to the circumstances under which the project is being undertaken which will require major revisions in the environmental impact report; or
3. New information, which was not known and could not have been known at the time the environmental impact report was certified as complete, becomes available.

As demonstrated in the Addendum, the Modified Project would not result in any new significant impacts, nor would it substantially increase the severity of any previously anticipated significant impacts set forth in the Certified EIR. No substantial changes would occur with respect to the circumstances under which the Modified Project is undertaken that require major revisions of the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.

Additionally, the Addendum supplements the information necessary for evaluation of the Modified Project and does not present new information of substantial importance which would create one or more significant effects not previously disclosed or increase the severity of the significant events already evaluated in the Certified EIR. In addition, all the applicable mitigation measures included as part of the Certified EIR, as modified in the Addendum, would continue to be implemented under the Modified Project as part of the Sign District Ordinance. Additionally, there are no known mitigation measures or alternatives that were previously considered infeasible but are now considered feasible that would substantially reduce one or more significant effects on the environment identified in the Certified EIR.

Based on this determination, the Modified Project does not meet the criteria for preparation of a Supplemental or Subsequent EIR pursuant to Sections 15162 and 15163 of the CEQA Guidelines.

### CEQA Findings

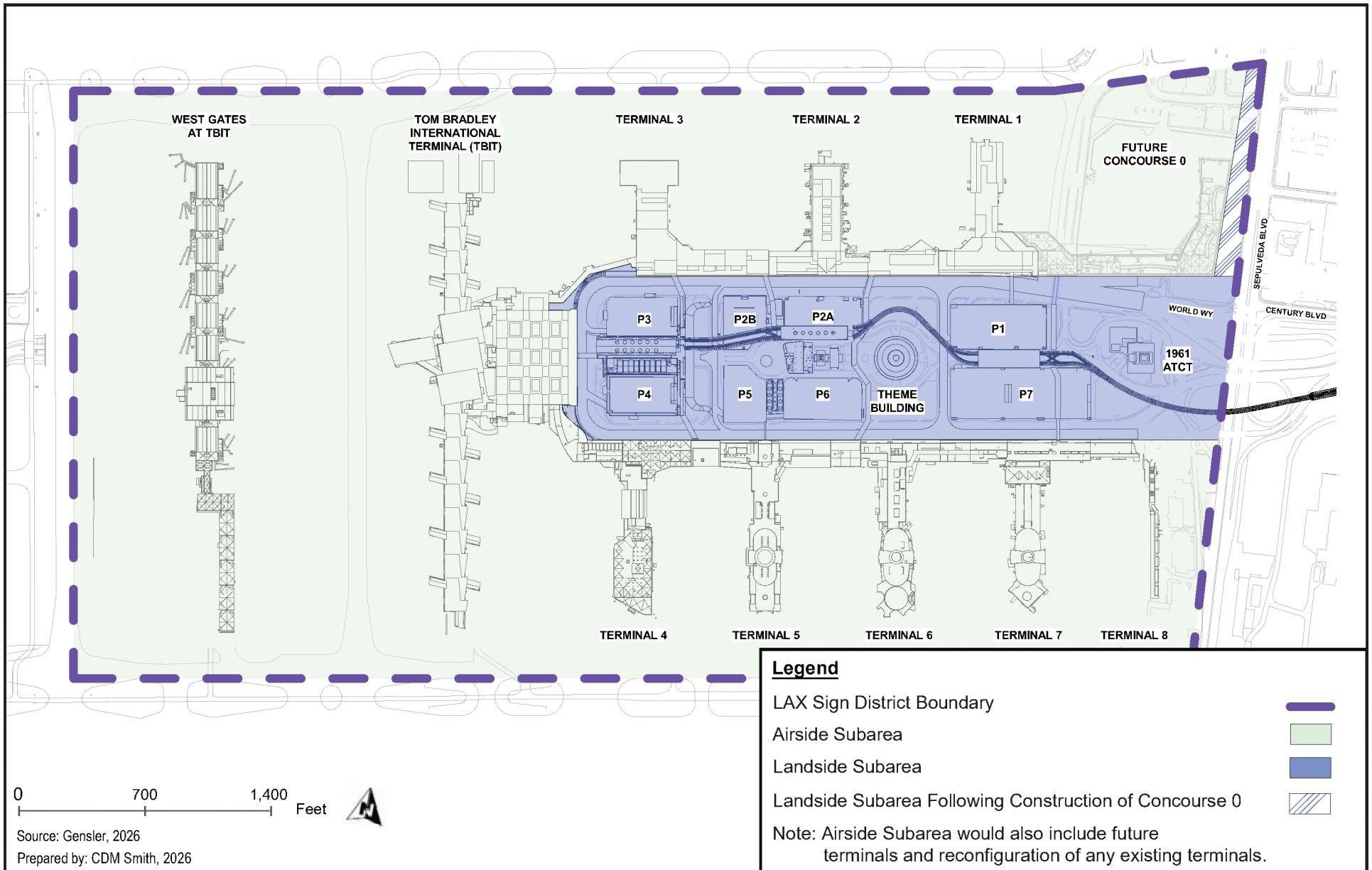
Pursuant to CEQA, the City serves as the lead agency with respect to the Modified Project in connection with the subject City actions. Accordingly, the City (a) has considered the Certified EIR, Addenda, and other pertinent evidence in the record, including studies, reports, and other information from qualified experts (collectively the Environmental Documents) and the environmental effects of the Modified Project as set forth in the Environmental Documents, and (b) makes the following findings:

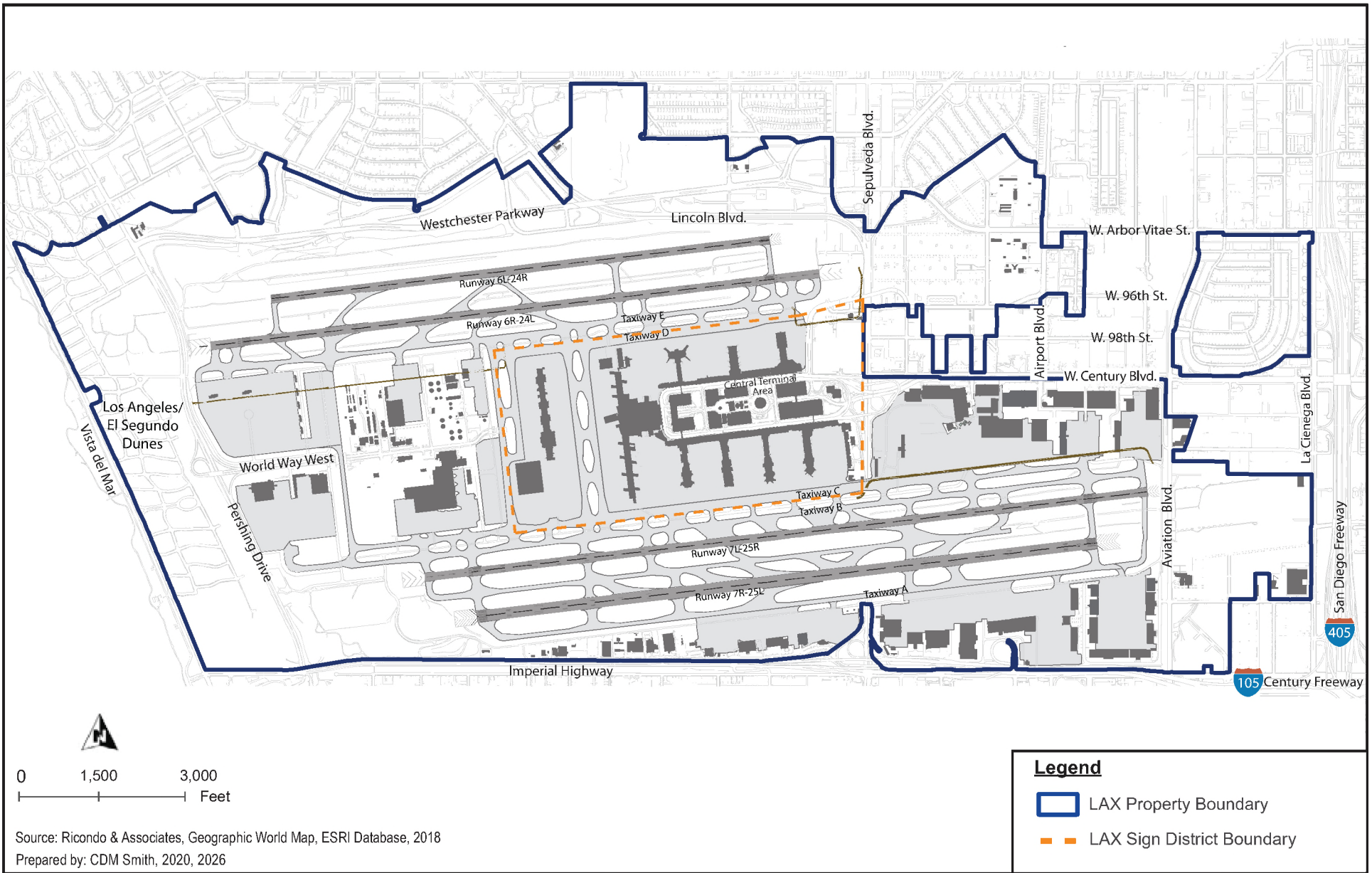
Based on the independent judgment of the decision-maker, after consideration of the whole of the administrative record, the project was assessed in the previously Certified EIR, certified on June 17, 2015; and pursuant to CEQA Guidelines 15162 and 15164, and the Addendum dated February 2026, that no major revisions are required to the EIR and no subsequent EIR is required for approval of the project.

### Record of Proceedings

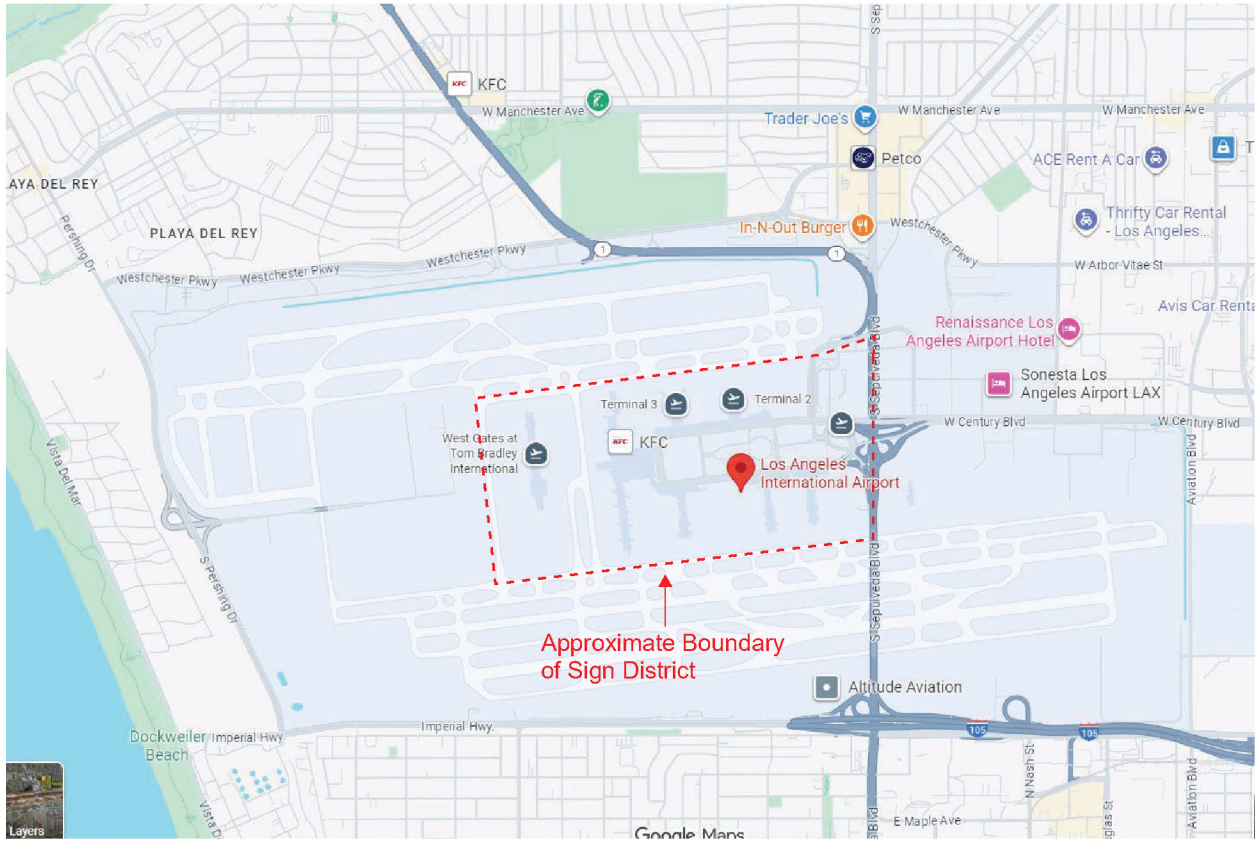
The record of proceedings for the decision includes the Record of Proceedings for the CEQA Findings for the Certified EIR, all items in the Addendum files, and all written and oral information submitted at the hearing on this matter. The documents and other materials that constitute the record of proceedings on which the City of Los Angeles' CEQA Findings are based are located at the Department of City Planning, 221 N. Figueroa Street, Suite 1350, Los Angeles, CA 90021, and any other relevant City department.

**EXHIBIT A**  
**Sign District Map**





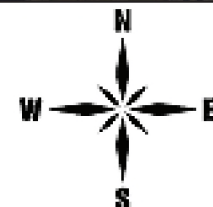
# VICINITY MAP





Address: 6601 W 96TH ST  
 APN: 4117034900  
 PIN #: 093B165 75

Tract: RANCHO SAUSAL REDONDO    Zoning: LAX  
 Block: None    General Plan: Airport Airside  
 Lot: PT LT 38  
 Arb: 93



**EXHIBIT B**  
**Draft Ordinance**

**ORDINANCE NO.**

**LAX SIGN DISTRICT**

- Section 1 - Establishment and Amendment of the Los Angeles International Airport Sign District
- Section 2 - Purpose
- Section 3 - Application of Supplemental Use District Regulations
- Section 4 - Definitions
- Section 5 - Existing Signs. Rights of Owners to Signs.
- Section 6 - Procedural Requirements
- Section 7 - General Requirements
- Section 8 - Standards for Specific Types of Signs
- Section 9 - Sign Reduction, Visual Blight Reduction, and Community Messaging Program
- Section 10 - Severability
- Section 11 - City Clerk Certification and Publication

An ordinance amending and restating the Los Angeles International Airport (LAX) Sign District (Ordinance No. 183,737) pursuant to the provisions of Section 13.11 of Chapter 1 of the Los Angeles Municipal Code (Code).

**WHEREAS**, the General Plan Framework identifies LAX as a Regional Center and major transportation hub, which plays a critical goal in the regional economy.

**WHEREAS**, the General Plan identifies LAX as a site adjacent to the communities of Westchester/Playa del Rey, El Segundo, Lennox and Inglewood, and is intended to promote an arrangement of airport uses that encourages and contributes to the modernization of the airport in an orderly and flexible manner within the context of the City and region;

**WHEREAS**, the Project Site is designated for Airport Landside and Airport Airside land uses and is located within the Los Angeles International Airport Community Plan (LAX Plan) area, a component of the Land Use Element of the City's General Plan, which envisions the evolution of LAX into a modern, safe, and secure airport of the 21st century, continuing to serve as the region's principal international gateway within a more regional Southern California aviation system and committed to setting the global airport standard for customer satisfaction and security, regional economic leadership and organizational performance;

**WHEREAS**, the LAX Plan, and the LAX Specific Plan (last amended by Ordinance 185,164), which is the implementing ordinance for airport-owned properties in the LAX Plan area, are the guiding land use policy and regulatory documents for LAX.

**WHEREAS**, the LAX Specific Plan establishes zoning and land use regulations and procedures to ensure consistency with the LAX Plan, allows and regulates on-site signage, and states that the LAX Sign District will regulate off-site, supergraphics, digital displays, and mural signs.

**WHEREAS**, in June 2015, the City Council approved the LAX Sign District to regulate off-site signage within a 469-acre interior portion of LAX surrounding passenger terminal areas.

**WHEREAS**, in 2024, LAX was the world's eleventh busiest passenger airport, moving approximately 76.6 million annual passengers, making LAX a regional destination that serves as a center of commerce and a gateway to Asia and the Pacific region and international transport.

**WHEREAS**, as a world-class airport and international gateway for local and visiting travelers, signage is a vital component of the local, regional, and state economy and world-class airports that contribute to an image of quality and excellence for the City and promotes Los Angeles as a destination of regional importance;

**WHEREAS**, signage is a common and important feature at world-class airports and plays a role in defining the image of the airport by enhancing travelers' visual experience by fostering a dynamic and engaging pedestrian, tourist, and work environment, and by promoting business, cultural, entertainment, and visitor-serving activities and events in the City of Los Angeles;

**WHEREAS**, in anticipation of hosting the 2026 FIFA World Cup and 2028 Olympic and Paralympic Games, the City has invested billions into modernizing LAX, including, but not limited to the renovation of terminals and construction of an Automated People Mover (APM) to transport travelers to and from the LAX facilities and the regional transportation system; and

**WHEREAS**, an amendment to the LAX Sign District would support the LAX Plan and LAX Specific Plan, and is necessary to update the signage regulations to be consistent with LAX modernization efforts, including the repurposing of the former 1961 Airport Traffic Control Tower as a placemaking and signage element, as well as include updates for consistency with other City signage regulations and Chapter 1A of the LAMC.

**NOW, THEREFORE,**

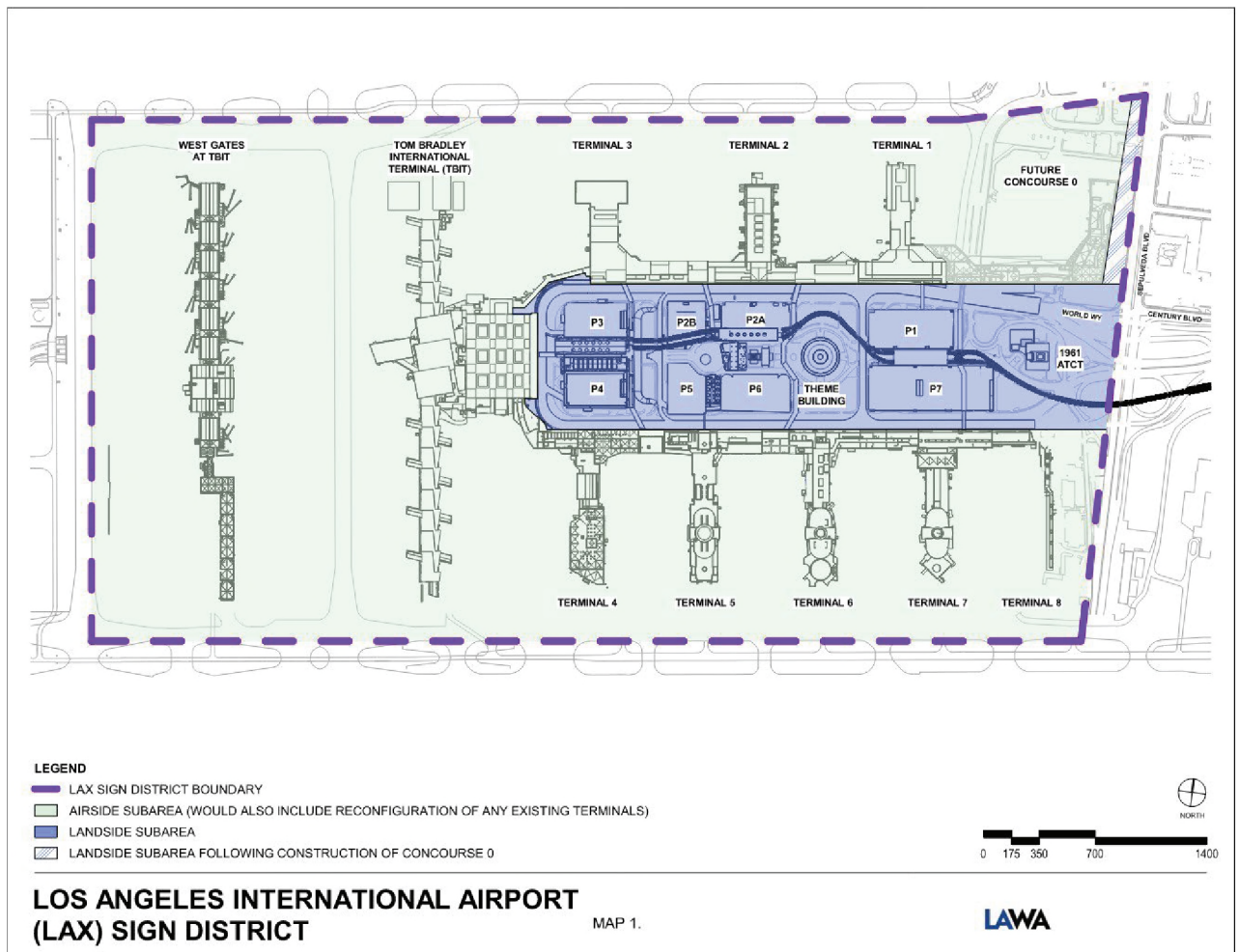
**THE PEOPLE OF THE CITY OF LOS ANGELES DO ORDAIN AS FOLLOWS:**

**SECTION 1. ESTABLISHMENT AND AMENDMENT OF THE LOS ANGELES INTERNATIONAL AIRPORT SIGN DISTRICT.**

- A. Boundaries.** The City Council hereby amends and restates the LAX Sign District (Sign District) in its entirety, which shall be applicable to the approximately 469-acre portion of LAX, covering an area that includes the LAX Central Terminal Area along World Way and the area along Sepulveda Boulevard as shown within the boundary lines on Map 1 - Sign District Boundary and Subarea Map. This section supersedes Section 13.11 B of Chapter 1 of the Code to the extent that Section 13.11 B limits Sign Districts to the C, M or R5 Zones, however, a Sign District shall also be permitted in the LAX Los Angeles International Airport Specific Plan Zone.
- B. Subareas.** This Sign District is divided into two Subareas as shown on Map 1. The purpose of the Subareas is to address the relationship between sign intensity and the uses surrounding each Subarea.
- 1. Airside Subarea.** The Airside Subarea as shown on Map 1 of this Ordinance is a portion of the Airport Airside area as shown on Figure 2 of the LAX Specific Plan and includes terminal concourses, gates, passenger boarding bridges, airport access ways and equipment which allow for the safe and efficient operation of airport airfield activities.

2. **Landside Subarea.** The Landside Subarea as shown on Map 1 of this Ordinance, which includes portions of the Airport Airside area and Airport Landside area and shown on Figure 2 of the LAX Specific Plan, including the arrival and departure roadways associated with the Central Terminal Area of LAX, as well as portions of the terminals facing the interior Central Terminal Area roadway, parking structures, roadway columns, the ground access areas and property generally located between Sepulveda Boulevard and Terminal 1, and the area along Sepulveda Boulevard immediately adjacent to the Central Terminal Area.

**MAP 1 - SIGN DISTRICT BOUNDARY AND SUBAREA MAP**



## SECTION 2. PURPOSE.

### A. The LAX Sign District is intended to:

1. Promote and enhance LAX as an international gateway to the Pacific Rim and as an important public amenity, and maintain an image as one of the nation's premier airports by encouraging creative, well-designed signs that

- contribute in a positive way to LAX's visual environment;
2. Recognize the uniqueness of LAX as a regional economic engine;
  3. Ensure that new Off-Site Signs are responsive to and integrated with the aesthetic character of the structures on which they are located, and are positioned in a manner that is compatible with the surrounding architecture and other signage at the airport;
  4. Promote compatibility between Off-Site Signs and adjacent residential communities by encouraging design strategies that minimize visual impacts such as clutter, excessive scale, illumination, and motion or animation. This approach supports the preservation of neighborhood character and enhances the overall visual environment;
  5. Support and enhance limited new Off-Site Signs for LAX in an effort to achieve the urban design, land use, economic development, and modernization objectives of the LAX Master Plan and LAX Specific Plan;
  6. Remove blight and improve the various gateways to Los Angeles by attaining public benefits that enhance the transportation and aesthetic resources surrounding LAX;
  7. Minimize potential traffic hazards and protect public safety;
  8. Allow for flexibility in the use of emerging technologies and evolving planning methodologies, while ensuring that signage remains compatible with the character, scale, and identity of the surrounding area;
  9. Encourage signs that contribute positively to the visual environment in a manner that accentuates the architectural characteristics of the airport;
  10. Permit a variety of signage elements to allow for creativity and flexibility in design over time; and
  11. Coordinate the location and display of signs to enhance the public pedestrian realm, minimize potential traffic hazards, protect public safety, and maintain compatibility with surrounding uses.

### **SECTION 3. APPLICATION OF SUPPLEMENTAL USE DISTRICT REGULATIONS.**

- A. The regulations of this Ordinance are in addition to those set forth in the Planning and Zoning provisions of the Code and the LAX Specific Plan. These regulations do not convey any rights not otherwise granted under the provisions and procedures contained in the Code or other relevant ordinances, except as specifically provided for in this Ordinance.

- B. Wherever this Ordinance contains provisions that establish regulations that are different from, more restrictive than, or more permissive than those contained in the Code, this Ordinance shall prevail. Whenever this Ordinance contains references to sign regulations in Section 4C.11 of Chapter 1A of the Code, they shall supersede references to Section 14.4 of Chapter 1 of the Code in Section 14 of the LAX Specific Plan.
- C. On-Site Signs located within this Sign District shall comply with Section 14.4 of Chapter 1 of the Code, except as modified by the LAX Specific Plan or this Sign District. Off-Site Signs shall comply with all applicable regulations in this Sign District and the LAX Sign District Design Standards and Guidelines. Notwithstanding any other provision of this Ordinance, the LAX Specific Plan, or the Code, any sign permitted and constructed as an Off-Site Sign may contain on-site messages without being subject to On-Site Sign regulations in the Code.

#### **SECTION 4. DEFINITIONS.**

Whenever the following terms are used in this Ordinance, they shall be construed as defined in this Section. Words and phrases not defined here shall be construed as defined in Section 4C.11 (Signs) and Section 14.3 (Glossary) of Chapter 1A of the Code and in the LAX Specific Plan.

**Can Sign.** A sign whose text, logos and/or symbols are placed on the plastic face of an enclosed cabinet.

**Captive Balloon Sign.** Any object inflated with hot air or lighter-than-air gas that is tethered to the ground or a structure.

**Column Wrap Sign.** A sign, attached to the existing columns that support the LAX Central Terminal Area upper-level roadway, which wraps around the entire circular column on the LAX Central Terminal Arrivals level.

**Digital Display.** A sign face, building face, and/or any building or structural component that displays still images, scrolling images, moving images, or flashing images, including video and animation, through the use of grid lights, cathode ray projections, light emitting diode displays, plasma screens, liquid crystal displays, fiber optics, or other electronic media or technology that is either independent of or attached to, integrated into, or projected onto a building or structural component, and that may be changed remotely through electronic means.

**LAX Sign District Design Standards and Guidelines.** LAX Sign District Design Standards and Guidelines approved by the Executive Director of Los Angeles World Airports (LAWA) established for the purpose of implementing this Ordinance by implementing parameters for a sign program that respond to the complex and fluid nature of the airport environment by: 1) prioritizing wayfinding signage, 2) structuring the approach to size and placement of Off-Site Signs to minimize distractions to drivers and pedestrians, and 3) providing signage opportunities

within a visual field of a consistent size and type.

**Off-Site Sign.** A sign that displays any message directing attention to a business, product, service, profession, commodity, activity, event, person, institution, or any other commercial message, which is generally conducted, sold, manufactured, produced, offered or occurs elsewhere than within the boundaries of the Sign District.

**On-Site Sign.** A sign that is other than an Off-Site Sign.

**Pole Sign.** A freestanding sign that is wholly independent of a building for support, permanently affixed to the ground using one or more poles or posts.

**Supergraphic Sign.** A sign, located within the Sign District, consisting of an image projected onto a wall or printed on vinyl, mesh or other material with or without written text, supported and attached to a wall by an adhesive and/or by using stranded cable and eye-bolts and/or other materials or methods, and that does not comply with the following provisions of the Code: Chapter 1A Sections 4C.11.6.C.9. (Projecting Signs), 4C.11.6.C.4. (Marquee Signs), 4C.11.2. (Temporary Signs), 4C.11.1.E.3 (Original Art Murals, Vintage Original Art Murals & Public Art Installations).

**Temporary Sign.** Any sign that is to be maintained for a limited duration, including paper signs and other signs that are not permanently affixed to the ground or building.

## **SECTION 5. EXISTING SIGNS. RIGHTS OF OWNER TO SIGNS.**

- A. Sign Rights.** Existing legally permitted signs may continue to exist and be constructed, operated, maintained, repaired, replaced or structurally altered in accordance with the requirements of Section 91.6216 of the Code.
- B. Historic Sign.** The Terminal 6 Sign Tower sign, as shown in Figure 1 below, located on the roof of Terminal 6, is the only known historic sign within the Sign District as of the effective date of this Ordinance. No modification or replacement of the historic sign is permitted without approval from the Department of City Planning's Office of Historic Resources (OHR). Any approval or determination by OHR shall be made based on conformance with the Secretary of the Interior's Standards.

Figure 1 – Historic Sign



LAX SIGN DISTRICT

FIGURE 1.

LAWA

## SECTION 6. PROCEDURAL REQUIREMENTS

**A. Building Permits.** The Los Angeles Department of Building and Safety (LADBS) shall not issue a permit for a sign, a sign structure, sign illumination, or alteration of an existing sign, unless the sign complies with: (1) the applicable requirements of this Ordinance as determined by the Executive Director (also known as the Chief Executive Officer) of LAWA, or their designee (Executive Director); and (2) the applicable requirements of the Code and state and federal laws.

### **B. Review Procedure.**

1. **Executive Director Sign-Off.** With respect to the following signs, only a ministerial Executive Director sign-off on the permit application shall be required prior to issuance of a building permit by LADBS for the following Off-Site Signs within the Sign District.

- a. Column Wrap Signs

- b. Digital Displays
  - c. Pole Signs
  - d. Supergraphic Signs
  - e. Wall Signs
2. **Executive Director Sign-Off Review Procedure.** The Executive Director shall approve the permit if the sign is within the Sign District and otherwise complies with all the applicable requirements of this Ordinance and the LAX Sign District Design Standards and Guidelines, the applicable requirements of the Code, and State and federal laws. The Executive Director's approval shall also be indicated by stamping and dating the permit plans.
- a. Each Off-Site Sign permit application shall include an inventory of all existing and proposed Off-Site Signs within the Sign District at the time of application. Prior to the issuance of any building permit resulting in a change to the total sign area of Off-Site Signs, the Executive Director shall verify that the amounts of total Off-Site Signs proposed do not exceed the maximum allowed by the Sign District. The Executive Director shall at all times maintain an updated summary sheet that accurately reflects: (a) the amount of Off-Site Sign area proposed and built within the Sign District; and (b) the remaining amounts of sign area available under the Sign District.

**C. Exempt Signs, Murals, and Art Installations.** LADBS permit applications for the following sign types shall be subject to the applicable LADBS review and approval and/or Executive Director review and approval under the LAX Specific Plan, the Code and the Administrative Code, but are exempt from Executive Director's review under this Ordinance:

- 1. On-Site Signs pursuant to the Code or the LAX Specific Plan;
- 2. Original Art Murals;
- 3. Public Art Installations;
- 4. Temporary Signs.

**D. Sign Reduction, Visual Blight Reduction, and Community Messaging Program.** As set forth under Section 9 (Sign Reduction, Visual Blight Reduction, and Community Messaging Program) of this Ordinance, no building permit shall be issued for any Off-Site Sign within the Landside Subarea prior to compliance with that section.

**E. Requests for Deviations, Modifications, and Interpretations of Regulations.**

1. **Adjustments, Exceptions, Interpretations.** The procedures for project adjustments, project exceptions, and specific plan interpretations to this Ordinance shall follow the procedures set forth in Section 13B.4 (Specific Plan Implementation) of Chapter 1A of the Code.
2. **Modifications of Entitlement.** The procedures for modifications of entitlement shall follow the procedures set forth in Section 13B.5.4 of Chapter 1A of the Code.
3. **Initiation.** An application for a Project Adjustment, Project Exception, Specific Plan Interpretation, or Modification of Entitlement is filed with LAWA instead of the Planning Department.
4. **Decision and Appeal Authority.** Notwithstanding the provisions of the Code, the initial decision maker for a Project Adjustment and a Specific Plan Interpretation shall be the Executive Director and all references to the Director shall mean the Executive Director instead. In each case where a Planning Commission has the authority for initial review, hearing, appeal and/or approval of a request for Modification of Entitlement, Project Adjustment, Project Exception, or Specific Plan Interpretation, such authority shall be vested in the Board of Airport Commissioners for Los Angeles World Airports (BOAC) in place of the Planning Commission.
5. **Sign District Ordinance Amendment.** The procedures for amendment of this Ordinance shall follow the procedures set forth in Section 13B.1.2 of Chapter 1A of the Code. In addition to the entities listed in Section 13B.1.2.B of Chapter 1A of the Code, BOAC may also initiate an amendment to this Ordinance.
6. **LAX Sign District Design Standards and Guidelines Amendment.** The LAX Sign District Design Standards and Guidelines may be amended by the Executive Director from time to time pursuant to procedures established by LAWA. LAWA shall notify the Department of City Planning, the Council Office, and local Neighborhood Council(s) at least 45 days prior to the Executive Director decision of the intent to amend the LAX Sign District Design Standards and Guidelines.
  - a) The Executive Director must make the following Findings prior to amending the LAX Sign District Design Standards and Guidelines:
    - i. The LAX Sign District Design Standards and Guidelines, as amended, will be in conformance with the goals, policies and intent of the LAX Sign District, LAX Specific Plan, the Community Plan and General Plan.
    - ii. The LAX Sign District Design Standards and Guidelines, as amended, will not result in any additional building mass, height,

or bulk.

- iii. The LAX Sign District Design Standards and Guidelines, as amended, will continue to be pedestrian and transit oriented, especially in the design and configuration of the street level facades.
- IV. The LAX Sign District Design Standards and Guidelines, as amended, will be in proper relation to adjacent uses or to the development or the community.
- V. The LAX Sign District Design Standards and Guidelines, as amended, will be desirable to the public convenience or welfare.
- VI. The LAX Sign District Design Standards and Guidelines, as amended, will not be materially detrimental to the character of development in the immediate neighborhood.

**F. Exceptions.** No Modification of Entitlement, or any Adjustment shall be required for: (a) a change in sign advertising or sign text, images or copy; (b) any construction for which a permit is required in order to comply with an order issued by LADBS to repair or replace an unsafe or substandard condition; or (c) a modification to any sign or any sign support structure that results in a change of a sign from a Digital Display to a Non-Digital Display.

## **SECTION 7. GENERAL REQUIREMENTS.**

**A. General Requirements of the Code.** Unless specified in this Ordinance to the contrary, the general sign requirements set forth in the Code shall apply to this Sign District for permits, plans, design and construction, materials, street address numbers, identification, maintenance, prohibited locations and sign illumination. Notwithstanding the foregoing, any combined area restrictions in Section 4C.11 (Signs) of Chapter 1A of the Code, shall not apply to signs within the Sign District area, but the area for all Off-Site Signs shall be counted in the combined area restrictions specified in this Ordinance. A building permit shall be obtained from LADBS in accordance with the applicable provisions of the Code for any signs, sign structures, and/or sign alterations, other than changes to or replacement of sign face copy.

**B. Permitted Signs.** Except as otherwise prohibited in Section 7.C (Prohibited Signs), below, and notwithstanding Section 4C.11.1.C.8 (Prohibited Signs) of Chapter 1A of the Code, all signs described and regulated in Section 8 (Standards for Specific Types of Signs) of this Ordinance, all signs otherwise permitted by the Code, and all previously legally permitted signs shall be permitted within the Sign District area.

**C. Prohibited Signs.** The following signs shall be prohibited:

1. Can Signs;
2. Captive Balloon Signs;
3. Roof Signs;
4. Signs covering window exteriors;
5. Any sign not specifically authorized by this Ordinance or by the Code.

**D. Illumination.** All signs may be illuminated by either internal or external means. The illumination regulations set forth in the Code shall apply, except as set forth herein. Methods of signage illumination may include but are not limited to electric lamps, such as neon tubes; fiber optics; incandescent lamps; LED; LCD; cathode ray tubes exposed directly to view; shielded spotlights and wall wash fixtures. In addition, signage shall be subject to the following regulations:

1. Signage within the Airside Subarea shall not be illuminated.
2. No sign or combination of signs shall produce greater than 0.3 footcandles (fc) of illumination as measured at the nearest residentially zoned property line.
3. Sign lighting contrast ratios shall be limited to a maximum of 30:1 to eliminate glare.

**E. Reflective Materials.** Signage shall not use highly reflective materials such as mirrored glass.

**F. Green Code.** All light sources, including illuminated signage, shall comply with the applicable provisions of CALGreen (Part 11 of Title 24, California Code of Regulations [CCR]), California Energy Code (Part 6 of Title 24, CCR) 130.3 and Section 140.8, and the Green Code of the City of Los Angeles.

**G. Digital Display Illumination Standards.**

1. **Brightness.** Digital Displays shall have a nighttime luminance, from sunset to sunrise, no greater than 300 candelas per square meter (cd/m<sup>2</sup>), all white, and a daytime brightness, from sunrise to sunset, no greater than 6,000 cd/m<sup>2</sup>. The Digital Displays shall transition smoothly at a consistent rate from their daytime luminance to their maximum permitted nighttime luminance levels, beginning not less than 45 minutes prior to sunset, and concluding at sunset. After sunrise, signs will transition smoothly from the applicable nighttime maximum luminance of 300 cd/m<sup>2</sup> for 45 minutes, up to their daytime luminance.
2. **Adjustment of Luminance.** Each Digital Display shall be fully dimmable and shall be controlled so that luminance levels may be adjusted according to the

time of day and applicable lighting standards. Sign luminance would be reduced to the maximum nighttime brightness (300 cd/m<sup>2</sup>) at any time when ambient sunlight is less than 100 footcandles (fc).

3. **Beam Spread.** All light emitting diodes used within a Digital Display shall have a maximum horizontal beam spread of 165 degrees and maximum vertical beam spread of 90 degrees, facing primarily downwards and shall conform to the applicable provisions of the Code as well as federal and State laws, including the California Business and Professions Code and Vehicle Code.

**H. Refresh Rate.** The Refresh Rate is the rate at which a Digital Display may change content. Refresh Rates are as follows and shall apply to Digital Displays as set forth in Section 8.B (Digital Displays) of this Ordinance.

1. **Controlled Refresh Rate I.** The refresh (change) rate of a Digital Display, inclusive of any change in whole or in part of the sign image, which is no more frequent than one refresh event every eight seconds, with an instant transition between images The sign image must remain static between refreshes.
2. **Controlled Refresh Rate II.** The refresh (change) rate of a Digital Display, inclusive of any change in whole or in part of the sign image, which is no more frequent than one refresh event every sixteen seconds, with an instant transition between images. The sign image must remain static between refreshes.
3. **Controlled Refresh Rate III.** The refresh (change) rate of a Digital Display, inclusive of any change in whole or in part of the sign image, which is no more frequent than one refresh event every 1 hour, with an instant transition between images The sign image must remain static between refreshes.

**I. Hazard Review.**

1. In connection with the adoption of this Ordinance, the City has completed the initial review otherwise required under Section 4C.11.1.C.5 (Hazard to Traffic) of Chapter 1A of the Code for signs that adhere to the specifications set forth in this Ordinance and, therefore, shall require no further hazard or Code compliance review, except as set forth below.
2. In the event:
  - a) Any Digital Display is not in substantial conformance with the LAX Sign District Design Standards and Guidelines;
  - b) Any sign does not adhere to the requirements of this Ordinance or requires an adjustment, exception or amendment to this Ordinance; or

- c) Any sign that utilizes New Technologies, as set forth in Section 7.M (New Technologies) of this Ordinance will require a permit from LADBS; and prior to submission to LADBS, must be submitted to the Los Angeles Department of Transportation (LADOT) for hazard evaluation and determination per Section 4C.11.1.C.5 (Hazard to Traffic) of Chapter 1A of the Code. Until LADOT determines that any such sign, individually or in the aggregate with other signs authorized under this Ordinance, does not constitute a hazard, LADBS shall not issue a permit for the sign or sign structure.

**J. Visual Maintenance.** All signs shall be maintained to meet the following criteria at all times:

1. The building and ground area around the signs shall be properly maintained. All unused mounting structures, hardware and wall perforation from any abandoned/removed sign shall be removed and building surfaces shall be restored to their original condition.
2. All sign copy shall be properly maintained and kept free from damage and other unsightly conditions, including graffiti. It must be repaired or replaced immediately upon tearing, ripping, or peeling, or when marred or damaged by graffiti.
3. All sign structures shall be kept in good repair and maintained in a safe and sound condition and in conformance with all applicable codes.
4. Razor wire, barbed wire, concertina wire, or other barriers preventing unauthorized access to any sign shall be hidden from public view.
5. No access platform, ladder, or other service appurtenance, visible from the sidewalk, street, or public right-of-way, shall be installed or attached to any sign structure.
6. Signs that are no longer serving the current tenants, including sign structures, shall be removed and the building facades originally covered by the signs shall be repaired and/or resurfaced with materials and colors that are compatible with the facades.

**K. Alterations, Repairs or Rehabilitation.** Any alteration, repair or maintenance work on a legally permitted sign or sign structure shall be governed by the Code.

**L. Materials.** The materials, construction, application, location and installation of any Sign shall be in conformance with the Los Angeles Building Code and the Los Angeles Fire Code.

**M. New Technologies.** The Executive Director may permit the use of any technology

or material provided that the material is approved by LADBS if the Executive Director finds that such technology or material is consistent with the regulations described herein.

## **SECTION 8. STANDARDS FOR SPECIFIC TYPES OF SIGNS.**

### **A. General Sign Area and Location Requirements.**

1. **Maximum Permitted Combined Sign Area.** The maximum permitted sign area for Off-Site Signs regulated by this Ordinance is 80,722 square feet within the Landside Subarea and 289,600 square feet within the Airside Subarea.
2. **Permitted Off-Site Sign Types.** Off-Site Signs are authorized to the extent permitted in this Ordinance and the LAX Sign District Design Standards and Guidelines. The following are the types of Off-Site Signs that shall be permitted within the Sign District; only Off-Site Signs shall be included in the calculations of maximum permitted combined sign area:
  - a. Column Wrap Signs;
  - b. Digital Displays;
  - c. Pole Signs;
  - d. Supergraphic Signs;
  - e. Wall Signs.
3. **Locations for Signs.**
  - a. **General.** No sign shall be placed over the exterior surface of any opening of a building, including its windows, doors, and vents, unless the LAFD determines, in writing, that the sign would not create a hazardous condition.
  - b. **Landside Subarea.** Off-Site Signs are prohibited on the Theme Building and the Clifton A. Moore Administration building (excluding the 1961 Former Airport Traffic Control Tower). The location of these buildings is shown on Map 1 of this Ordinance.
  - c. **Airside Subarea.** Off-Site Signs are only permitted on Passenger Boarding Bridges (structures located in the Airside Subarea that connect passengers from terminals to the aircraft at each aircraft gate).

4. **Design Standards and Guidelines.** In addition to the regulations in this section, Off-Site Signs shall also comply with the general, area, and location standards identified in the LAX Sign District Design Standards and Guidelines adopted by the Executive Director.
5. **Sign Reduction, Visual Blight Reduction, and Community Messaging Program.** As set forth under Section 9 (Sign Reduction, Visual Blight Reduction, and Community Messaging Program) of this Ordinance, no building permit shall be issued for any Off-Site Sign within the Landside Subarea prior to compliance with that section.
6. **Digital Displays.**
  - a. **Illumination.** Digital Displays shall be internally illuminated and subject to the applicable illumination standards of Sections 7.D (Illumination), 7.E (Reflective Materials), 7.F (Green Code), and 7.G (Digital Display Illumination Standards) of this Ordinance.
7. **Refresh Rates.**
  - a. Digital Displays located on the parking structures shall be limited to Controlled Refresh Rate I, except the single Digital Display authorized for the eastern facing facade of Parking Structure 1 shall be limited to Controlled Refresh Rate II. All Controlled Refresh Rate I signs shall refresh in a coordinated manner as established in the LAX Sign District Design Standards and Guidelines.
  - b. Digital Displays on the east face of Terminal 1 or the east face of future Concourse 0 shall be limited to Controlled Refresh Rate II. Digital Displays on these buildings and the Digital Display on the eastern facade of Parking Structure 1 shall refresh in a coordinated manner as established in the LAX Sign District Design Standards and Guidelines.
  - c. Digital Displays on the 1961 Former ATCT shall be limited to Controlled Refresh Rate II. Digital Displays on the 1961 Former ATCT do not need to refresh simultaneously with other signs.
  - d. Digital Displays on Sky Bridges, which are the pedestrian bridges that connect the LAX Central Terminal Area parking structures or an Automated People Mover (APM) Station to the passenger terminal buildings, shall be limited to Controlled Refresh Rate III. All Controlled Refresh Rate III signs shall refresh in a coordinated manner as established in the LAX Sign District Design Standards and Guidelines.
8. **Hours of Operation.** Digital Displays may be operated 24 hours a day, seven days a week.

9. **1961 Former Airport Traffic Control Tower (ATCT) Signs.** The 1961 ATCT is the existing 13-story tower at the eastern end of the Central Terminal Area within the center of the Clifton A. Moore Administration Building that was used for air traffic control from 1961 until 1996. Digital Displays on the 1961 ATCT shall comply with the following regulations.
  - a. Prior to the issuance of any sign permit for the 1961 ATCT, an application for a Historic-Cultural Monument shall be submitted to the Department of City Planning.
  - b. Prior to the issuance of any sign permit for the 1961 ATCT, sign plans shall be submitted for review and approval by the Department of City Planning's Office of Historic Resources (OHR). Any approval or determination by OHR shall be made based on conformance with the Secretary of the Interior's Standards.

## **SECTION 9. SIGN REDUCTION, VISUAL BLIGHT REDUCTION, AND COMMUNITY MESSAGING PROGRAM.**

**A. Sign Reduction Program.** As specified below, sign reduction is required for all signs within the Landside Subarea that are Off-Site Signs. Sign reduction shall not be required for Off-Site Signs within the Airside Subarea. As part of the installation of up to 80,722 square feet of Off-Site Signs in the Landside Subarea, LAWA shall be responsible for the removal of 23,520 square feet of legally permitted Off-Site Signs from within the LAX Vicinity Sign Reduction Area (the area within the following City of Los Angeles Community Plan Areas: LAX Plan, Westchester - Playa del Rey Community Plan, Palms - Mar Vista - Del Rey Community Plan, Venice Community Plan, and the West Los Angeles Community Plan) within five (5) years from the issuance of the first building permit for new Off-Site Signs pursuant to this Ordinance. All signage removed from within the LAX Vicinity Sign Reduction Area in the ten (10) years preceding implementation of this Ordinance shall be credited toward this Sign Reduction Program. LAWA shall develop the LAX Vicinity Sign Reduction Program in consultation with the Department of City Planning and Council District 11.

Upon the effective date of this Ordinance, LAWA may install up to 80,722 square feet of new Off-Site Signs in the Landside Subarea. As part of the leases, licenses or other agreements for all new Off-Site Signs within the LAX Sign District, the BOAC shall require that 12.5% of all revenue net of all expenses payable from sales companies for the placement of advertising media on Off-Site Signs be deposited into a LAWA Sign Removal (LSR) Trust Fund pursuant to the requirements below. The LSR Trust Fund monies, including, without limitation, any expenditures, shall be controlled by the BOAC in its sole discretion and used exclusively to satisfy the requirement in Section 9.A above for the removal of 23,520 square feet of existing Off-Site Signs in the LAX Vicinity Sign Reduction Area.

LAWA shall be solely responsible for documenting the total amount of total square footage of Off- Site Signs removed from the LAX Vicinity Sign Reduction Area, and shall provide to the Department of City Planning and Council District 11 an annual report documenting the number, location and square footage of approved Off-Site Signs, and the number, location and square footage of existing Off-Site Signs removed. This reporting requirement shall remain in place until 23,520 square feet of lawfully permitted Off-Site Signs from within the LAX Vicinity Sign Reduction Area have been removed. Payments into the LSR Trust Fund shall continue for as long as necessary to satisfy LAWA's obligation to remove 23,520 square feet of existing Off-Site Signs from the LAX Vicinity Sign Reduction Area. Upon satisfaction of this sign removal requirement, any remaining monies in the LSR Trust Fund shall be freed from any further restriction and LAWA may use such monies for any LAWA purpose.

**B. Sign Reduction Program.** The following shall be provided to demonstrate compliance with the Sign Reduction Program:

1. **Proof of Sign Removal.** A final demolition permit and photographic evidence that such sign has been removed prior to the issuance of any building permit for any Off-Site Digital Display or Supergraphic Sign subject to this Section of this Ordinance.

**C. Visual Blight Reduction Program.** Prior to the installation of any Off-Site Signs within the Landside Subarea, LAWA shall prepare and present a Visual Blight Reduction Program (VBR) to the BOAC for adoption and implementation. The BOAC shall be solely responsible for adoption and implementation of the Visual Blight Reduction Program, including, without limitation, any expenditure of funds from the VBR Trust Fund defined below.

1. **Scope and Adoption.** The Visual Blight Reduction Program shall be targeted to improve visual aesthetics at gateway corridor areas at or adjacent to LAX with high percentages of airport visitor traffic, and shall serve, in conjunction with the requirements of Section 9.A, to mitigate the ongoing impacts of new Off-Site Signs at LAX. The program shall be limited in geography to the LAX Plan Area and the Westchester - Playa del Rey Community Plan Area with the following priorities: (a) within the VBR Primary Area defined as the corridors of (1) Century Boulevard (west of Aviation Boulevard), (2) Sepulveda Boulevard (south of Manchester Avenue and north of Imperial Highway), (3) Lincoln Boulevard (south of Westchester Parkway), and (4) World Way (any location); (b) within the VBR Secondary Area defined as the corridor of Century Boulevard between Aviation Boulevard and the Interstate 405 freeway; and (c) areas outside of the VBR Primary Area and the VBR Secondary Area subject to review and approval by the Federal Aviation Administration (FAA). The program may include streetscape and landscape capital improvements, graffiti abatement, wayfinding signage, and streetscape maintenance funding.

2. **Development of VBR.** As required by the originally adopted LAX Sign District, LAWA has developed the Visual Blight Reduction Program in consultation with the Department of City Planning and Council District 11. The BOAC adopted it on April 20, 2017 (Resolution No. 26219), prior to the installation of any Off-Site Signs in the Landside Subarea.
3. **Funding.** The BOAC shall require that a one-time payment in the amount of \$3,750,000 be deposited into a LAWA Visual Blight Reduction (VBR) Trust Fund within five years from the issuance of the first building permit for new Off-Site Signs pursuant to this Ordinance. The VBR Trust Fund monies shall be controlled by the BOAC and used exclusively to satisfy the requirements in Section 9.C above.
4. **Term.** As required, the Visual Blight Reduction Program was adopted by the BOAC within two years of the effective date of this Ordinance. The funding requirement of Section 9.A shall remain in place to the extent that such Off-Site Signs approved by this Ordinance remain operational. Upon removal of all Off-Site Sign content on signs authorized by this Ordinance, the Visual Blight Reduction Program shall terminate. Upon termination of the Visual Blight Reduction Program, any remaining monies in the VBR Trust Fund shall be free from any further restriction and LAWA may use such monies for any LAWA purpose.

**D. Community Messaging Program.** Digital Displays may be utilized by LAWA and the City as follows.

1. **LAWA Messaging.** LAWA may utilize advertising time of the Digital Displays each month to promote: (a) LAWA, airport operations, awareness, events, branding and messaging; (b) public safety, emergency preparedness, homeland security warnings/updates, Amber alerts, etc.; (c) airport recognition and accolades; and/or (d) public and community art.
2. **Emergency Messaging.** LAWA may display emergency messages on the Digital Displays during emergencies that involve the need to reach the traveling public at-large.

## **SECTION 10. SEVERABILITY.**

If any provision of this Ordinance or its application to any person or circumstance is held to be unconstitutional or otherwise invalid by any court of competent jurisdiction, the invalidity shall not affect other provisions, clauses or applications of said Ordinance, which can be implemented without the invalid provision, clause or application, and to this end, the provisions and clauses of this Ordinance are declared to be severable. The City Council hereby declares that it would have passed this Ordinance and each portion or subsection, sentence, clause and phrase herein, irrespective of the fact that any one or more portions, subsections, sentences, clauses or phrases be declared invalid.

## **SECTION 11. CITY CLERK CERTIFICATION AND PUBLICATION**

The City Clerk shall certify to the passage of this Ordinance and have it published in accordance with Council policy, either in a daily newspaper circulated in the City of Los Angeles or by posting for ten days in three public places in the City of Los Angeles: one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall; one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall East; and one copy on the bulletin board located at the Temple Street entrance to the Los Angeles County Hall of Records.

## **EXHIBIT C**

### **Draft LAX Sign District Design Standards and Guidelines**

# **LAX Sign District Design Standards and Guidelines Draft**

January 9, 2026

DCP Comments: February 2, 2026

Responses: February 12, 2026

DCP Comments: February 17, 2026

Responses: February 20, 2026

## **SECTION 1: INTRODUCTION**

This document, the LAX Sign District Design Standards and Guidelines, contains provisions that direct change to public lands within the boundaries of the LAX Sign District (Ordinance No. XXXX, effective XX, 2026). This document includes criteria regarding location, illumination, size, and placement of signs located within the boundary of the LAX Sign District. The LAX Sign District Design Standards and Guidelines were initially adopted by the Executive Director of the Los Angeles World Airports (LAWA) and may be amended subject to the LAX Sign District Ordinance (Ordinance).

Applicants seeking to obtain Executive Director approval under the Ordinance for a new sign, a sign structure, sign illumination, or alteration of an existing sign, must demonstrate to the satisfaction of the Executive Director of LAWA, or the Executive Director's designee, that the provisions of the Design Standards and Guidelines have been met as well as the provisions of the LAX Sign District Ordinance.

These LAX Sign District Design Standards and Guidelines may contain both "Development Standards" and "Design Guidelines." Development Standards are legal requirements that address aspects of site development and sign design for which physical specifications can be described. Design Guidelines are strong recommendations that provide direction for more subjective considerations. All requirements in Sections 2 and 3 of the document are currently Development Standards. Design Guidelines, if needed, may be implemented at a later date.

Requests for deviations, modifications and interpretations of the Development Standards must follow the procedures in Section 6E of the Ordinance. Deviations from Design Guidelines must be justified or explained to the Executive Director or his/her representative during the Executive Director Sign-Off process.

Illustrative examples and other graphics are provided to demonstrate application of the LAX Sign District Design Standards and Guidelines. This document is not a substitute for the services of professional architects, engineers, and building contractors in new construction and renovation. All development will adhere to the City's Building and Safety Code, the LAX Sign District Ordinance, and to State and Federal laws.

## **SECTION 2: OVERVIEW OF LAX SIGN DISTRICT DESIGN STANDARDS AND GUIDELINES**

### **A. The LAX Sign District**

Exterior non-commercial and on-site advertising as well as off-site signage at LAX are authorized by the LAX Specific Plan Ordinance (Ordinance No. 185,164). Per the Specific Plan, off-site signs are more specifically governed by the LAX Sign District. As required by the LAX Sign District Ordinance, additional, more detailed requirements for off-site signs are contained in the LAX Sign District Design Standards and Guidelines, which are approved by the Executive Director of LAWA.

The geographic boundaries of the LAX Sign District are identified in the Ordinance (Map 1 – Sign District Boundary and Subarea Map) and are divided into the Airside Subarea and Landside Subarea. Within the Airside Subarea, “Off-Site Signs are on Passenger Boarding Bridges”. Within the Landside Subarea, Off-Site Signs are allowed on multiple locations, but prohibited on the Theme Building and the Clifton A. Moore Administration building (excluding the former 1961 Airport Traffic Control Tower). Also, within the Landside Subarea are special provisions for digital displays on the 1961 Airport Traffic Control Tower.

The Ordinance addresses prohibited, exempt, and allowable sign types, as well as the general parameters for the size, location, and illumination. The LAX Sign District Design Standards and Guidelines provide additional detail, regulations, and guidelines for the design and placement of signage. These Development Standards and Design Guidelines within this document are a companion document to the LAX Tenant Signage Standards and LAWA Terminal Wayfinding Standards which apply to exterior signage not covered by the Ordinance, including signage inside and outside of the LAX Sign District boundaries. The LAX Sign District Ordinance also includes standards governing illumination levels and refresh rates for Digital Display signs. The CEQA analysis for the Ordinance was based on these standards.

### **B. General**

The purpose of the LAX Sign District Design Standards and Guidelines is to ensure that new Off-Site Signs are responsive to and integrated with the aesthetic character of the structures on which they are located and are positioned in a manner that is compatible with both the surrounding architecture and other signage at the airport. As such, the Design Standards and Guidelines create parameters for a sign program that responds to the complex and fluid nature of the airport environment by:

- Allowing wayfinding signage to take priority
- Structuring the approach to size and placement of Off-Site Signs to minimize distractions to drivers and pedestrians
- Giving LAWA discretion to allow flexibility in the sign program as changes to the built environment occur

### **C. Locations**

As indicated above, within the Airside Subarea, Off-Site Signs are only permitted on passenger boarding bridges. Within the Landside Subarea, Off-Site Signs are allowed on Terminals, Parking Structures, Sky Bridges, Arrivals Level Roadway Columns, and Departures Roadway Poles. Also, within the Landside Subarea are special provisions for digital signage on the 1961 Airport Traffic Control Tower. These locations are shown on the Sign Type and Location Table in Section 2.E below.

Signs may be located within the areas shown as Opportunity Zones on Figure 1 included in the LAX Sign District Design Standards and Guidelines. The drawings are intended to illustrate the general location on building facades and Sky Bridges that signs may be placed, but not to limit the number of signs within each Opportunity Zone. Signs regulated by the Ordinance may not be placed outside of the Opportunity Zones.

### **D. LAWA Approval and City Permits**

The LAX Sign District Ordinance and these Design Standards and Guidelines describe what is allowed within the context of the LAX Sign District. Approvals given by LAWA ensure that the provisions of the Ordinance and the Design Standards and Guidelines are followed appropriately and consistently. LAWA approval is a necessary first step before obtaining clearances and permits from the Los Angeles Fire Department and Department of Building and Safety, etc. Documentation of signage materials, methods of attachment, structural analysis and electrical engineering etc. will need to be completed as part of the permit process. No sign authorized by the LAX Sign District Ordinance will be installed without the necessary approvals and permits.

Table 1 below summarizes the types of signs allowed at different locations within the LAX Sign District. The specific Design Standards and Guidelines for the different sign types and locations are described in the sections that follow.

**E. Table 1 - LAX Sign District Type and Location**

Sign Type	Landside Subarea						Airside Subarea
	Arrivals and Departures Level Roadway Poles	Arrivals Level Roadway Columns	Terminal Building Façades	APM and Terminal Sky Bridges <sup>1</sup>	CTA Parking Structures	1961 ATCT	Passenger Boarding Bridges
Column Wrap		•					
Digital Displays			•	•	•	•	
Pole Signs	•						
Supergraphic Signs			•	•	•		•
Wall Signs			•	•	•		•

Notes:

APM = Automated People Mover; ATCT = Airport Traffic Control Tower; CTA = Central Terminal Area

<sup>1</sup> Terminal sky bridges are bridges from the parking structures and the terminals at Terminals 1 and 7. APM Terminal sky bridges are bridges from the APM stations within the CTA to the terminals.

**F. Sign Types**

**I. Column Wrap Signs**

Column Wrap Signs shall comply with the following regulations:

**a. General**

A Column Wrap Sign shall not be a Digital Display.

**b. Illumination**

Signs may be externally illuminated and shall not be internally illuminated.

**c. Location**

Column Wrap Signs are allowed per Figure 1 and may be attached to columns on the lower (Arrivals) level roadway that have no information or wayfinding signs.

**d. Size, Placement, and Consistency**

**1. Size**

Height - The top of a Column Wrap Sign shall be at least 3 feet below the top of the column, and a minimum of 2 feet above the finished grade.

Width – The sign may cover the full surface of the column within the height constraints or only cover a part of the column.

## **2. Placement and Consistency**

All Column Wrap Signs displayed at a given time must be of the same size and placed with a consistent orientation with respect to the roadway traffic. The height above the finished grade and dimension from the top of the column must all be approximately the same, with the goal of establishing a consistent datum.

## **II. Digital Displays**

Digital Display shall comply with the following regulations:

### **a. General**

Digital Displays on the 1961 Airport Traffic Control Tower are subject to additional requirements, in Section 3.E.

Digital Displays shall be free of any visible bracing, angle iron, guy wires, cable, and/or similar supporting elements. All exposed portions of electronic displays, including backs, sides, structural support members and support poles, shall be screened to the satisfaction of the Executive Director.

### **b. Illumination**

The lighting of Digital Displays shall be subject to the regulations of the Ordinance.

### **c. Location**

Digital Displays are allowed within the Opportunity Zones at the locations identified on Figure 1 in the Landside Subarea on Parking Structures, Sky Bridges, certain Terminal Facades, and 1961 Airport Traffic Control Tower.

For Terminal Facades, Digital Displays are permitted on the Concourse 0 east-facing facade and south-facing facade, when that facility is constructed. The location opportunities for digital display signage on Concourse 0 would depend on the final design of that facility. Prior to the construction of Concourse 0, Digital Displays are permitted on the Terminal 1 east-facing facade. Digital Displays are not permitted on any other terminal facades.

Digital Displays shall be attached to or erected against the wall of a building or structure, with the exposed face of the sign in a plane approximately parallel to the plane of the wall. Digital Displays shall not cover distinct architectural features of a building's facade, with the exception of the 1961 Airport Traffic Control Tower.

### **d. Size, Placement, and Consistency**

For specifics on size, placement, and consistency see Parking Structures, Terminal Facades and Sky Bridges below in Sections 3.A, 3.B, and 3.C.

**e. Animation**

Full animation of any Digital Display, defined as images, videos, animation, parts and/or illumination that change, move, stream, scroll, or otherwise incorporate motion to change at an unrestricted rate, is not permitted.

**f. Refresh Rates**

All Digital Displays on a given parking structure that are subject to Controlled Refresh I shall refresh simultaneously. In addition, Digital Displays on Sky Bridges (subject to Controlled Refresh III) connected to a given parking structure shall only refresh when the Digital Displays on that parking structure (subject to Controlled Refresh Rate I) refresh.

Digital Displays with a Controlled Refresh II, except for the digital display on the 1961 Former ATCT, shall refresh at the same time.

**III. Pole Signs**

Pole Signs shall comply with the following regulations:

**a. General**

The Pole Sign shall be oriented so that the message, graphic or symbol on the sign is perpendicular to the adjacent roadway.

**b. Illumination**

Pole Signs may be externally illuminated and shall not be internally illuminated.

**c. Location**

A Pole Sign shall only be within the Landside Subarea.

**d. Size, Placement, and Consistency**

**1. Size**

*Height*- The lowest portion of a suspended portion of the Pole Sign shall be at least 15 feet above the roadway. The highest portion of the sign shall not extend above the top of the sign support.

*Width* – The width of the suspended portion of the Pole Sign must be less than the width of the horizontal support member to which it is attached.

**2. Placement**

The signs must be placed at a consistent rhythm on the installed poles, i.e., on every pole, every other pole, or every third pole, etc. but not less than one on every 10 poles. The signs may be placed on new or existing poles.

**3. Consistency**

Pole Signs on the Departures (upper) or Arrivals (lower) Level Roadways that are displayed at the same time must all be of the same size, attached in the same way and have the same orientation. The signs must be placed with continuous spacing along the roadway.

#### **IV. Supergraphic Signs and Wall Signs**

Supergraphic Signs and Wall Signs shall comply with the following regulations:

##### **a. General**

The exposed face of a Supergraphic Sign or Wall Sign shall be approximately parallel to the plane of the wall upon which it is located.

##### **b. Illumination**

In the Landside Subarea, Supergraphic Signs and Wall Signs may be externally illuminated and shall not be internally illuminated.

##### **c. Location**

In the Landside Subarea, Supergraphic Signs and Wall Signs are allowed only on Parking Structures, Terminal Facades, and Sky Bridges. In the Airside Subarea, they are allowed only on Passenger Boarding Bridges. See Figure 1.

##### **d. Size, Placement and Consistency**

For specifics on size and placement, see Parking Structures, Terminal Facades and Sky Bridges in Sections 3.A, 3.B, and 3.C below.

### **SECTION 3: SIZE, PLACEMENT AND CONSISTENCY FOR SPECIFIC LOCATIONS**

#### **A. Parking Structures**

##### **I. Sign Types**

Digital Displays, Supergraphic Signs and Wall Signs are allowed on Parking Structures as indicated below.

For Parking Garage facades with shear walls, Digital Display, Supergraphic Signs and Wall Signs may be placed on the solid walls which are at least 20 feet in width. For Parking Garage facades without shear walls, these signs may cover the vertical circulation elements that do not have openings facing the roadway or wayfinding graphics and are a minimum of 20 feet in width. Signs are also allowed on Parking Garage facades without shear walls within the Opportunity Zones.

##### **II. Size**

*Height* – The top of a sign shall be located no higher than 2 feet below the top of the structure on which it is placed, and no lower than 10 feet above finished grade.

*Width* – Signs shall be placed a minimum of 2 feet from both sides of the end of a

building or façade on which they are located.

### **III. Placement**

Signs shall be centered in the middle of the facade. Signs are allowed within the Opportunity Zones at the locations identified on Figure 1.

### **IV. Consistency**

On any given parking structure facade, signs displayed at any one time shall be the same size and placed so that the top and bottom edges align with each other.

## **B. Terminal Facades**

### **I. Sign Types**

Supergraphic and Wall Signs are allowed within the Opportunity Zones at the locations identified on Figure 1. Digital Displays would only be permitted on the Concourse 0 east-facing facade and south-facing facade, when that facility is constructed. The location opportunities for digital display signage on Concourse 0 would depend on the final design of that facility. Prior to the construction of Concourse 0, Digital Displays would be permitted on the Terminal 1 east-facing facade. Digital Displays would not be permitted on any other terminal facades.

### **II. Size**

*Height*- Signs on Terminal facades shall be a minimum of 2 feet above the finished grade, with the top of the sign located no higher than the top of the adjacent window/doorframe.

*Width* - Signs shall be placed a minimum of 2 feet from both sides of the wall surface on which they are located.

### **III. Placement**

Signs shall not be applied to windows or doors, and shall only be located on walls that do not have glass and have contiguous surfaces without protrusions, indentations or equipment. Signs shall be centered on the wall.

### **IV. Consistency**

On any given terminal facade, signs displayed at any one time shall be the same size and placed so that the top and bottom edges align with each other.

## **C. Sky Bridges**

### **I. Terminal Pedestrian Bridges**

Terminal Pedestrian Bridges are the existing bridges between the terminal and parking structures on Terminals 1, 4, and 7 that pre-date the construction of the Automated People Mover (APM), but may be removed or replaced by an APM Terminal Pedestrian Bridge at a later date.

**a. Sign Types**

Digital Displays, Supergraphic Signs, and Wall Signs are permitted to be placed on Terminal Pedestrian Bridges.

**b. Size**

*Height* – The maximum size of the sign shall not exceed the span from the top edge of the bridge to the bottom edge of the bridge.

*Width* – The maximum size of the sign shall not exceed the full width of the straight section of the bridge.

**c. Placement**

The sign shall be located on one side of the bridge, facing the dominant flow of traffic. The sign shall be centered on the bridge along the horizontal dimension. Signs are only allowed within the Opportunity Zones at the locations identified on Figure 1.

**d. Consistency**

Signs displayed at any one time on more than one Terminal Pedestrian Bridge must be of the same type, size and placement.

**II. APM Terminal Pedestrian Bridges**

APM Terminal Pedestrian Bridges are the new bridges between the terminal and parking structures or the APM stations on Terminals 2, 3, 4, 5, and 6 that were constructed as part of the APM Project. A future APM Terminal Pedestrian Bridge may be constructed on Terminal 1 replacing the existing Terminal Pedestrian Bridge.

**a. Sign Types**

Digital Displays, Supergraphic Signs, and Wall Signs may be placed on APM Terminal Pedestrian Bridges.

**b. Size**

*Height* – Signs may span from 1 foot below the top edge of the bridge to 1 foot above the bottom edge of the bridge.

*Width* – Signs may span a maximum of 2 structural modules (approximately 20 feet) and be a minimum of 1 foot from the vertical bridge element of the module.

**c. Placement**

Signs must be facing the dominant flow of traffic. Signs shall not be located within the area above the Arrivals Level Roadway. Signs shall only be above the Departures Level Roadway. Signs shall not be suspended below the bridges.

Signs are allowed within the Opportunity Zones at the locations identified on Figure 1, subject to the restrictions in this section.

**d. Consistency**

Signs displayed at any one time on more than one APM Terminal Pedestrian Bridge must all be of the same type, size and placement.

**D. Passenger Boarding Bridges**

**I. General**

Signs shall be Supergraphic Signs or Wall Signs.

**II. Illumination**

Signs may not be internally or externally illuminated.

**III. Location**

Signs shall only be located on Passenger Boarding Bridges in the Airside Subarea.

**IV. Size**

Signs may span from the top edge to the bottom edge of the telescoping section(s) and shall not exceed 1,040 square feet of sign area per sign, per side.

**V. Placement**

A single Passenger Boarding Bridge may have one sign on each of its two sides. A sign that is narrower than the full width of the telescoping section must be centered on that section.

**VI. Consistency**

All Signs displayed at any one time on more than one Passenger Boarding Bridge must be of the same type, size and placement within the Airside Subarea.

**E. 1961 Airport Traffic Control Tower**

Signs on the 1961 Airport Traffic Control Tower shall comply with the following regulations:

**I. General**

Prior to issuance of any permit for signage on this structure, an application for nomination of the 1961 Airport Traffic Control Tower to be a City of Los Angeles Historic-Cultural Monument (HCM) must be submitted. As such, any proposed signs must comply with the Secretary of Interior Standards.

Digital Displays may be placed on the 1961 Airport Traffic Control Tower.

## **II. Illumination**

The lighting of a Digital Display at the 1961 Airport Traffic Control Tower shall be subject to the illumination regulations of the Ordinance.

## **III. Location**

Digital Displays shall cover all of the four sides of the 1961 Airport Traffic Control Tower.

## **IV. Size**

*Area* - Signs on the 1961 Airport Traffic Control Tower are limited to a total sign area of approximately 25,725 square feet.

*Height* - Signs must span between the bottom of the second floor to the top of the twelfth floor.

*Width* - Signs must span the entire width of each facade.

## **V. Placement**

No sign shall cover the tower cab at the top of the building. No Off-Site Signs shall be allowed on the two-story portion of the Clifton A. Moore Administration building which surrounds the base of the 1961 Airport Traffic Control Tower.

## **VI. Off-Site Signs**

Off-Site Signs may consist of welcome messages and imagery; LAX-related travel and tourism messaging; content celebrating commemorations, City-hosted events, and special occasions; and artistic displays occurring outside the Sign District. Messages and imagery would be determined by LAWA and would be subject to applicable brightness, duration, and operational safety standards, and the City's federal grant obligations.

In addition, commissioned artworks as well as imagery depicting the historic building facade as originally constructed in 1961 may also be displayed.

## **VII. Animation**

Although images to be displayed on the 1961 Airport Traffic Control Tower cannot include full animation, color fades would be allowed similar to those that currently occur on the existing lighted LAX pylons.

## **VIII. Historical Resource Commitments**

Consistent with the LAX Preservation Plan and in coordination with the Department of City Planning's Office of Historic Resources, LAWA shall ensure that proposed signage on the 1961 ATCT respects the existing historic fabric, avoids adverse changes to historic resources, is compatible with the original design intent, and preserves the tower's integrity while accommodating contemporary airport needs through the implementation of the following features.

**a. Removal of Non-Contributing Elements**

LAWA shall remove the existing non-contributing exterior guardrails while retaining the original outriggers that the guardrails are attached to. Removal shall be completed prior to installation of new signage components and to avoid any disturbance or damage to the historic fabric. If a replacement is deemed necessary to comply with safety codes, it will be designed such that its visual profile is minimal and does not add to or interfere with the overall form and massing of the tower. Any replacement solution must also be such that if removed in the future, the essential form and integrity of the historic resource would be unimpaired, consistent with the Secretary of the Interior’s Standards for Rehabilitation.

**b. Visually Permeable LED Digital Media Displays**

LAWA shall require that any LED digital display mounted to the exterior of the 1961 ATCT employs permeable technology that allows visibility of the building’s original form and surviving features behind the added digital display.

The selected system shall incorporate variable transparency that maintains discernible views of the original tower wall, including the existing ribbon windows and plaster spandrels, similar to the variable transparency afforded by the original louver system.

LED modules, cabling, and structural attachments shall be minimized, concealed, or integrated to avoid obscuring or altering historic materials to the maximum feasible extent.

LAWA shall work with a design team to evaluate digital media options that maintain visibility of the original ATCT’s architectural elements while meeting functional display needs.

**c. Floor-by-Floor Horizontal Banding to Reflect Original Architectural Profile**

In order to recall the visual delineation of each floor characteristic of the ATCT’s 1961 appearance, the displays shall be mounted as horizontal bands across each floor, from the bottom of the second floor to the top of the twelfth floor, using the existing outrigger structure that historically supported the tower’s louvers, leaving a reveal between floors where no display is present. The displays shall span the entire width of each facade.

This floor-by-floor approach will re-establish the horizontal delineation of each floor, characteristic of the building’s original louvered facade.

The floor-by-floor approach, together with the visual permeability of the displays, will ensure that the displays do not obscure the full height of the ATCT, but instead recall the architectural rhythms of the ATCT’s original design.

Mounting systems must avoid attachment that would damage or obscure contributing historic materials.

**d. Interpretive and Educational Display Elements**

To enhance public understanding of the 1961 tower’s historic significance, LAWA shall incorporate digital interpretive elements into the digital display program.

LED-based digital content, projection treatments, or programmed lighting shall be used to periodically depict the tower’s historic appearance and recall its former use as an airport traffic control tower.

Interpretive content must be coordinated with LAWA’s Art Program, LA Welcome Program, and Celebratory Program to ensure regular and meaningful public communication of the tower’s history.

Content frequency, duration, and placement will be established during final design to ensure that a meaningful interpretive component can be integrated with more expressive displays.

**e. Continued Coordination with Preservation Professionals**

LAWA will engage qualified architectural historians and preservation architects who meet the Secretary of the Interior’s professional standards<sup>1</sup> during the final design, installation, and programming of the digital media displays to ensure compliance with the Secretary of the Interior’s Standards for Rehabilitation.

Review will include structural attachments, transparency levels, lighting intensity, interpretive elements, and operational protocols. Recommendations will be incorporated into the final design to avoid adverse effects to the historical significance of the 1961 ATCT.

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<sup>1</sup> The Secretary of the Interior’s Standards and Guidelines for Archeology and Historic Preservation (36 CFR Part 61) define minimum education, training, and experience required to perform identification, evaluation, registration, and treatment activities. U.S. Department of the Interior, *The Secretary of the Interior Professional Qualifications Standards*.

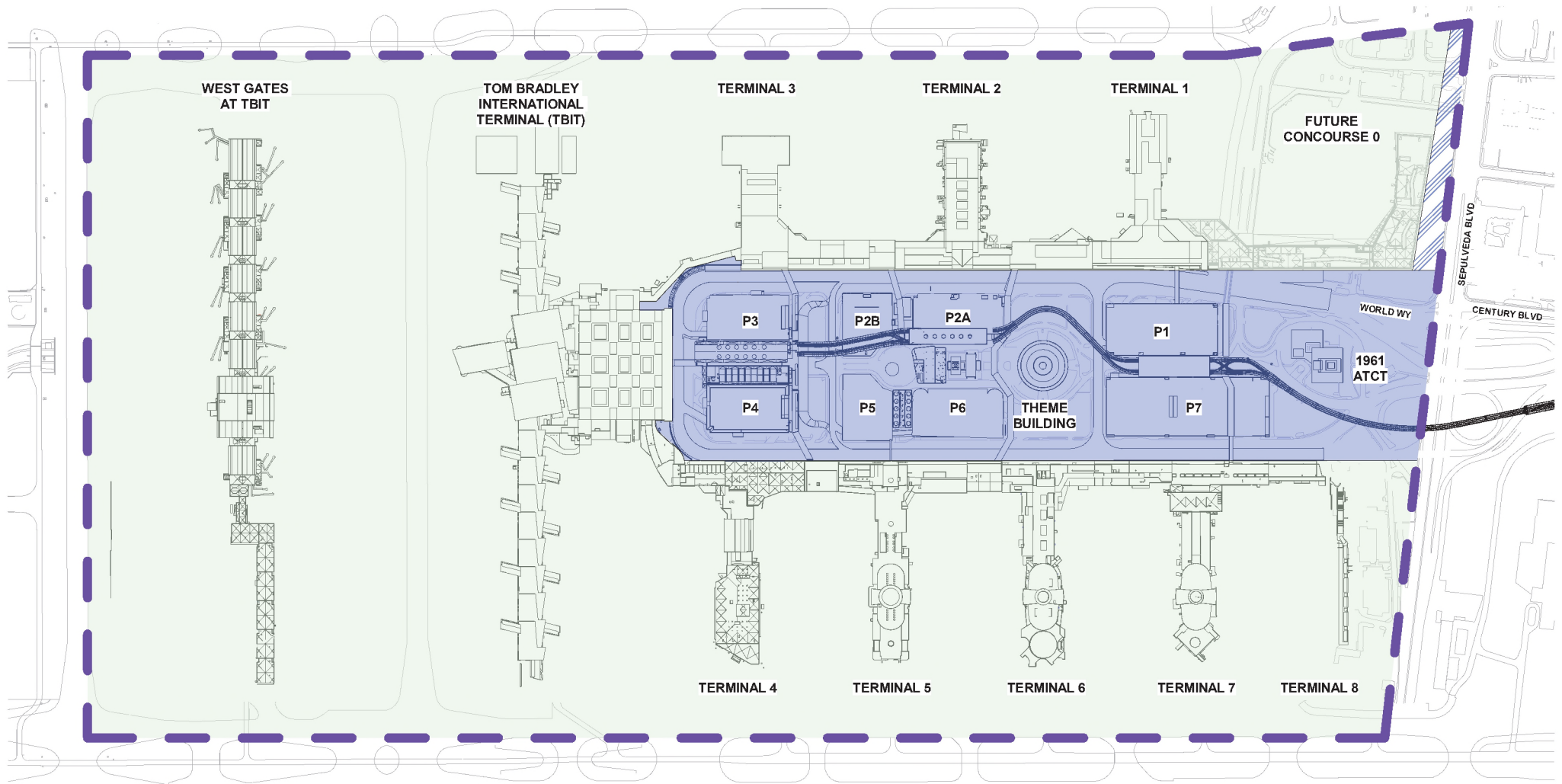
Available: <https://www.doi.gov/pam/asset-management/historic-preservation/PQS>.

Accessed January 9, 2026.

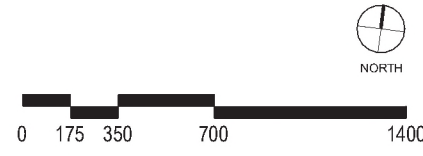
# **LAX SIGN DISTRICT**

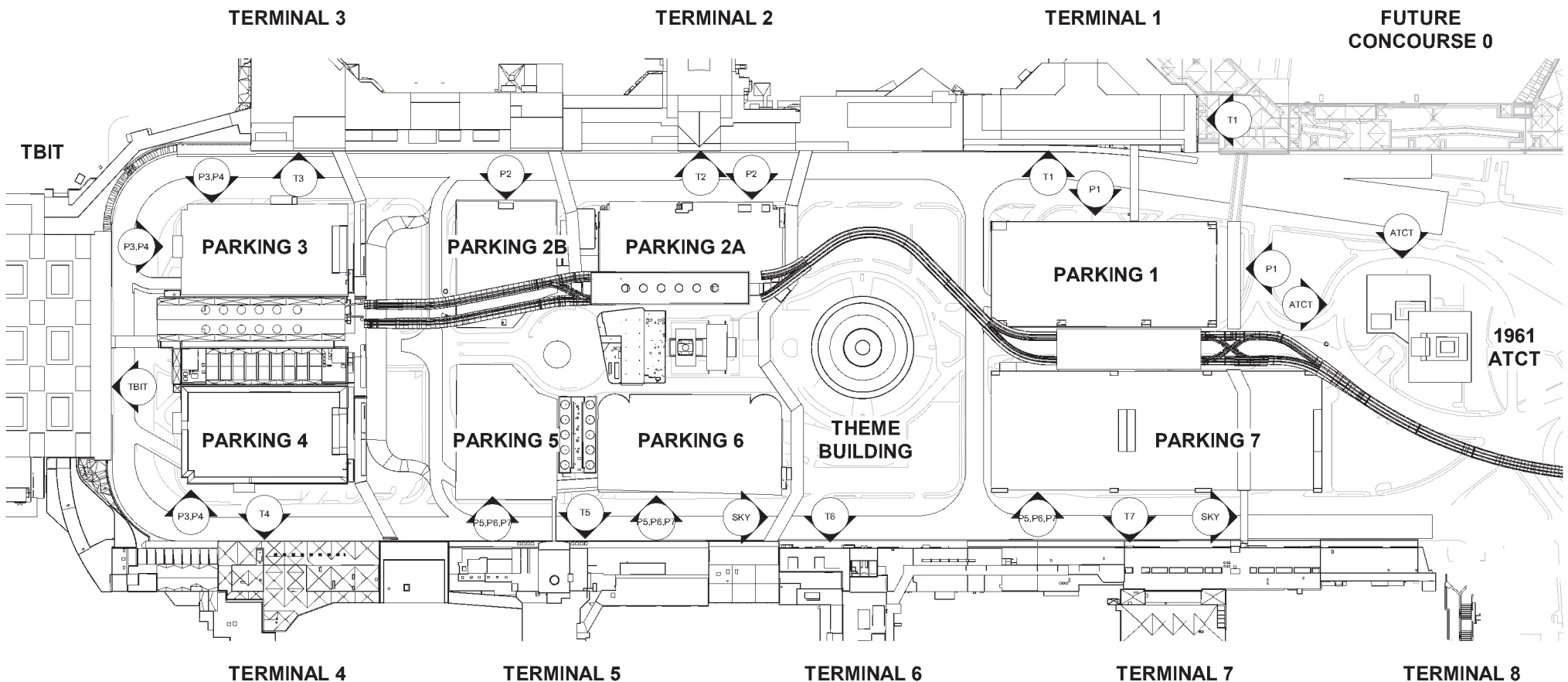
## **DESIGN STANDARDS & GUIDELINES**

### **ILLUSTRATIVE EXHIBITS**



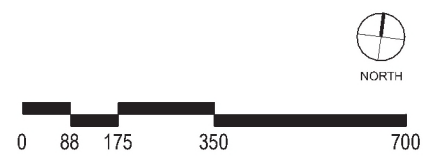
- LEGEND**
- LAX SIGN DISTRICT BOUNDARY
  - AIRSIDE SUBAREA (WOULD ALSO INCLUDE RECONFIGURATION OF ANY EXISTING TERMINALS)
  - LANDSIDE SUBAREA
  - LANDSIDE SUBAREA FOLLOWING CONSTRUCTION OF CONCOURSE 0

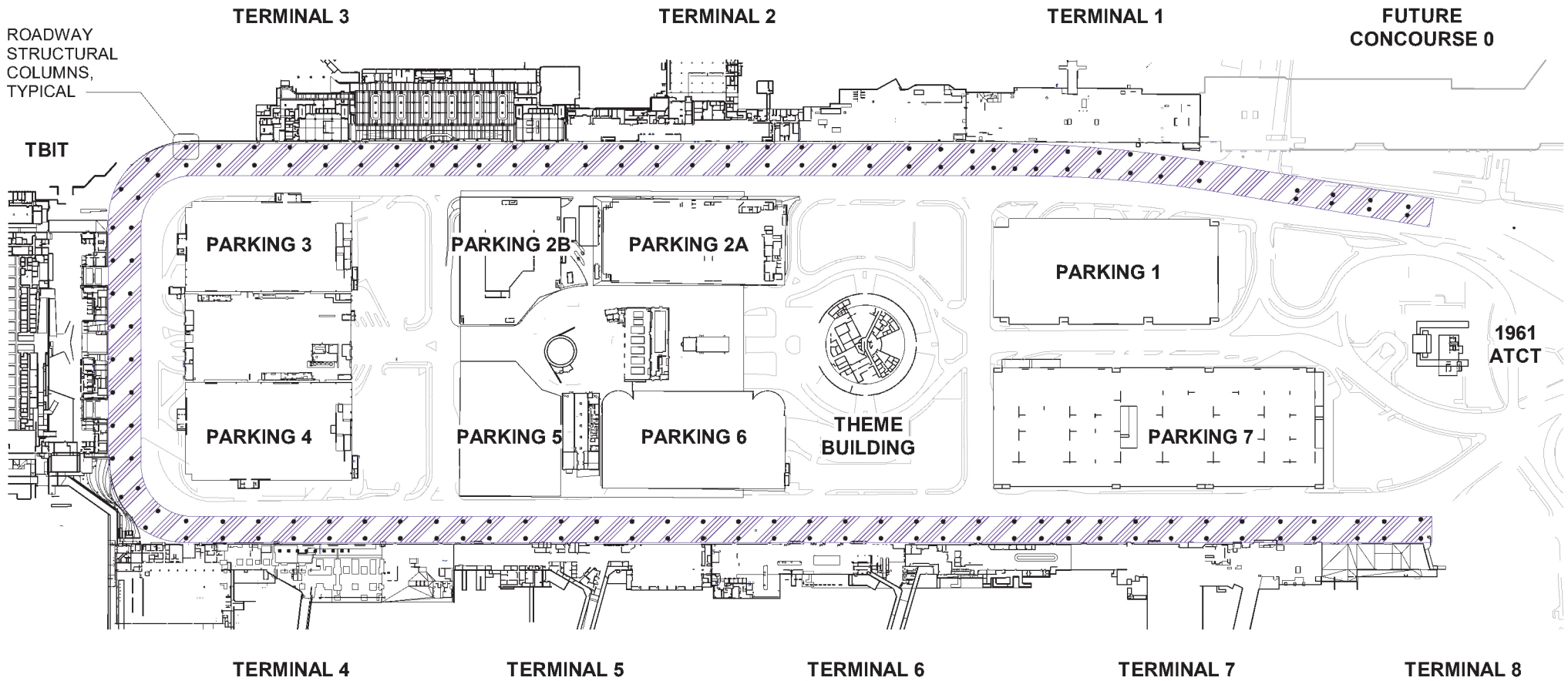




SITE - LANDSIDE SUBAREA

1" = 350'-0"





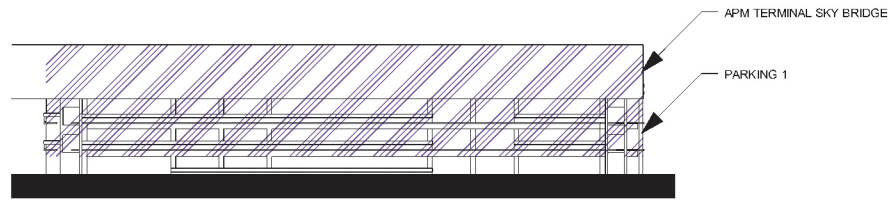
SITE - LANDSIDE SUBAREA - ARRIVALS LEVEL

1" = 350'-0"

**LEGEND**

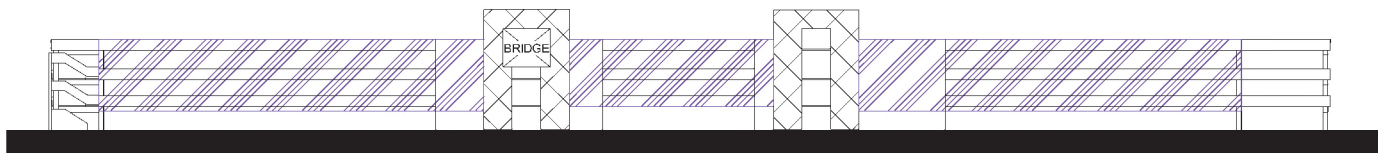
 OPPORTUNITY ZONE





PARKING 1 ELEVATION - EAST

1" = 80'-0"



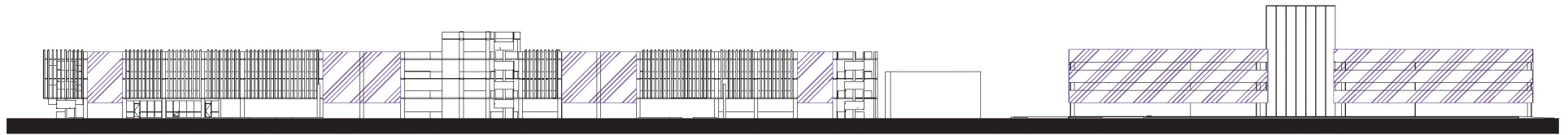
PARKING 1 ELEVATION - NORTH

1" = 80'-0"

LEGEND

 OPPORTUNITY ZONE





PARKING 2A ELEVATION - NORTH

1" = 80'-0"

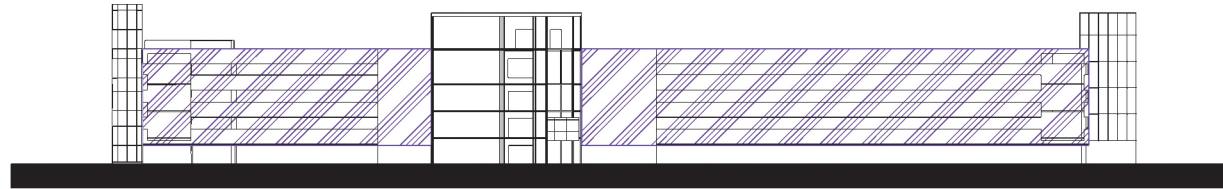
PARKING 2B ELEVATION - NORTH

1" = 80'-0"

LEGEND

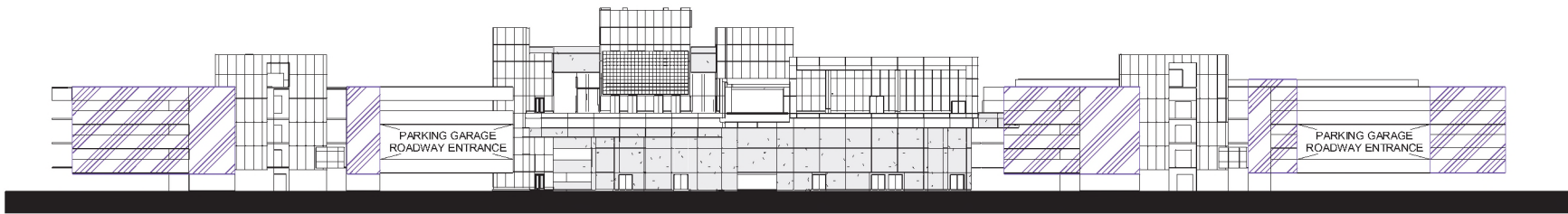
 OPPORTUNITY ZONE





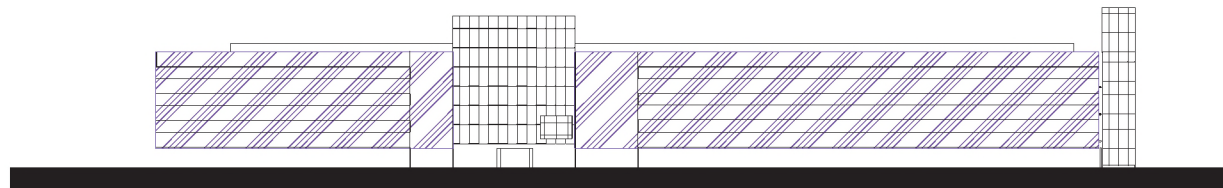
PARKING 3 ELEVATION - NORTH

1" = 80'-0"



PARKING 3 & 4 ELEVATION - WEST

1" = 80'-0"



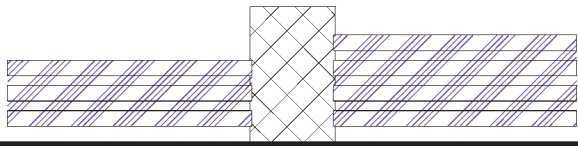
PARKING 4 ELEVATION - SOUTH

1" = 80'-0"

LEGEND

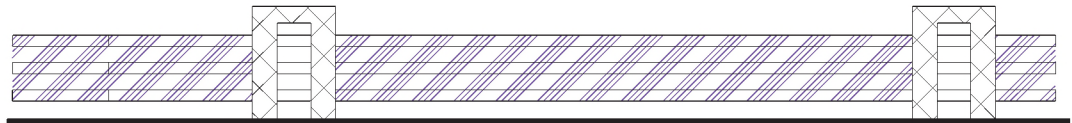
 OPPORTUNITY ZONE





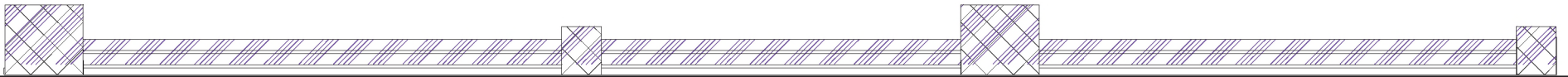
PARKING 5 ELEVATION - SOUTH

1" = 80'-0"



PARKING 6 ELEVATION - SOUTH

1" = 80'-0"



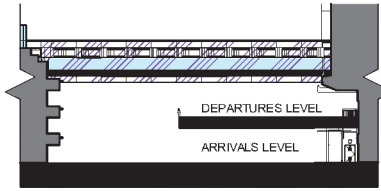
PARKING 7 ELEVATION - SOUTH

1" = 80'-0"

LEGEND

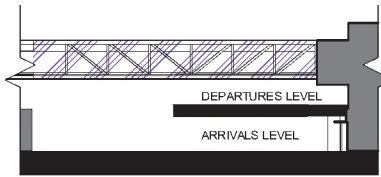
 OPPORTUNITY ZONE





TYPICAL APM TERMINAL SKY BRIDGE

1" = 80'-0"




TYPICAL TERMINAL SKY BRIDGE

1" = 80'-0"

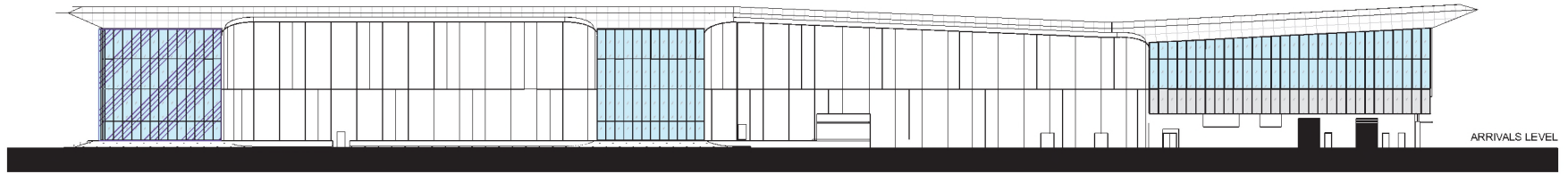
**NOTE**

- SKY BRIDGES SHALL FOLLOW TYPICAL LOCATION OPPORTUNITIES SHOWN

**LEGEND**

-  EXISTING GLASS AREAS
-  OPPORTUNITY ZONE





**FUTURE CONCOURSE 0 ELEVATION - EAST**



1" = 80'-0" DOCUMENT IS ILLUSTRATIVE OF POTENTIAL CONCOURSE FACADE TO BE DESIGNED

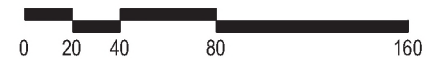


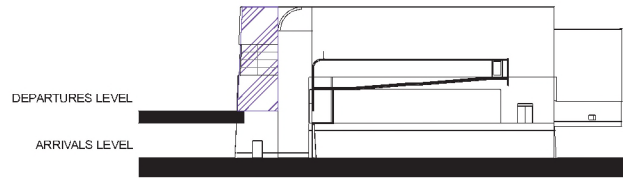
**FUTURE CONCOURSE 0 ELEVATION - SOUTH**

1" = 80'-0" DOCUMENT IS ILLUSTRATIVE OF POTENTIAL CONCOURSE FACADE TO BE DESIGNED

**LEGEND**

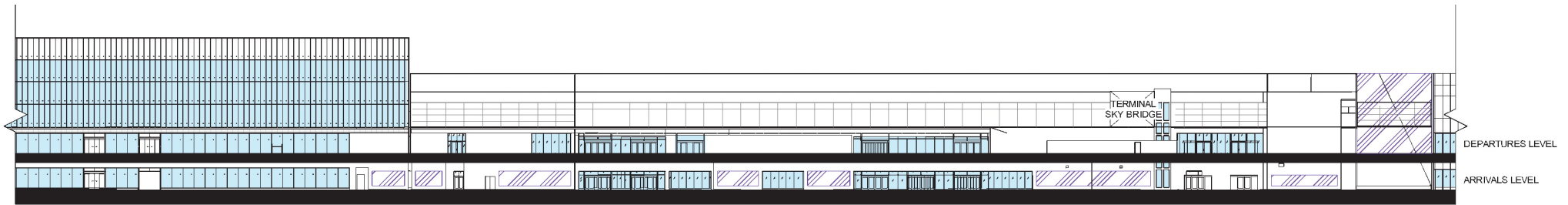
-  GLASS AREAS
-  OPPORTUNITY ZONE





TERMINAL 1 ELEVATION - EAST

1" = 80'-0"



TERMINAL 1 ELEVATION - SOUTH

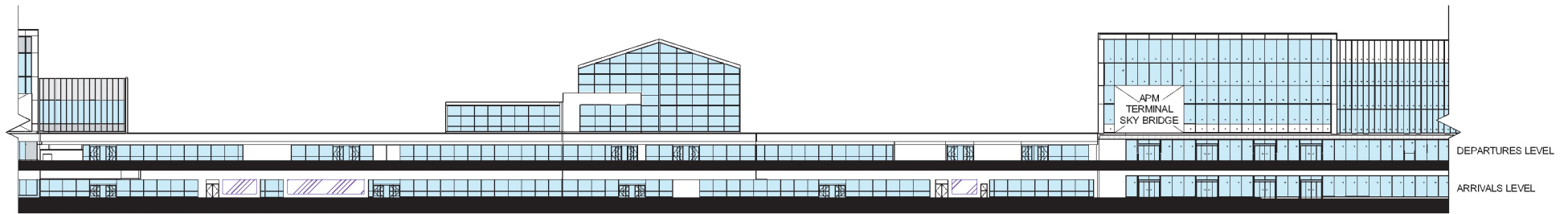
1" = 80'-0"

**LEGEND**

 EXISTING GLASS AREAS

 OPPORTUNITY ZONE





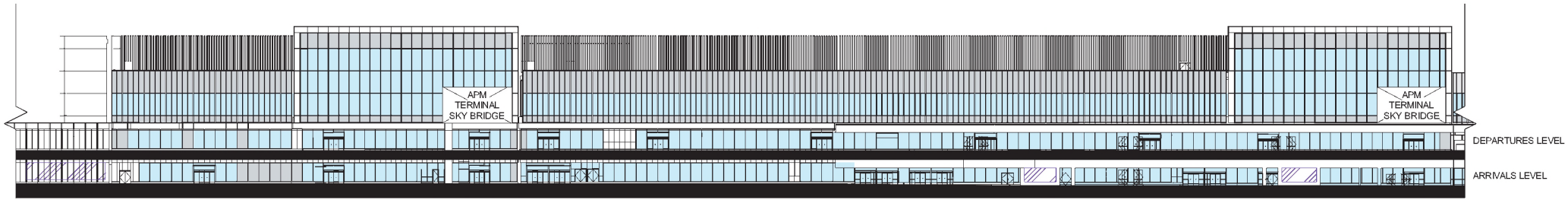
TERMINAL 2 ELEVATION - SOUTH

1" = 80'-0"

**LEGEND**

- EXISTING GLASS AREAS
- OPPORTUNITY ZONE





TERMINAL 3 ELEVATION - SOUTH

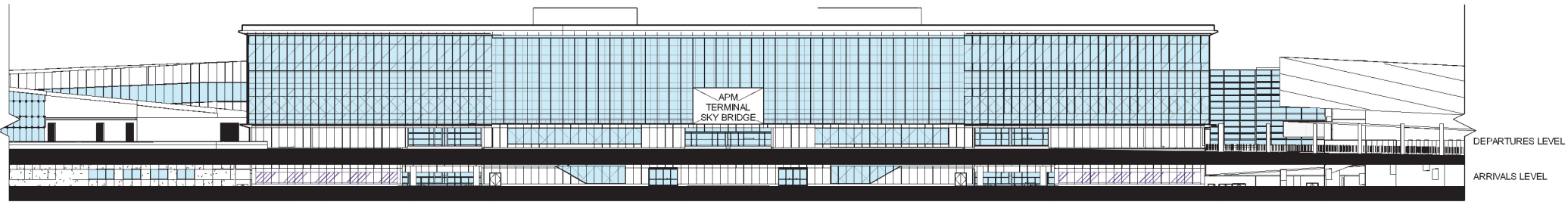
1" = 80'-0"

**LEGEND**

 EXISTING GLASS AREAS

 OPPORTUNITY ZONE



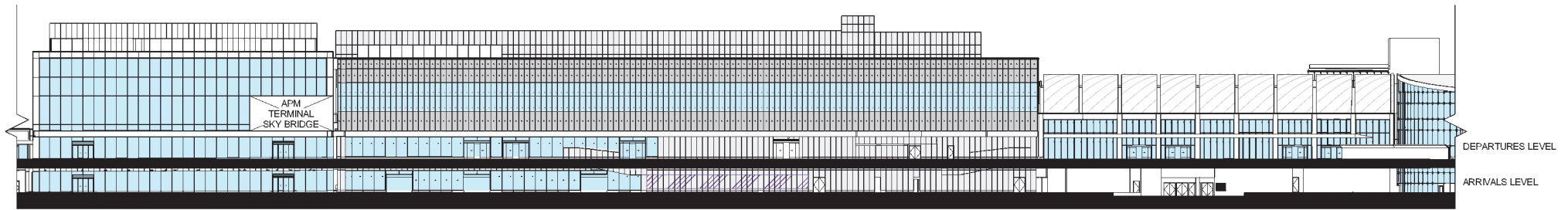


TBIT ELEVATION - EAST

1" = 80'-0"

- LEGEND**
- EXISTING GLASS AREAS
  - OPPORTUNITY ZONE





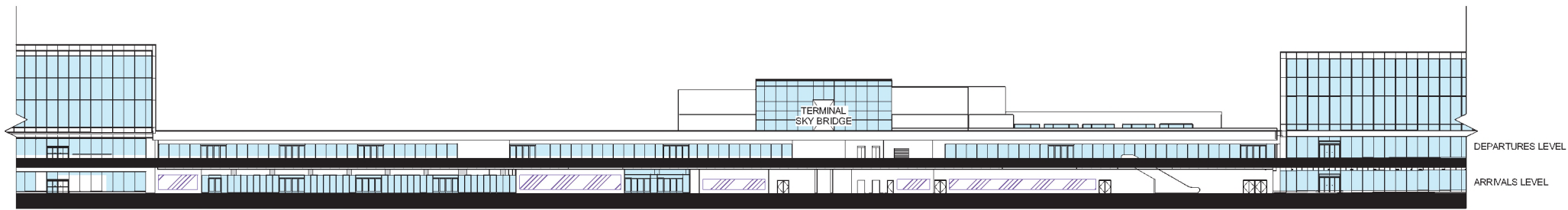
TERMINAL 4 ELEVATION - NORTH

1" = 80'-0"

**LEGEND**

- EXISTING GLASS AREAS
- OPPORTUNITY ZONE





TERMINAL 5 ELEVATION - NORTH

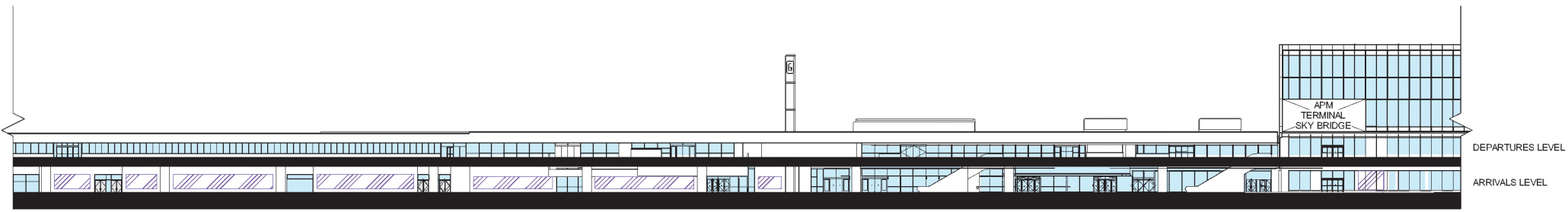
1" = 80'-0"

**LEGEND**

 EXISTING GLASS AREAS

 OPPORTUNITY ZONE



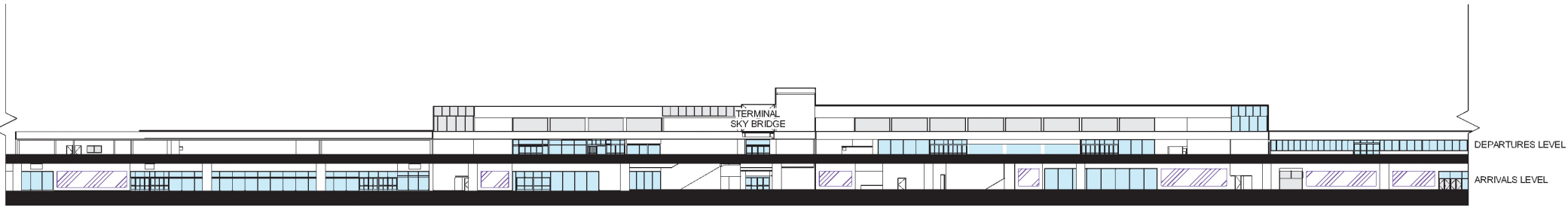


TERMINAL 6 ELEVATION - NORTH

1" = 80'-0"

- LEGEND**
- EXISTING GLASS AREAS
  - OPPORTUNITY ZONE



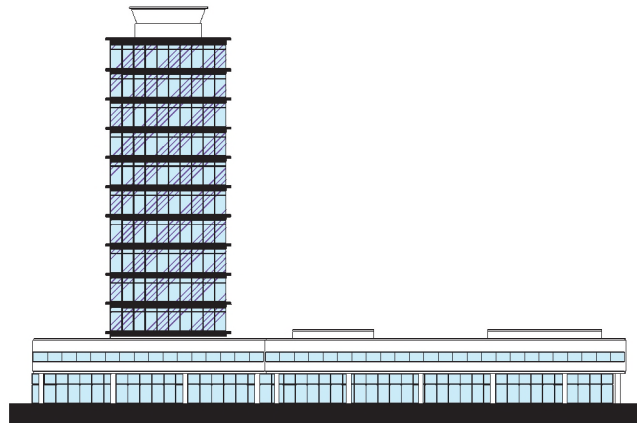


TERMINAL 7 ELEVATION - NORTH

1" = 80'-0"

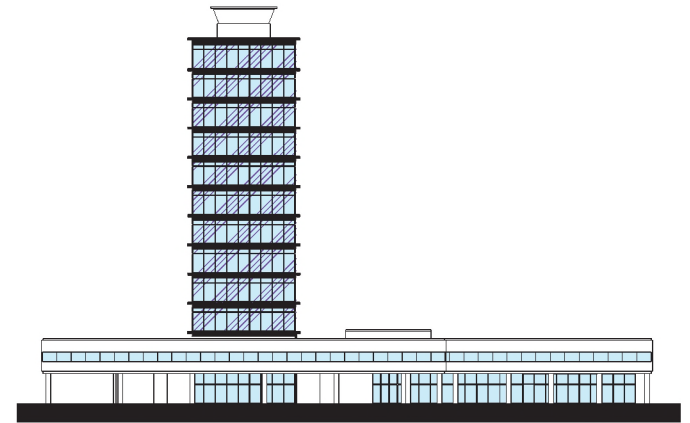
- LEGEND**
- EXISTING GLASS AREAS
  - OPPORTUNITY ZONE





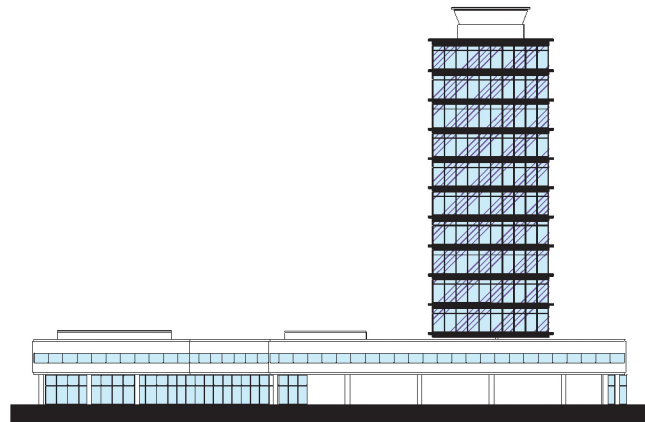
1961 ATCT ELEVATION - NORTH

1" = 80'-0"



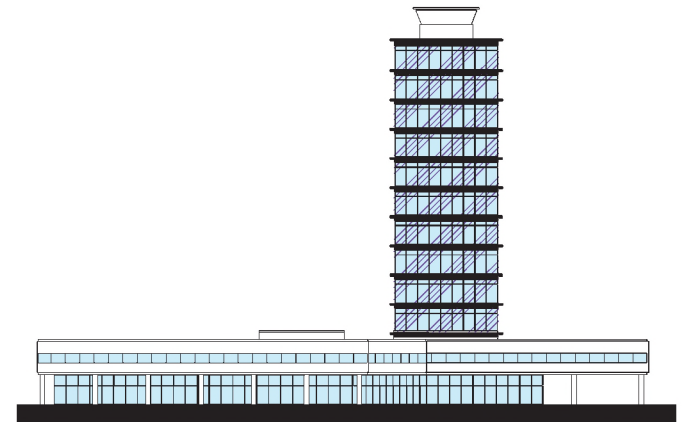
1961 ATCT ELEVATION - EAST

1" = 80'-0"



1961 ATCT ELEVATION - SOUTH

1" = 80'-0"



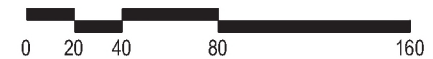
1961 ATCT ELEVATION - WEST

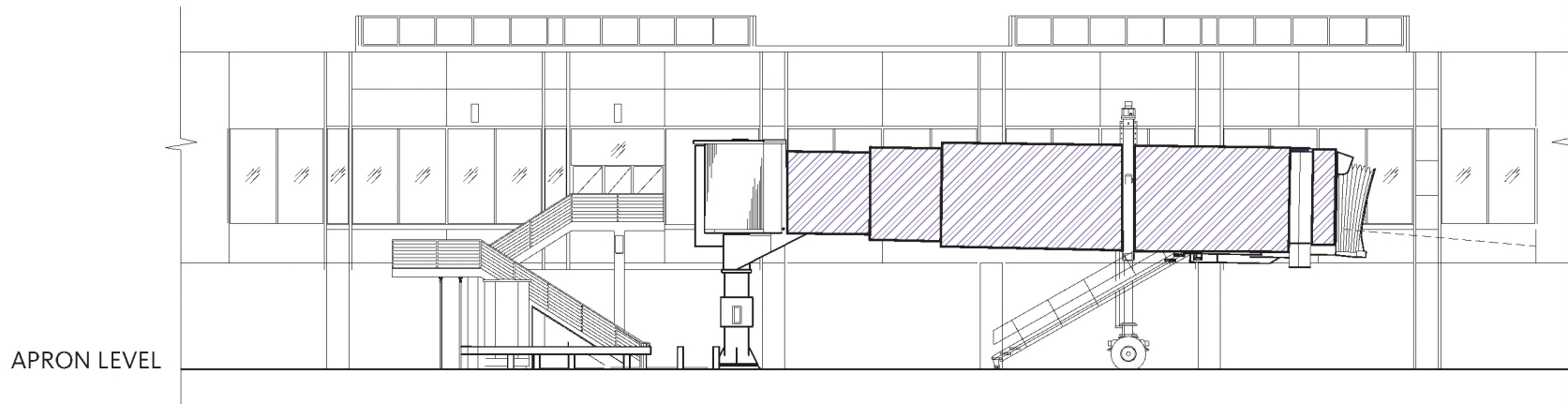
1" = 80'-0"

**LEGEND**

 EXISTING GLASS AREAS

 OPPORTUNITY ZONE

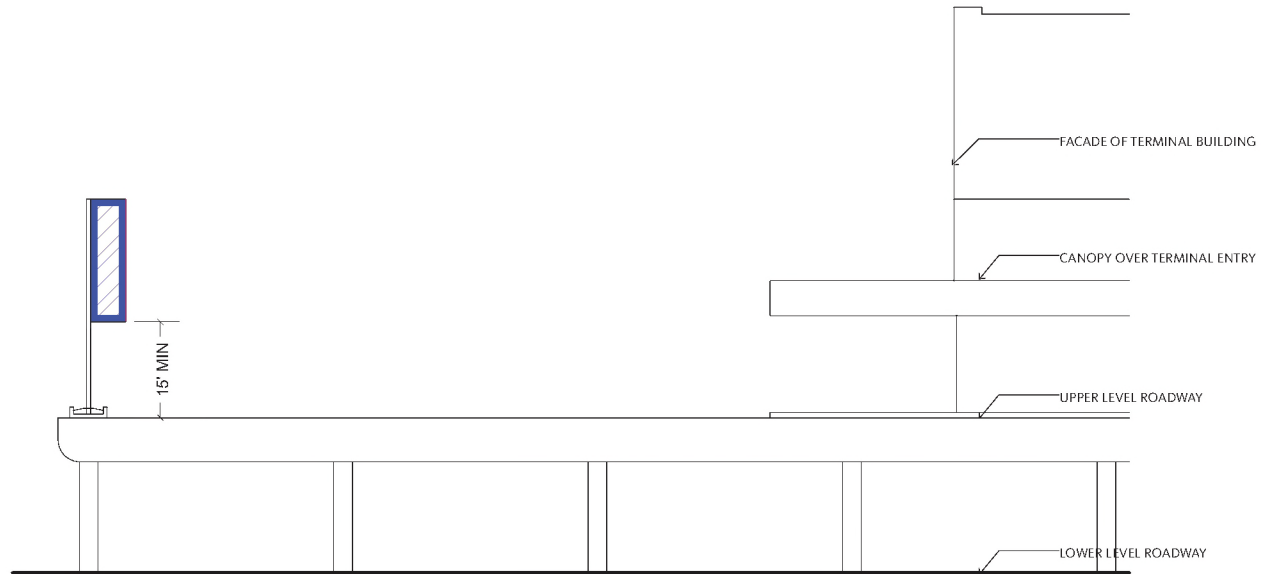




**LEGEND**

 OPPORTUNITY ZONE

DRAWING NOT TO SCALE



**LEGEND**

 OPPORTUNITY ZONE

DRAWING NOT TO SCALE