

# LOS ANGELES INTERNATIONAL AIRPORT (LAX)

## EXHIBIT A

## Specific Plan

Ordinance No. 176,345  
Effective January 20, 2005

As amended by Ordinance Nos. 179,148 and \_

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DCP recommends  
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Text in **BLUE** are amendments recommended by the Board of Airport Commissioners  
Text in **RED** are additional amendments recommended by Los Angeles World Airports

## **ORDINANCE NO. 182542**

An ordinance establishing a Specific Plan, known as the LAX Specific Plan, within the LAX Plan area.

### **THE PEOPLE OF THE CITY OF LOS ANGELES DO ORDAIN AS FOLLOWS:**

**Section 1. ESTABLISHMENT OF THE LAX SPECIFIC PLAN.** The City Council hereby establishes the LAX Specific Plan for the area bounded generally by 88<sup>th</sup> Street, Manchester Avenue, and 91<sup>st</sup> Street on the north, Imperial Highway on the south, Pershing Drive on the west, and La Cienega Boulevard on the east, as shown on Map 1.

#### **Sec. 2. PURPOSES.**

This Specific Plan is intended to:

1. Recognize the uniqueness of LAX as a regional economic engine, an international gateway to the Pacific Rim, and an important public amenity;
2. Provide regulatory controls and incentives for the systematic and incremental execution of the LAX Plan, an element of the General Plan, to provide for public needs, convenience and general welfare as the development of the airport necessitates;
3. Ensure the orderly development of infrastructure consistent with the intensity and design of the LAX Plan by establishing general procedures for development within the Specific Plan Area;
4. Promote the development of a regional airport system in Southern California through an improved regional ground access system;
5. Provide the appropriate zoning regulations for the development of the LAX Plan in conformance with the goals and objectives of other local and regional plans and policies;
6. Ensure the Los Angeles World Airport's ability to operate LAX safely and efficiently throughout implementation of the LAX Plan;
7. Recognize the important relationship between LAX and its neighbors and avoid development impacts to the extent practical and feasible;
8. Protect airport-related and community businesses by providing regulatory controls and incentives consistent with these goals; and



9. Ensure on-going participation in improvements to LAX by appropriate stakeholders- business, labor, community, airline industry trade groups, and government- through consultation with stakeholders.

### **Sec. 3. RELATIONSHIP TO THE LOS ANGELES MUNICIPAL CODE AND OTHER ORDINANCES.**

**A.** The regulations of this Specific Plan are in addition to those set forth in the planning and zoning provisions of the Los Angeles Municipal Code (LAMC), Chapter 1 as amended, and any other relevant ordinances, and do not convey any rights not otherwise granted under the provisions and procedures contained in the LAMC or other ordinances, except as specifically provided in this ordinance.

**B.** Wherever this Specific Plan contains provisions that establish regulations (including, but not limited to, standards such as densities, uses, parking, signage, open space and landscape requirements), which are different from, more restrictive or more permissive than would be allowed pursuant to the provisions contained in the LAMC or any other relevant ordinances, this Specific Plan shall prevail and supersede the applicable provisions of the LAMC and those relevant ordinances.

**C.** This Specific Plan shall supersede any and all "T" Tentative and "Q" Qualified classifications applicable to property within the Specific Plan Area prior to the adoption date of this Specific Plan, including those contained in Ordinance No. 159,526. The substance of the conditions in 159,526 is incorporated in this ordinance as Appendix A in Section 11 and the "LAX Northside Design Guidelines and Development Standards" and relate to property generally known as LAX Northside.

**D. Site Plan Review and "Major" Development Projects Ordinances.** Notwithstanding LAMC Sections 16.05 and 12.24 U 14, Site Plan Review and "Major" Development Projects approvals shall not be required within this Specific Plan Area.

**E. Specific Plan Procedures.** Notwithstanding LAMC Sections 11.5.7 B, 11.5.7 C, 11.5.7 D, 11.5.7 E, 11.5.7 H, and 11.5.7 J, Project Permit Compliance approvals, including modifications, adjustments and interpretations, shall not be required within ~~the Airport Airside and Airport Landside Sub Areas of~~ this Specific Plan Area.

**F. Mini-Shopping Centers and Commercial Corner Development Ordinance.** Notwithstanding LAMC Sections 12.22 A 23 and 12.24 W 27, Mini-Shopping Center and Commercial Comer Development approvals shall not be required within this Specific Plan Area.

**G. Landscape Ordinance.** The provisions of LAMC Sections 12.21 A 6, 12.40, 12.41, 12.42, and 12.43 shall not apply within the Specific Plan Area.

**H. Hotels When Located Within 500 Feet of an A or R Zone.** Notwithstanding LAMC Section 12.24 W 24, approvals for hotels located within 500 Feet of an A or R Zone shall not be required within this Specific Plan Area.

**I. Parking.** Where this Specific Plan contains language or standards that require more parking or permit less parking than LAMC Section 12.21 A 4, this Specific Plan shall supersede the LAMC. In addition, the requirements of Los Angeles Ordinance No. 182,386 shall not be required within this Specific Plan Area.

**J. Telecommunications.** Notwithstanding LAMC Section 12.24 W 49, approvals for Telecommunications Facilities shall not be required within this Specific Plan Area.

**K. Kennels When Located Within 500 Feet of a Residential Zone.** Notwithstanding LAMC Section 12.24 W 25, approvals for kennels or facilities for the boarding of dogs when located within 500 feet of any residential zone shall not be required within this Specific Plan Area.

**L. Citywide Design Guidelines.** Any ordinance amending LAMC Section 11.5.4 to require projects to comply with the Citywide Design Guidelines shall not apply within this Specific Plan Area.

#### **Sec. 4. APPLICATION OF SPECIFIC PLAN TO DEVELOPMENT IN SPECIFIC PLAN AREA.**

**A.** Except for the provisions of Section 7 [and 11](#), the regulations of this Specific Plan apply to all development located in the Specific Plan Area which requires a grading, building, mechanical, electrical, plumbing and/or sign permit. The provisions of Section 7 [and 11](#) apply only to Projects as defined in this Specific Plan.

**B.** No grading permit, building permit, or use of land permit shall be issued, and no construction shall occur, for any development within the Specific Plan Area unless the Executive Director has approved the proposed development.

**Sec. 5. DEFINITIONS.** Whenever the following terms are used in this Specific Plan, with the first letter or letters capitalized, they shall be construed as defined in this section. Words and phrases not defined in this section shall be construed as defined in LAMC Section 12.03.

~~**Administrative Clearance.** A ministerial process to determine compliance of a Project with all applicable provisions of this Specific Plan and the Exhibit "LAX Northside Design Guidelines and Standards" for the LAX Northside Sub-Area, as issued by the Executive Director pursuant to Section 11.~~

Note: LAWA/DCP recommends rejection of BOAC recommended text.

**Air Cargo Facility.** Buildings, including any associated equipment and vehicles, used for the storage and transport of cargo to and from commercial aircraft.

**Airport.** An area of land that is used or intended to be used for the landing and taking off of aircraft, and includes its buildings, facilities and ancillary uses, if any.

**Airport Airside Sub-Area.** That area as shown on Map 2.

**Airport Landside Sub-Area.** That area as shown on Map 2.



Note: LAWA/DCP recommends rejection of BOAC recommended text.

**Applicant.** Any entity, LAWA or any person, as defined in LAMC Section 11.01, submitting a request for LAX Plan Compliance Review or Administrative Clearance.

**Automated People Mover System (APM).** A rail or fixed guideway based transportation system or systems that moves passengers to and from the Central Terminal Area to the landside access facilities (CONRAC AND ITF) and other mass transportation facilities in an above-grade configuration, and to and from the Central Terminal Area to the Tom Bradley International Terminal and the Midfield Satellite Concourse in a below-grade configuration.

**BOAC.** The Board of Airport Commissioners of Los Angeles World Airports.

**Central Terminal Area (CTA).** The primary passenger check-in and processing center and the transition to and from landside facilities (ITF and CONRAC).

**CEQA.** California Environmental Quality Act.

**City Engineer.** The City Engineer of the Department of Public Works, Bureau of Engineering, or his or her designee.

**Commercial Passenger Vehicle Staging and Holding Area.** An area for taxis, rental car company, hotel/motel, door-to-door, and scheduled buses and vans, chartered buses and vans, etc., to wait before picking up passengers.

**Consolidated Rental Car Facility (CONRAC).** A consolidated facility to accommodate rental car operators at LAX. This facility may include a customer service facility, ready/return garage, rental car storage, and maintenance support.

**Director of Planning.** The Director of the City of Los Angeles Department of City Planning, or his or her designee.

Note: LAWA recommends rejection of BOAC recommended text.

**Executive Director.** The Executive Director of Los Angeles World Airports, or his or her designee.

**Federal Aviation Administration (FAA).** A federal agency charged with regulating air commerce to promote its safety and development, encouraging and developing civil aviation, air traffic control and air navigation, and promoting the development of a national system of airports.

**Fixed-Base Operators (FBO).** A person, firm or corporation authorized by issuance of a commercial operator's permit to provide aeronautical activities, services or products at the airport for compensation or hire.

**Fuel Farm.** Facility used for the storage of aircraft jet fuel prior to distribution to the aircraft via hydrant system or fueling trucks.

**Intermodal Transportation Facility (ITF).** A facility providing remote passenger pick up and drop off areas, public parking, and connections to public transit and other commercial vehicles (i.e. door-to-door shuttles and scheduled buses).

**LADOT.** The City of Los Angeles Department of Transportation.

**LADOT General Manager.** The General Manager of the City of Los Angeles Department of Transportation or his or her designee.

**LAMC.** Los Angeles Municipal Code.

**LAWA.** Los Angeles World Airports.

**LAX.** Los Angeles International Airport.

**LAX Master Plan Stakeholder Liaison.** A person appointed by BOAC to assist the board and other decision-makers in communicating with the stakeholders regarding LAX Master Plan decisions.

**LAX Northside.** The development project that is to be built out within the LAX Northside Sub-Area ~~previously the subject of Ordinance No. 159,526 and a Final Environmental Impact Report.~~

**LAX Northside Plan Update Mitigation Monitoring and Reporting Program (MMRP).** The program adopted by BOAC and City Council for applying, monitoring and reporting with respect to the LAX Specific Plan LAX Northside Sub-Area's commitments and mitigation measures as future site specific improvements are developed. The Board of Airport Commissioners may modify the program in accordance with CEQA.

**LAX Northside Sub-Area.** That area as shown on Map 2.

**LAX Plan.** The area plan adopted by City Council as the land use element of the City's General Plan for the airport and LAX Northside.

**LAX Plan Compliance Review.** A determination issued pursuant to Section 7 of this Specific Plan. ~~Approvals pursuant to Section 7 of this ordinance are not required for a Project within the LAX Northside Sub-Area of this Specific Plan.~~

Note: LAWA recommends reiection of BOAC/DCP recommended text.

**Midfield Satellite Concourse.** A component of the terminal facilities located west of the Central Terminal Area accessed via the Automated People Mover System. This component includes uses such as passenger waiting areas, aircraft loading and unloading, retail, and airline operations areas.

**Mitigation Monitoring and Reporting Program (MMRP).** The program adopted by BOAC and City Council for applying, monitoring and reporting with respect to the LAX

Plan's master plan commitments and mitigation measures as future site specific improvements and uses contemplated by the LAX Plan are developed. The Board of Airport Commissioners may modify the program in accordance with CEQA.

**Navigational Aid (Nav Aid).** Any facility used by an aircraft or its pilot for guiding or controlling flight in the air or the landing or takeoff of an aircraft.

**Project.** The construction, erection, addition to, or structural alteration of any building or structure, or use of building or land, or change of use of building or land located in whole or in part within the ~~Airport Airside or Airport Landside Sub Areas of the~~ Specific Plan Area.

A Project shall not include the following:

1. Interior or exterior remodeling of a building;
2. The change of use of a building or land, or the relocation of existing uses, if the change of use or relocation: (a) changes the footprint of a building or structure by 10% or less in square feet; or (b) increases the usable floor area by 10% or less;
3. Activity requiring building permits for the alteration of structures solely relating to mechanical, electrical, or plumbing work internal to the structure, fascia, or any combination of these;
4. Signs;
5. Fences/walls, retaining walls, or support structures;
6. The placement of Navigational Aids and any other equipment mandated by the FAA, TSA, or any other governmental agency;
7. Improvements mandated by the FAA, TSA, or any other governmental agency;
8. Airline operation activities;
9. Non-Master Plan Project infrastructure upgrades, airport maintenance activities, and repair of existing facilities approved by BOAC;
10. Temporary uses, for a period not to exceed 120 days;
11. Emergency uses and/or activities in response to safety and security issues (i.e., activities required by heightened security levels or unsafe operating conditions);
12. Special events, as authorized by the Executive Director or BOAC;



13. Notwithstanding LAMC Section 12.23 A 4, the rehabilitation or reconstruction of a conforming or nonconforming building or structure that was damaged or destroyed by fire, flood, wind, earthquake, or other natural or man-made disaster;

14. Any construction required in order to comply with an order issued by the U.S. Department of Transportation, the FAA or the Department of Building and Safety to repair or replace an unsafe or substandard condition;

15. Construction, the value of which is within the jurisdiction of the Executive Director to approve and does not need to be approved by BOAC under Los Angeles Administrative Code Section 10.1.1 and BOAC Resolution No. 19593 or any subsequent amendment; or

16. Security, safety and/or utility improvements to existing facilities and/or new security, safety and/or utility facilities.

17. Underground stormwater infiltration systems and facilities in order to comply with State or Federal mandatory environmental requirements or compliance.

**Run-up Enclosure.** Specialty facility used to test aircraft engines and disperse sound to reduce noise impacts on surrounding areas.

**Runway.** A defined rectangular area on the airport used to prepare for the takeoff or landing of aircraft along its length.

**Specific Plan Area.** That area within the heavy dashed lines on Map 1 in this Specific Plan.

**Taxiway.** A specially designated and prepared surface on an airport, including a taxilane, for aircraft to taxi to and from Runways, hangars, and aircraft parking positions.

**Trip.** A vehicle trip will be administratively defined by agreement between the General Manager of LADOT and LAWA to include the entrance or exit of a vehicle from airport or airport-related property.

**Sec. 6. SAFETY OF AIRPORT OPERATIONS.** Notwithstanding any other provision of this Specific Plan, no use, development or activity within the Specific Plan Area may compromise the safety of airport flight operations in any way. Final authority for determining whether airport flight operation safety is compromised rests solely with the U.S. Department of Transportation and the FAA.

## **Sec. 7. LAX PLAN COMPLIANCE REVIEW.**

**A. General.** The provisions of Subsections B, C, D, E, H of LAMC Section 11.5.7 do not apply to any Projects proposed for construction within the ~~Airport Airside and Airport Landside Sub-Areas of the LAX~~ Specific Plan Area.

**B. Prohibition.** No grading permit, building permit, or use of land permit shall be issued, and no construction shall occur, on any Project within the LAX Northside, Airport Airside and Airport Landside Sub-Areas unless the City Council grants an LAX Plan Compliance approval pursuant to the procedures set forth in this section.

**C. Recommendation by Executive Director.** The Executive Director shall have the authority to recommend approval, approval with conditions, modification or denial of a request for an LAX Plan Compliance determination. This recommendation shall be made to BOAC and the City Council pursuant to the procedures set forth in this section after consideration of the traffic generation report and aviation activity analysis required in Subsection G 1 below and following consultation with the LAX Master Plan Stakeholder Liaison.

**D. Findings.** The Executive Director shall recommend to BOAC that the proposed Project be granted an LAX Plan Compliance approval upon written findings that the Project satisfies each of the following requirements:

**1. LAX Plan Consistency.** That the Project complies with the LAX Plan, any design guidelines required by the LAX Plan, and all applicable provisions of this Specific Plan; and

**2. Environmental Compliance.** That the Project has been adequately analyzed in compliance with CEQA, and the applicable master plan commitments and mitigation measures contained in the MMRP (as may be modified by BOAC in accordance with CEQA) or identified in any subsequent environmental review have been incorporated into the Project to the extent feasible.

**E. Rights Granted Under LAX Plan Compliance.** The issuance of an LAX Plan Compliance approval indicates compliance with the LAX Plan and this Specific Plan, but does not in any way indicate compliance with other applicable provisions of LAMC Chapter I (Planning and Zoning Code), nor with Chapter IX (Building Code).

**F. Procedures.**

1. The Executive Director shall review a Project for LAX Plan Compliance based upon the following information:

(a) A written description of the Project including location, size, proposed use, and any other pertinent information;

(b) A completed initial environmental study, including a traffic study, where appropriate, or other analysis;

(c) The most recent annual traffic generation report as required in Subsection G 1 below; and

(d) The most recent annual aviation activity analysis as required in Subsection G 1 below, except for projects located within the LAX Northside.

## **2. Executive Director's Review.**

(a) Upon receipt of a request for review, the Executive Director shall transmit a copy of the written description of the Project and appropriate documents to the LADOT General Manager, the City Engineer, the Councilmember of the district in which the Specific Plan Area is located and the LAX Master Plan Stakeholder Liaison, and post notice of the application on the LAWA website. The LADOT General Manager and the City Engineer shall submit any written comments concerning parking, driveways, access, circulation, and infrastructure improvements to the Executive Director within 15 working days from the date the documents were received, unless the LADOT General Manager and the Executive Director agree more time is necessary. The Executive Director shall ensure that LAWA communicates with the LAX Master Plan Stakeholder Liaison. The Executive Director shall consider the comments and concerns of the stakeholders as early in the process as reasonable.

(b) The Executive Director shall determine whether the Project complies with the LAX Plan and all applicable provisions of this Specific Plan.

(c) The Executive Director shall determine whether the environmental clearance for the Project complies with CEQA.

(d) If the Executive Director determines that the Project is consistent with the LAX Plan, all applicable provisions of this Specific Plan and with the requirements of CEQA, the Executive Director shall prepare a written report and transmit this report to BOAC for its action on the LAX Plan Compliance request. This written report shall include findings to support the Executive Director's recommendation, the applicable master plan commitments and mitigation measures, the applicable mitigation measures identified in any subsequent environmental review, the applicable traffic improvements and right-of-way dedications, and any conditions of approval that shall be imposed on the Project. As a part of this written report, the Executive Director shall summarize the traffic generation report and aviation activity analysis required in Subsection G 1 below, and the results of the consultation with the LAX Master Plan Stakeholder Liaison. The Executive Director shall also attach the reports submitted by the LADOT General Manager and the City Engineer.

(e) If the Executive Director determines that the Project is not consistent with the LAX Plan and all applicable provisions of this Specific Plan, the Executive Director may direct staff to reconsider the Project, analyze or redesign the Project, or recommend that BOAC seek an amendment to the LAX Plan and/or an amendment or exception to the Specific Plan pursuant to LAMC Sections 11.5.6 and/or 11.5.7 F and G, as appropriate.

**3. Notice Requirements for BOAC Hearing.** After receipt of the Executive Director's report and recommendation, BOAC shall set the matter for hearing. Notice of the time, place, and purpose of the hearing shall be given in the following manner:



(a) By posting the BOAC meeting agenda in the LAWA offices, on the LAWA website, and on the City of Los Angeles website at least 72 hours prior to the meeting.

(b) For the CTA, ITF, CONRAC, Midfield Satellite Concourse, and APM (except that portion that connects the CTA with Tom Bradley International Terminal and the Midfield Satellite Concourse) Projects, notice shall also be given by sending written notice by First Class Mail at least 15 days prior to the meeting date to: the Department of City Planning; the Department of Building and Safety; the Councilmember(s) of the district in which the Specific Plan Area is located; LADOT, the Bureau of Engineering; the LAX Master Plan Stakeholder Liaison; the Department of Neighborhood Empowerment; the West Los Angeles Area Planning Commission; the president(s) of local neighborhood councils; the Airlines for America trade association, Airlines Airport Affairs Committee, and any other airline industry trade groups identified by the LAX Master Plan Stakeholder Liaison as stakeholders; the LAX Coastal Chamber of Commerce; and local homeowners groups and interested parties who have filed a written request with LAWA.

**4. Recommendation by BOAC.** BOAC shall recommend to City Council that it approve, approve with conditions, modify or deny a request for LAX Plan Compliance. BOAC shall make the same findings required to be made by the Executive Director, supported by facts in the record. BOAC shall recommend that all appropriate master plan commitments and mitigation measures, the applicable mitigation measures identified in any subsequent environmental review, and any other requirements are included as a condition of the approval.

**5. City Council Determination.** City Council shall approve, approve with conditions, modify or deny a request for LAX Plan Compliance. The City Council shall make the same findings required to be made by the Executive Director, supported by facts in the record. The City Council shall ensure that all appropriate master plan commitments and mitigation measures, the applicable mitigation measures identified in any subsequent environmental review, and any other requirements are included as a condition of the approval. Except for the Projects referred to as the Intermodal Transportation Facility, Automated People Mover System, and North Runway configuration, if the City Council does not take final action on the application for an LAX Plan Compliance determination within 45 days of receipt of the recommendation from BOAC, the recommendation from BOAC shall become the final decision on the application.

## **G. Monitoring and Reporting.**

1. LAWA shall prepare and submit annual reports to BOAC, the Department of City Planning, LADOT and City Council for the following:

**(a) Traffic Generation Report.** A traffic report, based on the information required by Section 12 C 1, that identifies the current number of Trips being generated by LAX (inclusive of all three Sub-Areas), the number of Trips anticipated to be generated at the completion of any Master Plan Project(s) in development at the time of the report, the Trips proposed to be generated following the full implementation of the Master Plan as informed by current and Project-based Trip counts, and the number of Trips anticipated to be generated by on-going Master Plan construction activities.

**(b) Aviation Activity Analysis.** An aviation activity analysis that identifies the current number of passengers, volume of air cargo and aircraft operations served at LAX, and the volume of aviation activity anticipated to be served by on-going Master Plan construction activities.

In order to monitor that regional aviation system improvements are taking place in a timely manner, LAWA will also compile aviation activity statistics for other airports in the Los Angeles region for monitoring and reporting purposes. Passengers, volume of air cargo and aircraft operations activity at all airports with scheduled passenger or cargo activity in Los Angeles, Orange, Riverside, San Bernardino and Ventura counties will be compiled in coordination with the Southern California Association of Governments (SCAG). The analysis shall also include the proportion of aviation activity served at each airport in the region.

**(c) Mitigation Monitoring and Reporting Program.** A status report on compliance with Master Plan commitments and mitigation measures contained in the Mitigation Monitoring and Reporting Program.

2. The first of each annual report required in Subdivision 1 shall be submitted one year following the City Council's approval of the LAX Plan and MMRP, and the last report shall be submitted at the end of the year following completion of any pending Master Plan Project.

#### **H. Additional Study Requirements.**

1. **Specific Plan Amendment Study.** LAWA shall initiate a Specific Plan Amendment Study with corresponding environmental analysis in compliance with CEQA, in the following two circumstances:

**(a)** If the annual traffic generation report required in Subsection G 1 above, and/or the annual traffic generation report considered together with any Project-specific traffic study, shows that any Master Plan Projects will be generating net new airport peak hour Trips in excess of 8,236 (unless the total Trips for that year are related to construction or phasing impacts).

**(b)** If the annual aviation activity analysis required in Subsection G 1 above forecasts that the annual passengers for that year are anticipated to exceed 78.9 million.



2. **LAX Domestic Passenger and Airline Market Survey/Study.** LAWA shall initiate an LAX Domestic Passenger Survey/Study and corresponding Airline Survey/Study, if the annual aviation activity analysis required in Section G 1 above forecasts that the annual passengers for that year are anticipated to exceed 78.9 million.

(a) **LAX Domestic Passenger Survey and Study.** LAWA shall conduct a survey and study of LAX domestic passengers (those passengers not flying internationally or connecting to international flights) designed to identify, at a minimum, (i) those LAX domestic passengers with origination or destination locations closer to other commercial airports in the region; (ii) why those domestic passengers chose to fly out of, or into, LAX rather than another commercial airport closer to their location of origin or destination; and (iii) what actions, consistent with federal, state and local laws, LAWA could take to encourage those domestic passengers to use an airport closer to their location of origin or destination for domestic flights.

(b) **Airline Survey and Study.** Upon completion of the LAX Domestic Passenger Survey and Study described in 2(a) above, LAWA shall conduct a survey and study of airlines then serving the Southern California commercial air travel market designed to identify what action(s), consistent with federal, state and local laws, LAWA could take to encourage those airlines to provide increased domestic service at other airports in the region, particularly those owned or operated by LAWA.

I. **LAX Master Plan Stakeholder Liaison.** The LAX Master Plan Stakeholder Liaison shall communicate with the stakeholders on Master Plan issues. The LAX Master Plan Stakeholder Liaison shall ensure that notice is provided to the stakeholders at the earliest reasonable time of initiation of Projects and any Specific Plan Amendment Study.

## **Sec. 8. LAND USE.**

A. **Designation of Sub-Areas.** The Specific Plan is divided into three Sub-Areas, as shown on Map 2. The Sub-Areas are designated as: Airport Airside, Airport Landside, and LAX Northside.

B. **Unified Development.** For purposes of applying building ordinances and regulations, as well as the regulations of this Specific Plan, to the Airport Airside and Airport Landside Sub-Areas, these areas may be treated as a unified development (or unified developments).

1. A unified development shall mean an area or development that:

(a) has a combination of functional linkages, such as pedestrian or vehicular connections; and

(b) is composed of two or more contiguous parcels or lots of record, except where divided by a public street (or streets).

2. Before applying building ordinances and regulations to any area or

development within the Airport Airside and Airport Landside Sub-Areas, the Department of Building and Safety shall determine whether or not the area or development is a unified development, or part of a unified development, as defined in this subsection. The following provisions may be applied to any area or development determined to be a unified development:

**(a) Location of Parking Area.** Notwithstanding LAMC Section 12.21 A 4(g), parking to serve the area or development may be located at any location within the Airport Airside and Airport Landside Sub-areas. Further, the provisions of LAMC Section 12.26 E 5 shall not apply;

**(b) Floor Area Ratio (FAR).** The total buildable area of all parcels may be used to determine FAR;

**(c) Building Site.** Construction of a building and/or structure may be across common interior property lines;

**(d) Driveways, Entry/Exiting, and Drainage.** Driveway access, building entry and exiting, and drainage may be across common interior property lines;

**(e) Fire Resistance and Opening Protection of Exterior Walls.** The requirements for fire resistance and opening protection of exterior walls are not applicable to the common interior property lines. The property line abutting public roadways or adjacent to properties outside of the boundaries of the unified development is not a common interior property line;

**(f) Construction and Maintenance.** The construction and maintenance of all buildings, structures, and site improvements within each unified development shall be in accordance with all building ordinances and regulations that would otherwise be applicable if the buildings, structures, and improvements were located on or within a single lot.

**C. Yard and Setback Regulations.** Notwithstanding the provisions set forth in the LAMC, no front, side, or rear yards or building setbacks are required in the Airport Airside or Airport Landside Sub-Areas. Landscape buffers shall be consistent with any street frontage and landscape guidelines as may be required by the LAX Plan and adopted by BOAC. These landscape buffers shall not be measured from lot lines, but rather from perimeter roadways or as specified otherwise in any adopted street frontage and landscape guidelines. Development in the LAX Northside Sub-Area shall provide setbacks and landscape buffers as specified in [Appendix A Section 11 and the Exhibit "LAX Northside Design Guidelines and Development Standards."](#)

## **Sec. 9. AIRPORT AIRSIDE SUB-AREA.**

**A. Purpose.** To allow for the safe and efficient operation of airport airfield activities, Aircraft are permitted to operate under power in this Sub-Area, except as prohibited in Subsection D below.

**B. Permitted Uses.** The following uses shall be permitted in the Airport Airside Sub-Area, also designated as the LAX-A Zone, within the Specific Plan Area, subject to approval by the Executive Director:

1. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14, including, but not limited to:

- (a) Airline clubs, retail uses, and restaurants;
- (b) Aviation school;
- (c) Establishments for the sale and service of alcoholic beverages for on-site and off-site consumption shall be allowed as permitted by and pursuant to any application procedures set forth in the LAMC;
- (d) Incidental retail uses- permanent or temporary retail uses, which may include kiosks and carts; and
- (e) Surface and structured parking lots (including those at-grade, above-grade, and subterranean).

2. All of the uses permitted in the M2 Zone, as specified in LAMC Section 12.19, including, but not limited to:

- (a) Aircraft under power;
- (b) Airline catering and food preparation;
- (c) Airline maintenance and support, including but not limited to storage, aircraft engine or airframe repair and testing, and aircraft maintenance shops;
- (d) Air Cargo Facilities;
- (e) Commercial Passenger Vehicle Staging and Holding Area;
- (f) Fixed-Base Operators;
- (g) Helicopter operations, including but not limited to helicopter landings and take-offs, helipads, heliports, and helistops;
- (h) Navigational Aids and any other equipment mandated by the FAA, TSA, or any other governmental agency;
- (i) Run-up Enclosures;
- (j) Runways, Taxiways, aircraft parking aprons, and service roads; and
- (k) Passenger handling facilities, including but not limited to baggage handling and processing, passenger holdrooms, boarding gates, ticketing, and passenger check-in functions.

3. In addition, the following uses shall be permitted:

- (a) Aggregate/asphalt grinding and recycling facility;
- (b) Airport police fire arms training facility;
- (c) Aircraft rescue, firefighting and training facilities;
- (d) Automated People Mover System, its stations and related facilities;
- (e) Compressed Natural Gas/Liquefied Natural Gas ("CNG/LNG") stations, central utility plant, and other fueling and energy sources;
- (f) Fuel Farm;
- (g) Hazardous waste storage;
- (h) Hydrogen cell;



- (i) Oil drilling for remediation purposes only;
- (j) Security-related equipment and facilities;
- (k) Surface water runoff treatment plant;
- (l) Uses customarily incident to any of the above uses, and accessory buildings or uses;
- (m) Uses and operations determined to be of a similar nature, or deemed necessary for the safe and efficient operation of the airport, by the Executive Director; and
- (n) All uses requiring a Conditional Use Permit pursuant to LAMC Section 12.24.

### **C. Prohibited Uses.**

The following uses shall be prohibited within the LAX-A Zone:  
Any building containing dwelling units.

**D. Imperial Terminal Area.** The Imperial Terminal Area is the approximately 42.5-acre area north of Imperial Highway between Main Street and California Street, as shown on Map 2. In this area, aircraft maneuvering may be conducted by tug and tow procedures. The use regulations, for both permitted and prohibited uses, specified in this section for the LAX-A Zone shall apply, except the following uses shall be prohibited:

- 1. Aircraft under power; and
- 2. Helicopter operations, including but not limited to helicopter landings and take-offs, helipads, heliports, and helistops.

## **Sec. 10. AIRPORT LANDSIDE SUB-AREA.**

**A. Purpose.** To allow for the safe and efficient operation of airport facilities, the primary function of which is to provide access to the airport and process passengers. Aircraft are not permitted to operate under power in this Sub-Area.

**B. Permitted Uses.** The following uses shall be permitted in the Airport Landside Sub-Area, also designated as the LAX-L Zone, within the Specific Plan Area, subject to approval by the Executive Director:

- 1. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14, including, but not limited to:

- (a) Airline clubs, retail uses, and restaurants;
- (b) Aviation school;
- (c) Establishments for the sale and service of alcoholic beverages for on-site and off-site consumption shall be allowed as permitted by and pursuant to any applicable procedures set forth in the LAMC;
- (d) Rental car operations, including but not limited to vehicle maintenance and car washing;

- (e) Incidental retail uses- permanent or temporary retail uses, which may include kiosks and carts; and
- (f) Surface and structured parking lots (including those at-grade, above-grade, and subterranean).

2. All of the uses permitted in the M2 Zone, as specified in LAMC Section 12.19, including, but not limited to:

- (a) Airline catering and food preparation;
- (b) Airline maintenance and support, including but not limited to storage, aircraft engine or airframe repair and testing, and aircraft maintenance shops;
- (c) Air Cargo Facilities;
- (d) Commercial Passenger Vehicle Staging and Holding Area;
- (e) Helicopter operations, including but not limited to helicopter landings and take-offs, helipads, heliports, and helistops;
- (f) Navigational Aids and any other equipment mandated by the FAA, TSA, or any other governmental agency;
- (g) Passenger handling facilities, including but not limited to baggage handling and processing, passenger holdrooms, boarding gates, ticketing, and
- (h) Service roads.

3. In addition, the following uses shall be permitted:

- (a) Passenger pick up and drop off areas;
- (b) Automated People Mover System, its stations and related facilities;
- (c) CNG/LNG stations, central utility plant, and other fueling and energy sources;
- (d) Security-related equipment and facilities;
- (e) Uses customarily incident to any of the above uses, and accessory buildings and uses;
- (f) Uses and operations determined to be of a similar nature, or deemed necessary for the safe and efficient operation of the airport, by the Executive Director; and
- (g) All uses requiring a Conditional Use Permit pursuant to LAMC Section 12.24. Notwithstanding the foregoing, as specified above, all establishments for the sale and service of alcoholic beverages shall be allowed as permitted by and pursuant to any applicable procedures set forth in the LAMC.

### **C. Prohibited Uses.**

The following uses shall be prohibited within the LAX-L Zone:

- (a) Aircraft under power; and
- (b) Any building containing dwelling units.



## Sec. 11. LAX NORTHSIDE SUB-AREA.

**A. Purpose.** To provide for the redevelopment of land previously used for residential purposes with uses that are consistent with airport needs and neighborhood conditions. This Sub-Area serves as an airport buffer zone for the Westchester community.

**B. General Area Districts.** The LAX Northside Sub-Area has three Districts (the LAX Northside Campus District, LAX Northside Center District, and LAX Northside Airport Support District) and fifteen areas- Areas 1, 2, 3, 4 through 11, 12A East, 12A West, and 13 fifteen areas—Areas 1, 2, 3, 4A, 4B, 5 through 11, 12A, 12B and 13 - as shown on Map 3.

**C. Relationship to Section 7 and the LAMC.** The provisions of Section 7 or LAMC Section do not apply to a Project within the LAX Northside Sub-Area. The provisions of this Section 7 of this Specific Plan shall replace the Project Permit compliance provisions in LAMC Section 11.5.7., however, requests for Exceptions from Specific Plans shall comply with LAMC Section 11.5.7 F.

**DC. Relationship to Ordinance No. 159,526 Modification Procedures.** The LAX Northside Sub-Area, also designated as the LAX-N Zone, is comprised of property previously entitled under Ordinance No. 159,526 with the "Design Guidelines for LAX Northside" prepared by Albert Martin and Associates, dated April 20, 1989, and are hereby superseded by The limitations, including permitted uses, requirements and conditions of development in Ordinance No. 159,526 are incorporated into the Specific Plan as Appendix A. The requirements in Appendix A shall apply to all development in this Sub-Area, this ordinance, and guided by the referenced Exhibit is guided by the "LAX Northside Design Guidelines and Standards," as may be amended by BOAC from time to time. Los Angeles World Airports (LAWA) shall notify the Department of City Planning, the Council Office and local Neighborhood Council(s) at least 45 days prior to the proposed BOAC Board Meeting for which changes to the Exhibit "LAX Northside Design Guidelines and Standards" is agendized.

**E. Administrative Clearance.** The procedures set forth in this Subsection shall apply to all Projects in the LAX Northside Sub-Area.

LAWA /DCP recommend that the Administrative Clearance procedure (Subsection E) be replaced with the LAX Plan Compliance procedure (See subsection C).

1. Except as provided herein, no grading permit, foundation permit, building permit, use of land permit, or permit for a change of use shall be issued for any Project on any lot located in whole or in part within the LAX Northside Sub-Area unless an Administrative Clearance has been issued pursuant to the procedures set forth in this Section.

(a) No Administrative Clearance shall be approved for a Project that would cause overall development in the LAX Northside Sub-Area to exceed the Site Development Standards in Subsection G Site Development Standards below.

2. Executive Director's Authority. The Executive Director shall have the authority to review and approve each Project for compliance with all applicable provisions of this Specific Plan and the Exhibit "LAX Northside Design Guidelines and Standards" if in compliance, to grant an Administrative Clearance.



~~(a) **Procedures.** The Executive Director shall establish the appropriate procedures, forms and fees required for the Administrative Clearance application.~~

~~(b.) **Ministerial Review.** The Administrative Clearance shall be a ministerial review of the applicable provisions of this Specific Plan and determination of whether a Project is in conformance with this Specific Plan, as applicable, including the Exhibit "LAX Northside Design Guidelines and Standards."~~

~~(c.) **Department of City Planning Review.** The Director of Planning shall review the application and determine, by signature, that the Project complies with Subsection G Site Development Standards below.~~

**E. Permitted Uses.** The following uses shall be permitted in the LAX Northside Sub-Area, also designated as the LAX-N Zone, within the Specific Plan Area:

**1. Buffer Uses (B)**

(a) Undeveloped, landscape buffer areas

(b) Underground infrastructure and utilities, with limited related surface structures

(c) Ancillary uses and structures related to the primary permitted uses

**2. Community & Civic Uses (CC)**

(a) Non-profit business or institutions that serve the local community

(b) Outside recreation related to non-profit institutions such as swimming pools and athletic courts / fields

(c) City, county or state government buildings including senior center, police stations, fire stations, and libraries

(d) Cultural institutions such as performing arts or museums

(e) Other community-serving uses

(f) Non-profit club or lodge

(g) Underground infrastructure and utilities, with limited related surface structures

(h) Ancillary uses and structures related to the primary permitted uses

(i) Farmer's Market

**3. Office & Research Uses (OR)**

(a) Community & Civic (CC) land uses

(b) Office, general business or professional

(c) Medical or dental Office, including surgery center, outpatient services, primary care clinic, and pharmacy

DCP  
recommends  
that Section E,  
Permitted Uses  
be modified.  
Please see the  
recommended  
text below  
preceding  
Section F.

DCP  
recommends  
that Section E,  
Permitted Uses  
be modified.  
Please see the  
recommended  
text below  
preceding  
Section F.

(d) Research and development including office, engineering, showroom, laboratory. Limited test and assembly of not-for-sale prototypes is permitted

(e) Business college, professional or scientific school or college, not including trade schools involving shop work, or the repair or maintenance of machinery or equipment

(f) Media, post-production, motion picture, or broadcast studio, without transmission towers

(g) Animal care, kennel, or boarding facility

(h) Underground infrastructure and utilities, with limited related surface structures

(i) All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14

(j) Farmer's market

(k) Ancillary uses and structures related to the primary permitted uses

#### 4. Recreation & Open Space Uses (OS)

a. Golf course

b. Athletic fields, outdoor athletic courts

c. Public shade structures, picnic areas, and restrooms

d. Dog park

e. Below grade storm water treatment facilities

f. Underground infrastructure and utilities, with limited related surface structures

g. Farmer's markets

h. Ancillary uses and structures related to the primary permitted uses

#### 5. Mixed Use-Commercial Uses (MU)

a. Banks or financial institutions

b. Retail, including merchandise sales and local services such as bakery, barber shop, beauty shop, book store, stationary store, software or computer Store, toy store, clothes cleaners, tailors, florist or gift shop, real estate, hardware or appliance store, or jewelry store.

c. Professional office

d. Restaurants, including sit down, quick serve, and drive through

e. Hotel, including related restaurants, services, and parking

f. Transit station

g. Medical or dental office, including surgery center, outpatient services, primary care clinic, and pharmacy

h. Animal medical clinic

i. Parking

j. Wholesale stores

k. Auto repair or service



DCP  
recommends  
that Section E,  
Permitted  
Uses be  
modified.  
Please see the  
recommended  
text below  
preceding  
Section F.

l. Auto fueling stations

m. Farmer's market

n. Underground infrastructure and utilities, with limited related surface structures

o. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14

p. Ancillary uses and structures related to the primary permitted uses

#### 6. Airport Support Uses (AS)

a. Maintenance and repair shops

b. Indoor storage and warehouses

c. Exterior storage

d. Administrative offices

e. Radars and surveillance facilities

f. Utilities and utility-related structures

g. Construction material temporary storage

h. Recycling sorting and storage

i. Parking

j. Underground infrastructure and utilities, with limited related surface structures

k. All of the uses permitted in the M2 Zone, as specified in LAMC Section 12.19

l. Ancillary uses and structures related to the primary permitted uses

7. In addition, establishments for the sale and service of alcoholic beverages for on-site and off-site consumption shall be allowed as permitted by and pursuant to any application procedures set forth in the LAMC.

DCP recommends that following text for Section E replace the above text recommended by LAWA:

**E. Permitted Uses.** The following land use categories shall be permitted in the LAX Northside Sub-Area, also designated as the LAX-N Zone, within the Specific Plan Area, subject to approval by the Executive Director. For a more detailed list of permitted uses for each land use category, see the "LAX Northside Design Guidelines and Standards".

1. Recreation and Open Space
2. Office, Research and Development
3. Community and Civic
4. Mixed Use – Commercial
5. Airport Support
6. Buffer
7. Uses permitted by and pursuant to the procedures set forth in LAMC Section 12.24W

**F. Prohibited Uses.** The following uses shall be prohibited in the LAX Northside Sub-Area, also designated as the LAX-N Zone, within the Specific Plan Area:

1. Residential, or dwelling units of any kind
2. K-12 education
3. A retail store over 100,000 gross square feet of floor area
4. Auto dealerships
5. Adult business as defined in LAMC 12.70
6. Massage parlors as defined in LAMC 12.70
7. Parking as a primary use, except in the Airport Support and Mixed Use-Commercial permitted use categories Sub-Zones
8. Hazardous materials testing
9. Aircraft under power

**G. Site Development Standards.** Land uses, setbacks, building orientation, heights, and floor area shall comply with the Site Development Standards contained in Table 1, Section 11 of this Specific Plan, and the "LAX Northside Design Guidelines and Standards."

1. **Total Floor Area Permitted.** The Total Floor area of all development within the LAX Northside Sub- Area shall not exceed 2,320,000 square feet.

2. **Intensity Allocation.** The total floor area within the LAX Northside District shall not exceed the following:

- (a) 1,075,000 sf of Floor Area for Campus District (Areas 1, 2, 2A, 2B, 2C, 2D, 2E and 3)
- (b) 645,000 sf of Floor Area for Center District (Areas 11, 12A East, 12A West, 12B and 13)
- (c) 600,000 sf of Floor Area for Airport Support District (Areas 4 - 10)

DCP Recommends an additional Site Development Standard Number 3



3. **Undeveloped Parcels.** At the time any District reaches the maximum allowable floor area permitted, any undeveloped parcels shall be landscaped and maintained pursuant to Section 7 of the LAX Northside Design Guidelines and Standards.

**Table 4**  
**LAX Northside Sub-Area Development Standards**  
**Permitted Land Uses, Heights, and Floor Area**

Area	Permitted Land Use Category <sup>a</sup>	Maximum Height From Grade	Net New Floor Area
<b>LAX Northside Campus District</b>			
	Open Space and Recreation		
Area 1	Office, Research and Development	45'	10,000 sf
Areas 2A, 2B, 2C, 2D,	Office, Research and Development	Area 2: 45'	1,065,000 sf



**Table 1**  
**LAX Northside Sub-Area Development Standards**  
**Permitted Land Uses, Heights, and Floor Area**

Area	Permitted Land Use Category <sup>a</sup>	Maximum Height From Grade	Net New Floor Area
2E & 3	Community and Civic Open Space and Recreation	Area 3: 60'	
<b>LAX Northside Center District</b>			
	Mixed Commercial Use		
Areas 11, 12A East	Community and Civic	60'	470,000 sf
Area 12A West	Community and Civic	20'	130,000 sf
Area 12B	Open Space and Recreation	N/A	N/A
Area 13	Community and Civic	45'	45,000 sf
<b>Airport Support District</b>			
Areas 4-10	Airport Support	30'	600,000 sf
<b>Total</b>			<b>2,320,000 sf</b>

**Table 1**  
**LAX Northside Sub Area Development Standards**

District (Areas)	Permitted Land Use Category <sup>a</sup>	Maximum Height from Grade <sup>b</sup>	Net New Floor Area
<u>LAX Northside Campus District (Area 1, 2, 3)</u>	<u>Open Space and Recreation</u> <u>Community and Civic</u> <u>Buffer</u> <u>Office, Research and Development</u>	<u>60'</u>	<u>1,075,000</u>
<u>LAX Northside Center District (Area 11, 12A East, 12A West, 12B, 13)</u>	<u>Mixed Use-Commercial</u> <u>Community and Civic</u> <u>Open Space and Recreation</u>	<u>60'</u>	<u>645,000</u>
<u>LAX Northside Airport Support District (Area 4, 5, 6, 7, 8, 9, 10)</u>	<u>Airport Support</u>	<u>30'</u>	<u>600,000</u>
<b>Total</b>			<b>2,320,000 sf</b>

**Footnotes:**

(a) For a more detailed list of permitted uses, see the Exhibit "LAX Northside Design Guidelines and Standards."

(b) For additional height restrictions, see the "LAX Northside Design Guidelines and Standards."

**H. Additional Permitted Uses.** In addition to those uses permitted by the Exhibit "LAX Northside Design Guidelines and Standards," uses similar or incident to those hereby permitted, all uses requiring a Conditional Use Permit pursuant to LAMC Section 12.24, and uses and operations deemed necessary for the safe and efficient operation of the airport shall be allowable as determined by the Executive Director.

~~H.4. Transfer and Equivalency Program.~~ Transfers of floor area ~~between in~~ all Areas within a District ~~are is allowable-permitted per the Land Use Equivalency Matrix of the "LAX Northside Design Guidelines and Standards", however, transfers between all LAX Northside Districts are is prohibited.~~

(a) All land uses within a District may be exchanged, so long as the proposed use is permitted by this Specific Plan.

(b) ~~Floor Area All land uses in all Areas~~ within a District may be exchanged, so long as the proposed use and the maximum allowable Floor Area are permitted by this Specific Plan.

(c) ~~An applicant seeking a Requests for~~ Transfer or Equivalency Exchange shall file be indicated in the ~~an~~ application for LAX Plan Compliance Review Administrative Clearance Approval with the Executive Director.

~~I. **Setbacks.** Setbacks are measured from the LAX Northside Sub-Area or Right-of-Way within the LAX Northside Sub-Area, regardless of internal parcels or lot lines. Setbacks apply to Buildings, and Ancillary Structures.~~

~~(a) Setbacks shall comply with the standards included in the Exhibit "LAX Northside Design Guidelines and Standards."~~

DCP recommends retaining Section I, Setbacks, with modified language as shown below:

**I. Setbacks.** Setbacks in the LAX Northside Sub-Area are measured from the LAX Northside Sub-Area or Right-of-Way, regardless of internal parcels or lot lines. Setbacks apply to buildings and ancillary structures and shall comply with the standards in the "LAX Northside Design Guidelines and Standards."

~~J. **Building Orientation.** Building Orientation shall comply with the standards included in the Exhibit "LAX Northside Design Guidelines and Standards."~~

DCP recommends retaining Section J, Building Orientation, with modified language as shown below:

**J. Building Orientation.** Building orientation shall comply with the standards in the "LAX Northside Design Guidelines and Standards".

~~E. **Design Plan and Guidelines.** In addition to Appendix A, projects shall comply with the "Design Plan and Guidelines for LAX Northside" by Albert C. Martin and Associates, dated April 20, 1989, and any amendment adopted by BOAC after that date~~

~~F. **Area 13.** Notwithstanding the provisions of Subsections C and E above, the requirements in Appendix A and the "Design Plan and Guidelines for LAX Northside" by Albert C. Martin and Associates, dated April 20, 1989, shall not apply to Area 13 of the LAX Northside Sub-Area. Area 13 shall be used for recreational facilities and other public benefit type uses, including~~



~~child care, children's play area, picnic amenities, athletic fields, parks, libraries, and similar uses.~~

## **Sec.12. TRANSPORTATION REGULATIONS.**

### **A. Right-of-Way Dedications.**

1. Right-of-way dedications, or any similar grant of rights to use land for public street purposes, shall be made for any public streets to the satisfaction of the LADOT General Manager and the City Engineer. All dedications shall be in conformance with City of Los Angeles standard street dimensions, unless deemed unwarranted by the LADOT General Manager and the City Engineer based on any environmental review and/or traffic analyses. The LADOT General Manager and the City Engineer may also allow variations from dedications to standard street dimensions if the variation is necessary to assure proper integration of a dedication into existing on-site conditions. The following public roadways within the Specific Plan Area are designated as major or secondary highways:

- (a) 96th Street
- (b) 111th Street
- (c) Airport Boulevard
- (d) Aviation Boulevard
- (e) Bellanca Avenue
- (f) Century Boulevard
- (g) Falmouth Avenue
- (h) Imperial Highway
- (i) La Cienega Boulevard
- (j) La Tijera Boulevard
- (k) Lincoln Boulevard
- (l) Pershing Drive
- (m) Sepulveda Boulevard
- (n) Westchester Parkway/Arbor Vitae Street

2. The use of airspace above a public street shall be permitted so long as the surface elements of that street may be maintained for street purposes.

3. Any roadway within the Airport Airside and Airport Landside Sub-Areas, which is not already designated as a public street, may be designated as an internal airport roadway. The LADOT General Manager's authority in the design of these roadways shall be advisory only, except that the intersection of these roadways with public streets shall be to the satisfaction of the LADOT General Manager and the City Engineer. Maintenance of internal airport roadways shall be the responsibility of LAWA.

### **B. Required Traffic Improvements - Airport Airside and Airport Landside Sub-Areas Only.**

A transportation improvements phasing plan shall be prepared by LAWA and approved by the LADOT General Manager that is in conformance with the LAX Plan and the certified Master Plan Final Environmental Impact Statement/Environmental Impact Report, and includes all on-site and off-site improvements as required by the LADOT General Manager. Revisions may be

made to the transportation improvements phasing plan in conformance with any subsequent environmental review, or where appropriate, as determined by the LADOT General Manager.

Transportation improvements shall be constructed or suitably guaranteed to the satisfaction of the LADOT General Manager and the City Engineer in connection with individual Projects or development sites as specified in the transportation improvements phasing plan. Prior to the issuance of any final certificate of occupancy in the final phase of the transportation improvements phasing plan, all required improvements in the entire phasing plan shall be funded, completed, or resolved to the satisfaction of the LADOT General Manager. If a proposed traffic mitigation measure does not receive the required approval, a substitute mitigation measure may be provided subject to approval by the LADOT General Manager, or other governing agency with jurisdiction over the mitigation location, upon demonstration that the substitute measure is equivalent or superior to the original measure in mitigating the Project's significant traffic impact.

### **C. Project Trip Generation.**

1. In an effort to monitor traffic impacts and traffic mitigation measures, LADOT and LAWA shall jointly conduct traffic counts or otherwise determine the traffic impacts of Projects within the Master Plan. The conclusions of these counts and other determinations shall be incorporated into a traffic generation report, which shall be approved by the LADOT General Manager and annually submitted to BOAC, City Council, and the Department of City Planning.

The Master Plan FEIS/EIR forecasts the net new Trips at full build out of the Master Plan, after implementation of mitigation measures, to be no more than 8,236 trips at airport peak hour. If the annual traffic generation report described above, and/or the annual traffic generation report considered together with any Project-specific traffic study, shows that development of the Master Plan is likely to increase the Trips beyond 8,236, LAWA shall complete the Specific Plan Amendment Study required in Section 7 H of this Specific Plan.

No Specific Plan Amendment Study shall be required if the annual traffic generation report, and/or the annual traffic generation report considered together with any Project-specific traffic study, determines that the net new Trips are anticipated to exceed 8,236 in the airport peak hour, but this increase in Trips will only be temporary until the Project(s) and associated mitigation measures are complete and/or if this increase in Trips is consistent with the number of Trips anticipated to occur during the peak year of traffic impacts as analyzed in the Master Plan FEIS/EIR. In this case, the traffic generation report shall evaluate the effectiveness of future Projects and mitigation measures in ultimately reducing the number of net new Trips to 8,236 in the airport peak hour at buildout of the Master Plan. Any LAX Plan Compliance approval for a Project shall include any conditions necessary to ensure the ultimate, reduction. If Trip reduction program measures are recommended, LAWA shall include in future annual reports an analysis of the on-going effectiveness of those measures and, if the Trip reductions are not effectuated, additional measures may be implemented and/or a Specific Plan Amendment Study may be triggered.



**2. LAX Northside Sub-Area.** As part of the annual traffic generation report, the number of Trips generated by each project shall be documented so that the total number of Trips generated by on-going development is monitored and reviewed by the LADOT General Manager and the Executive Director for consistency with the maximum allowable number of a.m. and p.m. peak Trips. LADOT and LAWA shall agree on procedures for this documentation. These counts shall be taken at the expense of the Applicant.

A reduction in the total number of Trips permitted to be generated by development in the LAX Northside Sub-Area, from that which was approved for this area under previous entitlements is required as a result of the LAX Plan and shall be imposed by way of this Specific Plan.

All projects within the LAX Northside Sub-Area together shall not generate more than 3,922 2,009 project-related Trips in the a.m. peak hour (part of the total 6,496 net new a.m. peak hour Trips for the LAX Master Plan) and 4,421 2,543 project-related Trips in the p.m. peak hour (part of the total 6,914 net new p.m. peak hour Trips for the LAX Master Plan). The number of Trips generated by a project shall be based on the trip generation rates used in Ordinance No. 168,999 (Coastal Transportation Corridor Specific Plan, as amended (CTCSP)) and/or determined appropriate by the LADOT General Manager and on square footages of the proposed project. In conjunction with each application for LAX Plan Compliance Review Project Permit Compliance Review Administrative Clearance for a Pproject within the LAX Northside Sub-Area, the Applicant shall estimate the number of Trips generated by each Pproject and submit the estimate to the LADOT General Manager for review and approval. This subsection shall not apply to development within Area 13 as shown on Map 3.

**D. Automated People Mover System.** APM(s) shall be optimally designed and constructed to minimize disruption and vehicle delay on the public roadway and transit system, and shall be elevated above street level wherever possible to minimize at-grade crossing points with public roadways. LAWA shall consult with the LADOT General Manager and the City Engineer early in the design process of the APM.

Any rules and regulations of the Public Utilities Commission of the State of California governing the APM, including but not limited to its design, operation, and maintenance, shall supersede any other provision of this Specific Plan and any building or zoning ordinances to the contrary.

## **Sec. 13. PARKING REGULATIONS.**

### **A. Requirements- Airport Airside and Airport Landside Sub-Areas Only.**

1. It is anticipated that approximately 32,155 off-street parking spaces may be provided at build out of the LAX Master Plan, however, notwithstanding LAMC Section 12.21 A 4, no more than 35,712 off-street parking spaces shall be provided at build-out of the LAX Master Plan for passengers, visitors, and airport and airline employees. Parking shall be comprised of short-term, long-term, and employee parking spaces.

2. Notwithstanding LAMC Section 12.21 A 4 (g), parking may be located at any location within the Airport Airside and Airport Landside Sub-Areas.

3. Subsections 1 and 2 above shall not apply to off-street parking for visitors and employees of cargo-only facilities and commercial/industrial buildings located within the area bounded by Century Boulevard, Aviation Boulevard, Imperial Highway and Sepulveda Boulevard, and the area west of Sepulveda Boulevard and north of Imperial Highway. Off-street parking for these cargo-only facilities and commercial/industrial buildings shall be provided in compliance with the provisions of LAMC Section 12.21.A.4(c). The Executive Director may also permit two or more of these uses to share off-street parking spaces, if the Executive Director determines that a lower total number of parking spaces than would otherwise be required will provide adequate parking for these uses.

**B. Requirements - LAX Northside Sub-Area Only.** All projects within the LAX Northside Sub-Area shall provide off-street parking ~~as specified in Appendix per LAMC 12.2124.A.4. and the "Design Plan and Guidelines for LAX Northside" by Albert C. Martin and Associates, dated April 20, 1989, and any amendment adopted by BOAC after that date~~

#### **Sec.14. SIGN REGULATIONS.**

**A. General Requirements.** The Department of Building and Safety shall issue sign permits for any signs otherwise requiring a permit pursuant to Article 6, Chapter IX of the LAMC that are regulated by this Specific Plan. All signs and sign support structures that are erected and maintained on property owned or controlled, in whole or in part, by LAWA shall be reviewed by the Department of Building and Safety pursuant to LAMC Sections 14.4.1, 14.4.2, 14.4.3, 14.4.4 A-14.4.4 I, 14.4.11 C - 14.4.11. E, 14.4.12 F, 14.4.15, 14.4.16A, 14.4.16 C, 14.4.16 E, 14.4.18 H, 14.4.181, 14.4.19, 91.6201.2.1, 91.6201.3, 91.6202, 91.6204, 91.6205, 91.6205.18, 91.6207, 91.6209, 91.6210, 91.6211, 91.6212, 91.6213, and 91.6216. The Executive Director shall review signs for conformance with all other sign provisions of Article 4.4, Chapter 1 and Article 1, Division 62 of Chapter IX of the LAMC and provide a written approval to the Department of Building and Safety prior to issuance of any sign permit. Types of signs identified in this Specific Plan are defined in LAMC Section 14.4.2. Where materials for signs are not specified in the LAMC, materials shall be approved by the Departments of Building and Safety and Fire. Except for Subsections D and E, the provisions of this Section shall apply only to signs within the Airport Airside and Airport Landside Sub-Areas.

**B. Area, location, Spacing and Height and Review Procedure.** Signs shall not be subject to the LAX Plan Compliance Review procedure, but shall be reviewed and approved by the Executive Director prior to issuance of any permit for, or installation of, a sign or sign support structure.

Prior to approving any sign, the Executive Director shall determine if the proposed sign is in conformance with the sign guidelines adopted by BOAC. If the adopted guidelines address area,



spacing, location, and height of signs, the Executive Director may only approve a sign if it is consistent with those guidelines. If the guidelines do not address area, spacing, location, and height of signs, then the Executive Director may only approve signs that are consistent with the appropriate area, spacing, location, and height regulations in Article 4.4, Chapter I of the LAMC.

Prior to approving any sign, the Executive Director, after consultation with LADOT, shall also determine that the sign is not a hazard to traffic nor will it result in unsafe freeway exposure.

**C. Off-Site, Supergraphic, and Mural Signs.** Alteration, redesign or replacement of existing off-site signs, or erection, construction or installation of new off-site signs, supergraphic signs, and mural signs shall be permitted pursuant to the establishment of a sign district as set forth in LAMC Section 13.11.

**D. Requirements - LAX Northside Sub-Area Only.** Signs within the LAX Northside Sub-Area shall be in compliance with the requirements set forth in ~~Appendix A and the Exhibit "LAX Northside Plan Design Guidelines and Development Standards" for the LAX Northside" by Albert C. Martin and Associates, dated April 20, 1989.~~

**E. State and Federally Required Signage.** Any signs or sign restrictions mandated by the FAA, TSA, or any other State or Federal regulatory agency shall supersede any other provision of this Specific Plan to the contrary.

**Sec. 15. SEVERABILITY.** If any provision of this Specific Plan or its application to any person or circumstance is held to be unconstitutional or otherwise invalid by any court of competent jurisdiction, the invalidity shall not affect other Specific Plan provisions, clauses or applications, which can be implemented without the invalid provision, clause or application, and to this end the provisions and clauses of this Specific Plan are declared to be severable.

**Sec. 16.** The City Clerk shall certify to the passage of this ordinance and have it published in accordance with Council policy, either in a daily newspaper circulated in the City of Los Angeles or by posting for ten days in three public places in the City of Los Angeles: one copy on the bulletin board located in the Main Street lobby to the City Hall; one copy on the bulletin board located at the ground level at the Los Angeles Street entrance to the Los Angeles Police Department; and one copy on the bulletin board located at the Temple Street entrance to the Los Angeles County Hall of Records.

I hereby certify that the foregoing was introduced at the meeting of the Council of the City of Los Angeles of \_\_\_\_\_, and was passed at its meeting of  
MAY 21 2013

~~JUNE LAGMAY, City Clerk~~

Approved \_\_\_\_\_

Mayer

~~Approved as to Form and Legality~~

~~CARMEN A. TRUTANICH, City Attorney~~  
~~By \_\_\_\_\_~~

~~Pursuant to Charter Section 559, I approve this ordinance on behalf of the City Planning Commission and recommend it be adopted.~~

~~File No(s). CF No. 13-0285; CPC File No. 2012-3357-GPA-SP~~



# LAX NORTHSIDE

DESIGN GUIDELINES AND STANDARDS

JUNE 2015

EXHIBIT B



June 2015

Vision and Design provided by :

**RIOS CLEMENTI HALE STUDIOS**

Edited and prepared with :



Prepared for :





# ***LAX NORTHSIDE***

***DESIGN GUIDELINES AND STANDARDS***



An aerial photograph of the LAX Northside development, showing a large, rectangular building complex with a grid-like structure, surrounded by parking lots and other infrastructure. The image is in black and white and has a grainy, high-contrast appearance.

# **LAX NORTHSIDE**

**DESIGN GUIDELINES AND STANDARDS**

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A group of people, including two men in the foreground wearing black t-shirts and baseball caps, are gathered around a large table. They are looking at a large-scale architectural model of a city or urban development. The model features a mix of green spaces, roads, and building footprints. One man is pointing at a specific area on the model. In the background, another person is visible, and a framed picture of a bridge hangs on the wall.

# PART I

INTRODUCTION, CONTEXT, AND VISION





# PART I

## 1 INTRODUCTION

*The overall purpose of the Los Angeles International Airport (LAX) Northside Design Guidelines and Standards is to provide a framework for appropriately scaled development that is consistent with airport needs and neighborhood conditions. These guidelines and standards have taken into consideration Federal Aviation Administration (FAA) regulations, market conditions, sustainability, and the context of the LAX Northside (Project site), which includes an active airfield and residential neighborhoods. These design guidelines and standards will direct land uses and address issues of urban design, architecture, landscape materials and design, pedestrian infrastructures, and signage.*

### 1.1 OVERVIEW

This document is intended to create a vibrant, sustainable center of employment, retail, restaurant, office, hotel, research and development, higher education, civic, airport support, recreation, and buffer uses that support the needs of surrounding communities and of Los Angeles World Airports (LAWA), the City of Los Angeles department that manages LAX. LAWA proposes the LAX Northside Design Guidelines and Standards to provide regulations for future development occurring within the LAX Northside sub-area of the LAX Specific Plan. LAWA acquired the LAX Northside, which was once primarily single-family homes, in part using FAA grants which require the conversion of the Project site to compatible land uses in close proximity to airport operations at LAX. In 1984, the City of Los Angeles approved 4,500,000 square feet of commercial development on the Project site. In 1989, LAWA prepared the Design Plan and Development Guidelines for LAX Northside to provide additional guidance on development of the Project site. The City of Los Angeles subsequently incorporated the 1984 entitlements and 1989 Design Plan and Development Guidelines for LAX Northside into later planning documents, including the adopted 2004 LAX Specific Plan.

The LAX Northside Design Guidelines and Standards update the 1989 Design Plan and Development Guidelines for LAX Northside to reduce the amount of development allowed on the approximately 340 acre Project site to a maximum of 2,320,000 square feet. In order to allow for flexibility of future development to respond to future market conditions, transfers and exchanges of uses and development rights are allowed within limited areas of the Project site, not to exceed any specified environmental constraints, provided that all design guidelines and standards are met.

**FIGURE 01.1**  
DISTRICT MAP





This document brings the existing design standards up-to-date; responds to current market realities and stakeholder interests; complies with FAA requirements and regulations, including FAA grant requirements; allows the development of the LAX Northside in line with current best-practices in urban design and sustainability; and reinforces the LAX Northside as a buffer area between LAX and the residential neighborhoods to the north by reshaping the topography and introducing compatible development. The objectives of this document include: balancing the needs of neighborhoods and LAX; meeting rigorous environmental sustainability standards in design, construction, operation, and landscaping; managing vehicle traffic through smart engineering and trip reduction; achieving the best use of the property and fair market value; complying with all applicable zoning, land use, and airport land use compatibility regulations; and providing a foundation for other neighborhood improvements and services.

## 1.2 ORGANIZATION AND CONTENT

The LAX Northside Design Guidelines and Standards are intended to supplement Section 11 LAX Northside Sub-Area of the LAX Specific Plan with detailed development guidelines and standards for the LAX Northside. The design guidelines and standards are organized to address three specific geographic districts: the LAX Northside Center District, the LAX Northside Campus District, and the LAX Northside Airport Support District. These districts are depicted in Figure 01.1.

This document consists of two parts. Part I includes Chapters 1, 2, and 3. Chapter 1 provides the introduction, organization and content, and relationship to other documents. The context for the LAX Northside project area is described in Chapter 2, providing insight into both the regional and local influences on the LAX Northside. The vision and overall design intent for the LAX Northside is articulated in Chapter 3, which includes graphics and illustrations intended to facilitate the understanding and implementation of the guidelines and standards.

Part II contains the design guidelines and standards that will guide development in the LAX Northside. Part 2 consists of Chapters 4 through 8. Chapters 4 through 8 contain guidelines and standards as follows: Urban Design, Architectural, Landscape, Paseo & Public Realm, and Signage & Graphics. The guidelines and standards

are formatted in a checklist manner to help facilitate use, provide clarity, and standardize the review process.

## 1.3 RELATIONSHIP TO OTHER DOCUMENTS

The LAX Northside is located in the City of Los Angeles, and development of the Project site is governed by the City of Los Angeles' land use policy and related ordinances. The City of Los Angeles Citywide General Plan Framework defines the City of Los Angeles' long-range comprehensive growth strategy, and sets forth policies, goals, and objectives to guide land use regulations for Community Plans. The City of Los Angeles General Plan Land Use Element consists of 35 local Community Plans that set forth land use regulations and zoning for specific areas. The LAX Plan is the City of Los Angeles General Plan Land Use Element for LAX, including the LAX Northside. The LAX Plan is intended to promote an arrangement of airport uses that encourages and contributes to the modernization of the Airport in an orderly and flexible manner within the context of the City of Los Angeles and region. Finally, the LAX Specific Plan implements the goals and objectives of the LAX Plan through zoning and development standards, and contains specific provisions for the LAX Northside. This document supplements the LAX Specific Plan with design guidelines and standards for the LAX Northside.

Relevant documents and ordinances that have established the development pattern for the the LAX Northside are summarized here:

- 1984 Zoning Ordinances (159,526; 169,254; and 169,768) and Final Tract Map No. 34836: Permit up to 4.5 million square feet of commercial development. These entitlements were incorporated into the 2004 LAX Specific Plan.
- 2004 LAX Plan: Provides the long-range land use policy framework and serves as the land use element for Los Angeles' General Plan for LAX, including the LAX Northside. The currently adopted LAX Plan land use designation for the Project site is LAX Northside. This land use designation provides for the development of a variety of uses that are consistent with airport needs and neighborhood conditions. The primary allowable uses within the LAX Northside include: commercial development; office; light industrial; research and development; hotel and conference facilities; retail and restaurant uses; schools and community facilities; open space; bicycle paths; and greenway buffers.

- 2004 LAX Master Plan: Sets forth the comprehensive development program for LAX properties, including runway and taxiway system modernization, redevelopment of terminal areas, airport access improvement, and passenger safety, security, and convenience enhancements.
- 2004 LAX Specific Plan: Implements the goals and objectives of the LAX Plan through zoning and development standards, and contains specific provisions for the Project site. The currently adopted LAX Specific Plan zoning for the Project site is Los Angeles International Airport Northside Zone (LAX-N Zone). The purpose of the LAX-N Zone is to provide for development that is consistent with airport needs and neighborhood conditions.
- Los Angeles World Airports (LAWA) Sustainable Airport Planning, Design, and Construction Guidelines: Provides a comprehensive set of performance standards applicable to airports that can be utilized to integrate sustainable practices into airport planning, design, and construction for both on-airport and off-airport facilities, including the LAX Northside.
- Los Angeles Municipal Code: Includes regulatory provisions for development within the City of Los Angeles, including building regulations, noise standards, specific plans, and zoning.
- Coastal Transportation Corridor Specific Plan: Regulates phased development of land uses to ensure that transportation infrastructure can accommodate uses, and establishes programs and fees to reduce trips, encourage public transportation, and fund transportation improvements.
- Federal Aviation Administration Guidelines: Several Federal regulations intended to guide and control aviation noise, building heights, and uses adjacent to airports apply to the LAX Northside. These include, but are not limited to, Federal Aviation Regulation (FAR) Part 150, Airport Noise Compatibility Planning and FAR Part 77, Obstructions to Navigation.
- Los Angeles County Airport Land Use Plan: Intended to protect the public health, safety, and welfare by ensuring orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around public airports.

# PART I

## 2 CONTEXT

*The LAX Northside is comprised of approximately 340 acres within Los Angeles, located approximately 15 miles southwest of downtown Los Angeles. The LAX Northside vicinity includes the Westchester community of Los Angeles to the immediate north, the City of El Segundo and unincorporated community of Del Aire to the south of LAX, the City of Inglewood and unincorporated community of Lennox to the east of LAX, the Los Angeles community of Playa del Rey to the immediate west, and the Pacific Ocean further west. Major surrounding regional landmarks include Loyola Marymount University to the north, Dockweiler Beach State Park to the west, and Interstate 405 to the east.*

The LAX Northside is generally bounded by Sepulveda Westway and Sepulveda Boulevard to the east, LAX to the south, South Pershing Drive to the west, and generally 91st Street, Manchester Avenue, and 88th Street to the north. Westchester Parkway runs from east to west through the LAX Northside. Westchester Parkway was completed in anticipation of up to 4.5 million square feet of development in 1993 as a requirement of the original entitlements for the LAX Northside.

The LAX Northside vicinity includes a diverse mix of low- to medium-density commercial, residential, and industrial development. To the north of the LAX Northside are single- and multi-family residences in Westchester and Playa del Rey. Further northeast are the Playa del Rey Bluffs. Directly to the south are airfields, terminals, and LAX airport support uses. Retail and commercial uses are located to the east, primarily along Sepulveda Boulevard. The residential community of Playa del Rey is located to the west, and further west are beaches and the Pacific Ocean.

Open space, educational, public, and community-serving uses are also located near the LAX Northside and include Otis College of Art and Design, Westchester Recreation Center, St. Bernard High School, Westchester Senior High School, Paseo del Rey Elementary School, St. Anastasia School, Loyola Village Elementary, Visitation School, Playa del Rey Care and Rehabilitation Center, several churches, and Carl E. Nielsen Park. The Westchester Golf Course exists within the LAX Northside and Westchester Parkway is used for biking, walking, and jogging.

Figures 02.1 through 02.4 depict the character of uses surrounding the LAX Northside.

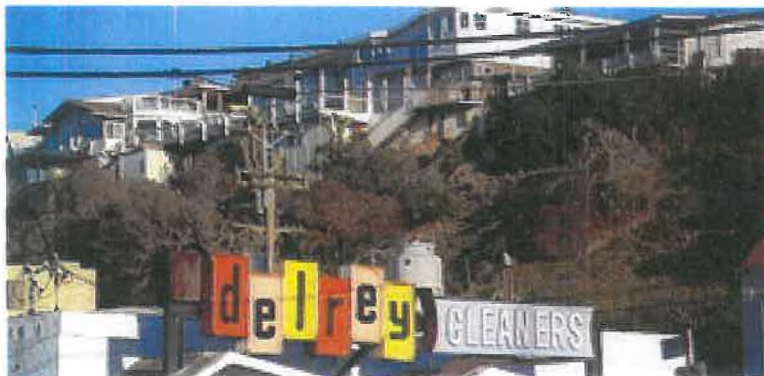




**FIGURE 02.1 // LOS ANGELES INTERNATIONAL AIRPORT (LAX)**  
The distinctly lit LAX Gateway Pylons located at Century Boulevard and Sepulveda Boulevard.



**FIGURE 02.2 // WESTCHESTER NEIGHBORHOOD**  
Typical single-family houses and tree-lined streets throughout the Westchester neighborhood.



**FIGURE 02.3 // PLAYA DEL REY**  
View of the bluffs and residences that characterize Playa Del Rey.



**FIGURE 02.4 // WESTCHESTER BUSINESS DISTRICT**  
Aerial view of Sepulveda Boulevard looking north at Westchester Parkway.

# PART I

## 3 VISION

*The LAX Northside Design Guidelines and Standards were developed through an extensive stakeholder engagement process that resulted in a comprehensive vision for the future of the LAX Northside. These guidelines and standards seek to address concerns about preserving sight lines and neighborhood privacy, controlling light spillage, creating better buffers between functioning active airfields and adjacent neighborhoods, controlling wildlife near airfields, and improving aesthetics and landscape in the LAX Northside.*

Input was sought from a variety of community groups, resident organizations, business associations, environmental leaders, recreation advocates, the development community, and regulatory agencies, including the FAA and other stakeholders. The overall vision is composed of three main components: (1) reshaping the topography of the LAX Northside and introducing low-scale development to reinforce the LAX Northside as a buffer area between LAX and surrounding uses, (2) introducing a consolidated pedestrian infrastructure for passive and active recreation

to preserve and enhance an already vibrant recreation culture, and (3) implementing an appropriate landscape palette that promotes sustainability and livability in overt and specific ways.

### 3.1 RESHAPING THE LAND AS A BUFFER

The LAX Northside Design Guidelines and Standards seek to ensure that all future development is compatible with the safe operation of aircraft at LAX today and into the future. Building heights and grading strategies accomplish two primary goals. The first goal is to ensure that building heights comply with applicable FAA restrictions for the safety of individuals adjacent to an active airfield. The second goal is to better connect the LAX Northside with Westchester Parkway, while buffering and creating compatible transitions with existing residences to the north.

The LAX Northside Design Guidelines and Standards provide a grading strategy that will better align future development along Westchester Parkway, provide marketable building frontages, and lower the grade of development on the LAX Northside relative to existing residential neighborhoods to the north. Grading the LAX Northside achieves the vision of the area as a buffer between LAX and adjacent uses and ensures that future development is sensitive to the surrounding context and compatible with aircraft operations. Figures 03.1, 03.2, and 03.3-1 through 03.3-6 illustrate the vision of reshaping the land as a buffer through grading strategies, building orientation, and height limits.

### 3.2 THE LAX NORTHSIDE PASEO

Westchester Parkway currently provides a substantial pedestrian infrastructure that is capable of accommodating both active and passive forms of recreation. The main unifying design feature of the LAX Northside is a pedestrian accessible paseo that connects all areas of the LAX Northside Center District and Campus District from east to west along Westchester Parkway. Starting in Area 11, adjacent to the existing Sepulveda Business District and downtown

Westchester, the paseo will continue west along Westchester Parkway until it reaches Pershing Drive, where it will connect with an existing recreation path to the beach. This nearly three (3) mile stretch will accommodate active and passive forms of recreation using a variety of appropriate materials. The paseo will be composed of the existing ten (10) feet of concrete sidewalk intended for walking and a twelve (12) foot wide path of stabilized decomposed granite intended for various forms of recreation, as depicted in Figures 03.4, 03.5, and 03.5-1 through 03.5-8. Continuity of the pedestrian experience will be maintained by minimizing vehicular entries and breaks in the paseo. Pedestrian safety will be promoted with appropriate pedestrian crosswalk signage and specific finish materials to reinforce crossing areas. Streetscape lighting will enhance pedestrian awareness and safety at all hours of the day, as depicted in Figures 03.6-1 and 03.6-2.

Activity along the paseo will be promoted with the introduction of entry plazas and shared common spaces that complement adjacent land uses. These could include outdoor restaurant patios, additional features such as fountains and green space, potential for a community farmer's market, or plaza spaces that connect buildings to the pedestrian realm along Westchester Parkway. Landscape materials shall be introduced along the paseo that foster a dynamic sense of color and annual bloom, while being resilient enough for the high pedestrian traffic in the area.

Aside from supporting an existing active recreation culture throughout the Westchester Community, the introduction of the paseo will consolidate pedestrian traffic and activity in the LAX Northside away from residential neighborhoods. This critical aspect to the design and location of the paseo reinforces the overall concept of the LAX Northside serving as a buffer between LAX and adjacent neighbors. By focusing pedestrian activity along Westchester Parkway and restricting access from the north and into adjacent neighborhoods, a safe environment will be maintained that does not infringe on the security of the airfield to the south, or the comfort and privacy of the communities to the north.

### 3.3 CREATING AN APPROPRIATE AND SUSTAINABLE LANDSCAPE

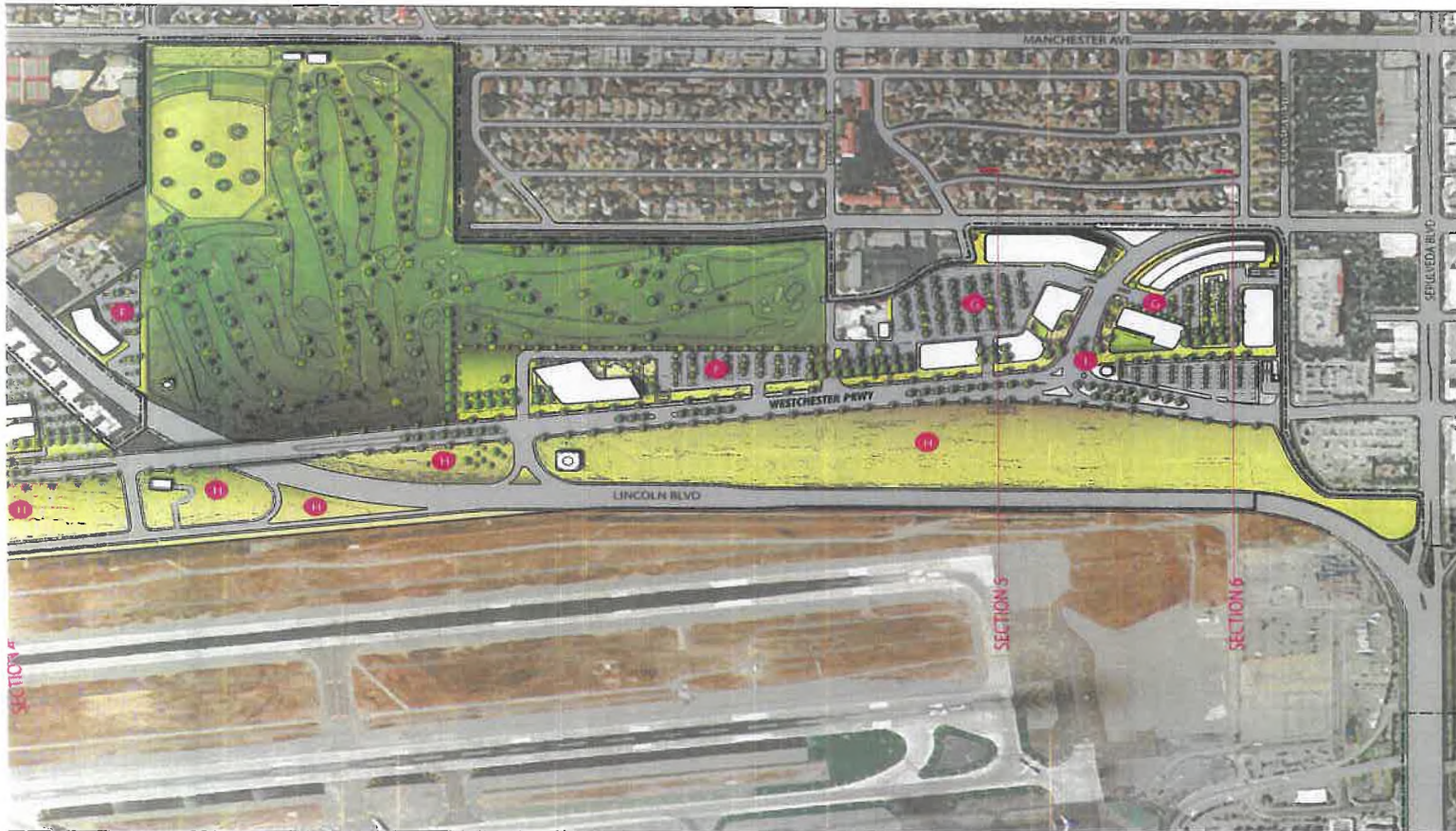
A primary goal for landscaping at the LAX Northside is to create a sustainable and functional urban landscape that prevents any unnecessary impact on adjacent users. The LAX Northside Design Guidelines and Standards allow landscaping that unifies the site, is compatible with aircraft operation per FAA guidelines, is sustainable, and responds to the local plant palette. Landscaping at the LAX Northside must not promote the proliferation of wildlife that may disrupt or endanger the functioning of the airfield. As such and per consultation with the US Fish and Wildlife Service, plant materials are restricted to those that: 1) have a sparse to moderately dense foliage growth, 2) do not produce fruits or seeds, 3) and do not require extensive maintenance to maintain appropriate foliage. Additional requirements, such as tree spacing and the prohibition of casting and spraying of seed for sod installation will further reduce the possibility of attracting flocking birds. To help implement these approaches consistently across the LAX Northside, landscape zones have been established to help isolate materials to their appropriate locations.

Landscaping is also designed to advance sustainability. Drought-tolerant plant materials are allowed to preserve water resources. Storm water will be managed in all surface parking lots through the integration of permeable materials in parking spaces and in the medians that separate them. The LAX Northside Design Guidelines and Standards also require the use of bioswales to remove silt and pollution from surface runoff water.

Finally, the planting palettes presented as a part of these guidelines seek to respond to native plant communities. Planting materials that are locally native to the surrounding coastal area (in comparison to native to the Southern California region) have been considered, and where appropriate, integrated into the required plant palettes. Additionally, native and non-native species are allowed to provide increased flexibility for project development. Figures 03.7-1 and 03.7-2 depict the vision for landscaping in the LAX Northside.







**FIGURE 03.1**  
ILLUSTRATIVE SITE PLAN



**LEGEND**

BOUNDARIES

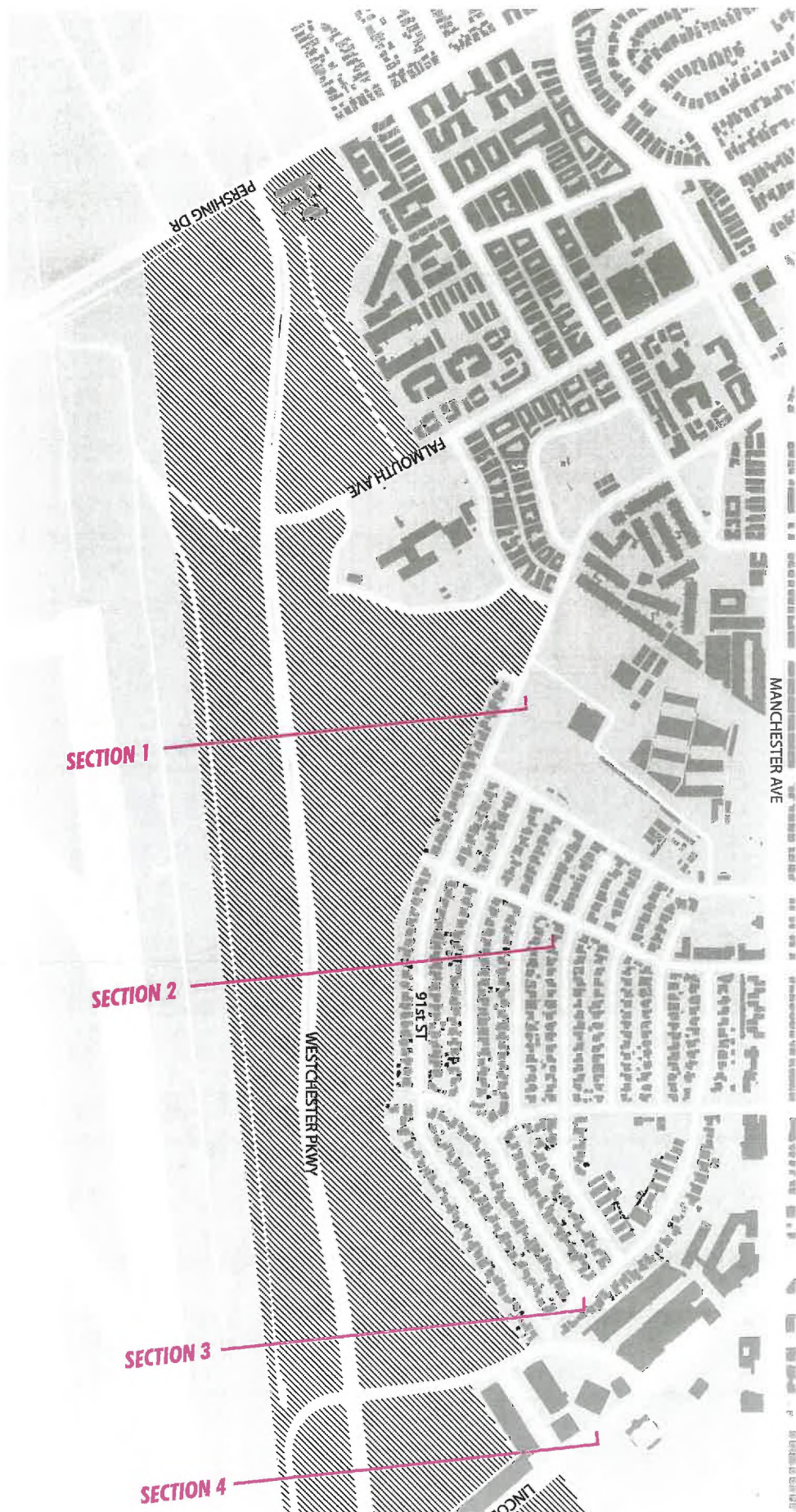
PROJECT BOUNDARY

**PROPOSED LAND USES**

- PARK and RECREATION USE
- PARK and RECREATION USE
- LANDSCAPE BUFFER and SECURITY FENCE
- OFFICE, and RESEARCH and DEVELOPMENT USE
- EDUCATION USE
- COMMUNITY and CIVIC USE
- MIXED USE
- AIRPORT USE
- FUTURE PUBLIC TRANSIT LOCATION

*For illustrative purposes only.*









**FIGURE 03.2**

SECTIONS KEY PLAN

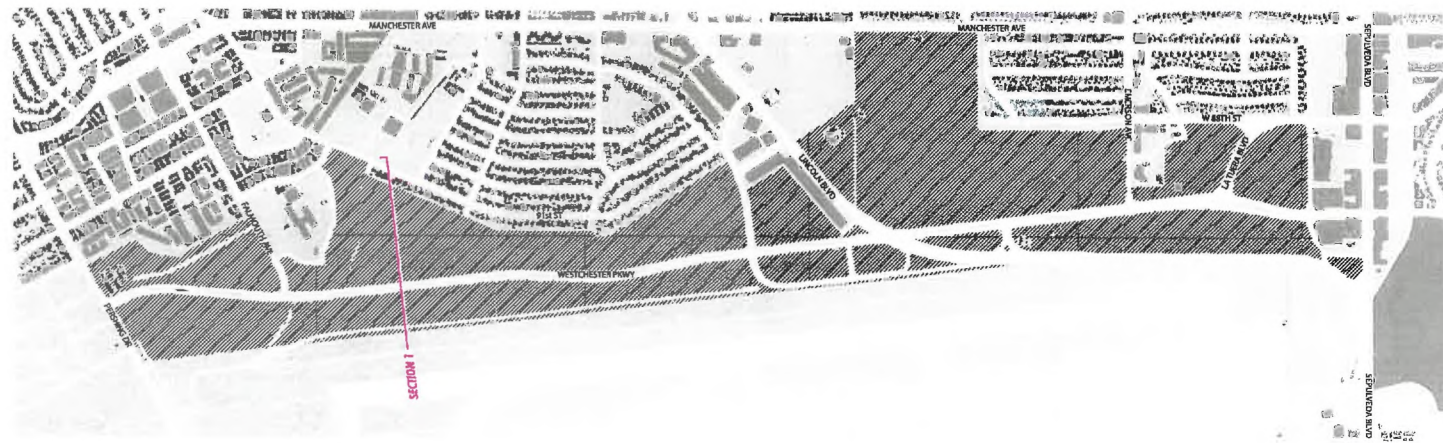
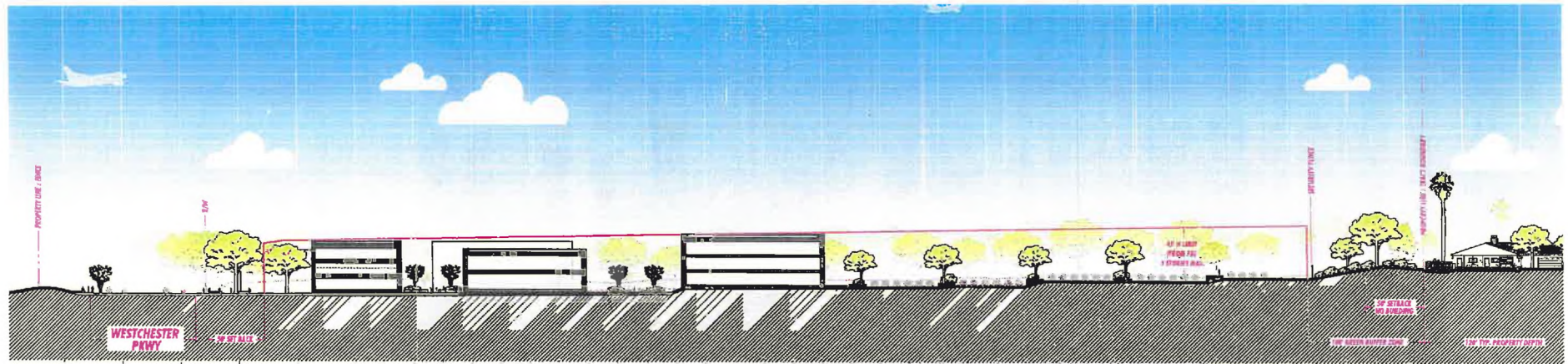
This figure shows the location of the section cuts presented in Figures 3.3-1 through 3.3-6. These section figures show potential development scenarios.



**FIGURE 03.3-1**

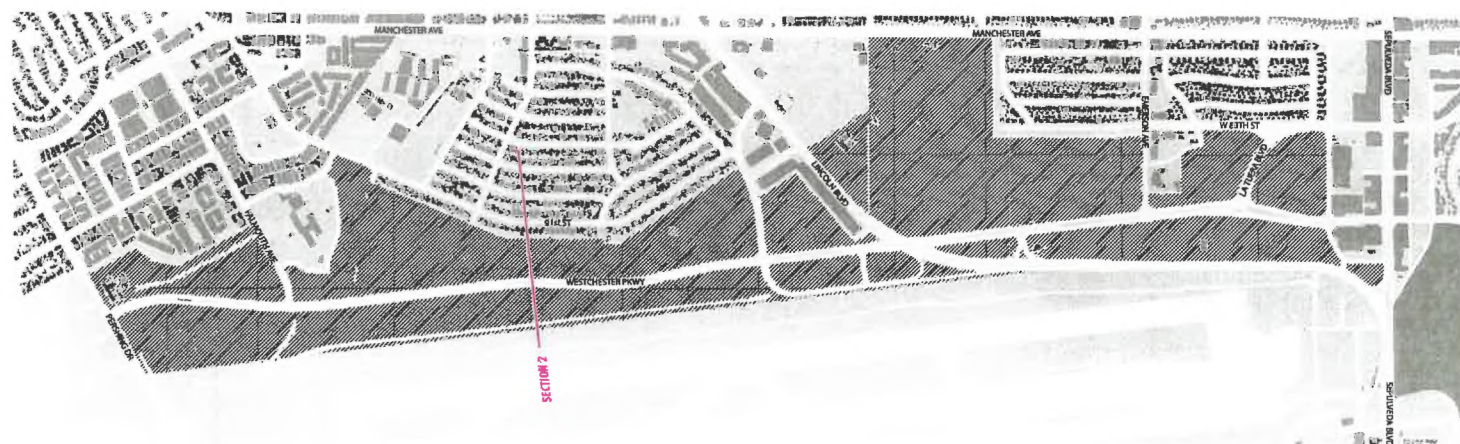
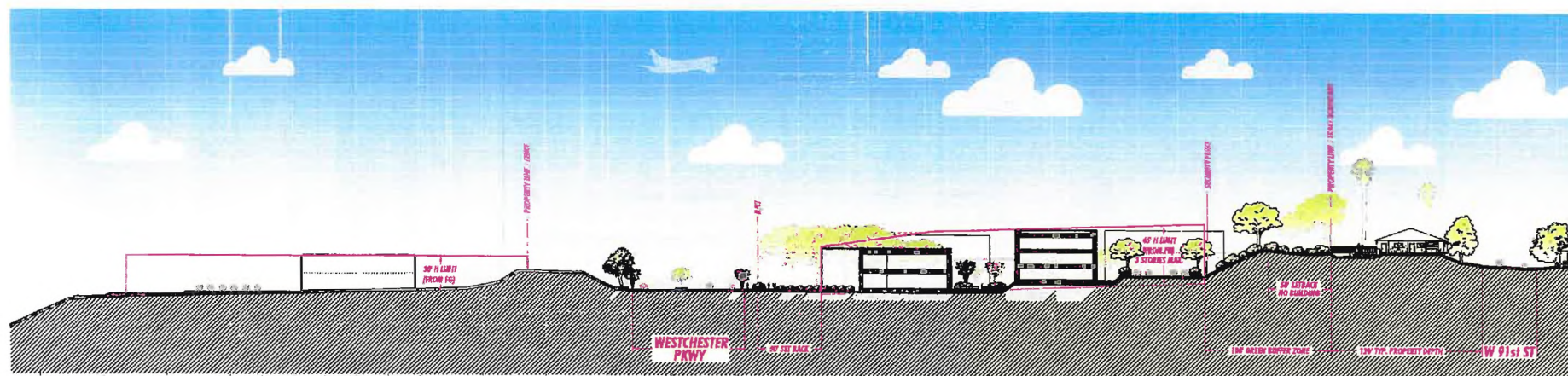
SECTION 01 / Along Cumi Laude Avenue

Development Scenario





SECTION 02 / Along Stanmoor Drive  
Development Scenario

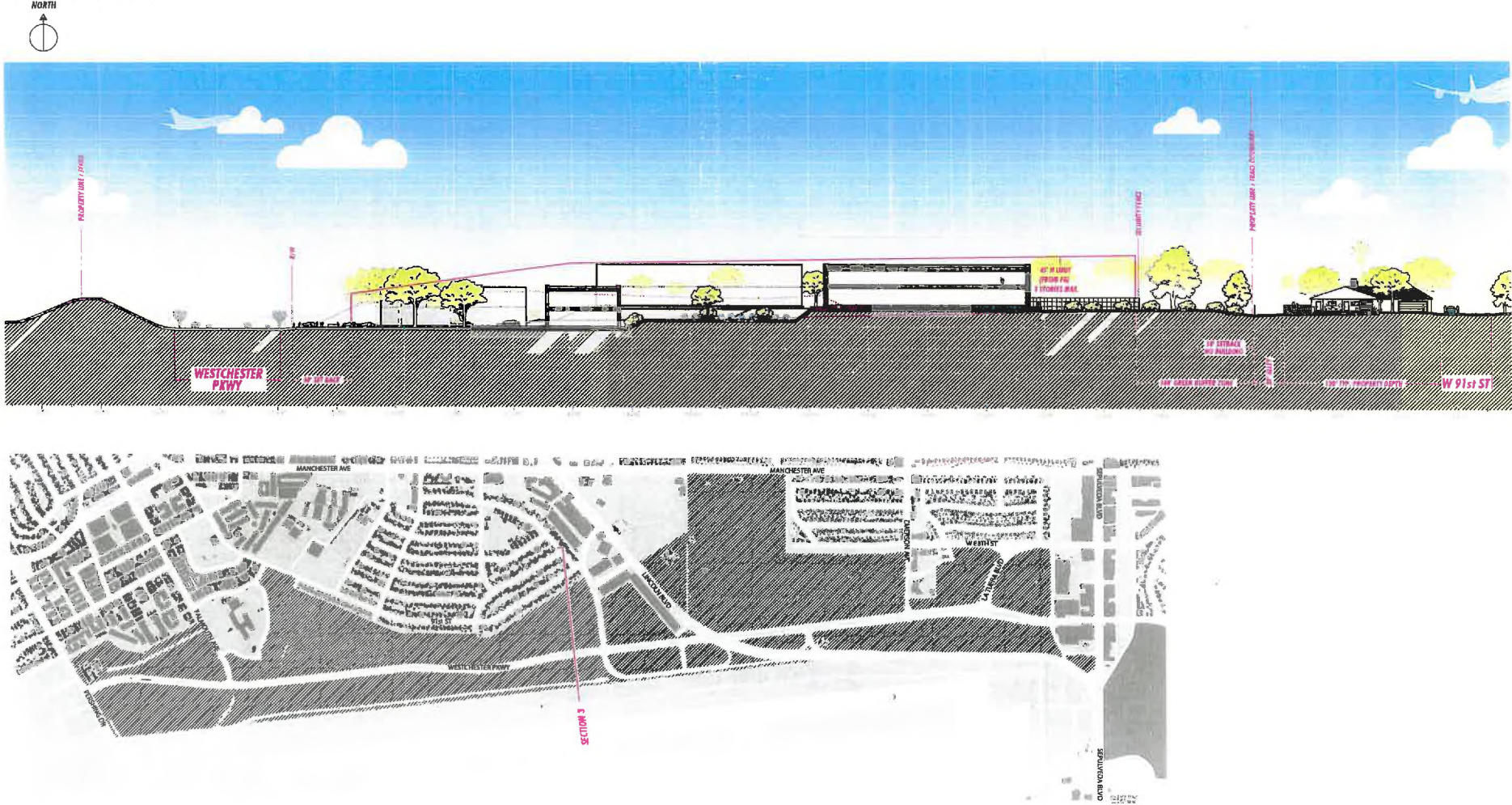




**FIGURE 03.3-3**

SECTION 03 / West of Loyola

Development Scenario



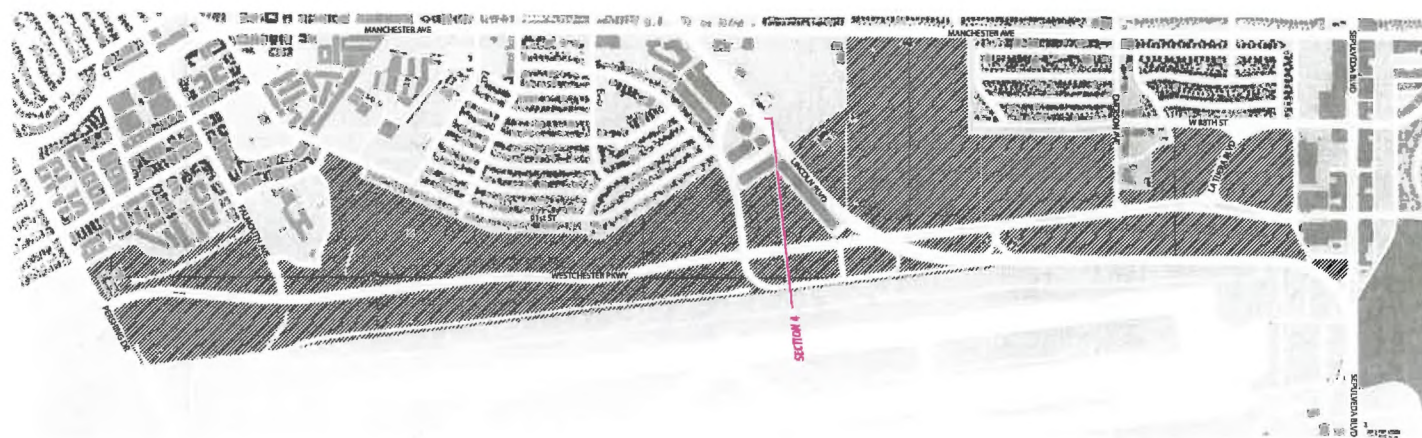
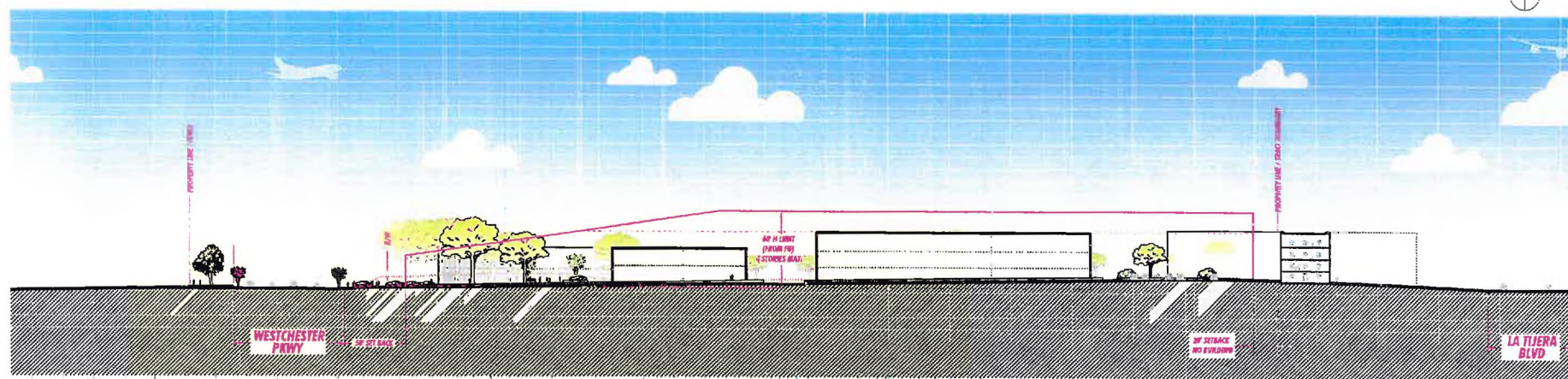


**FIGURE 03.3-4**

SECTION 04 / East of Loyola Boulevard

Development Scenario

NORTH

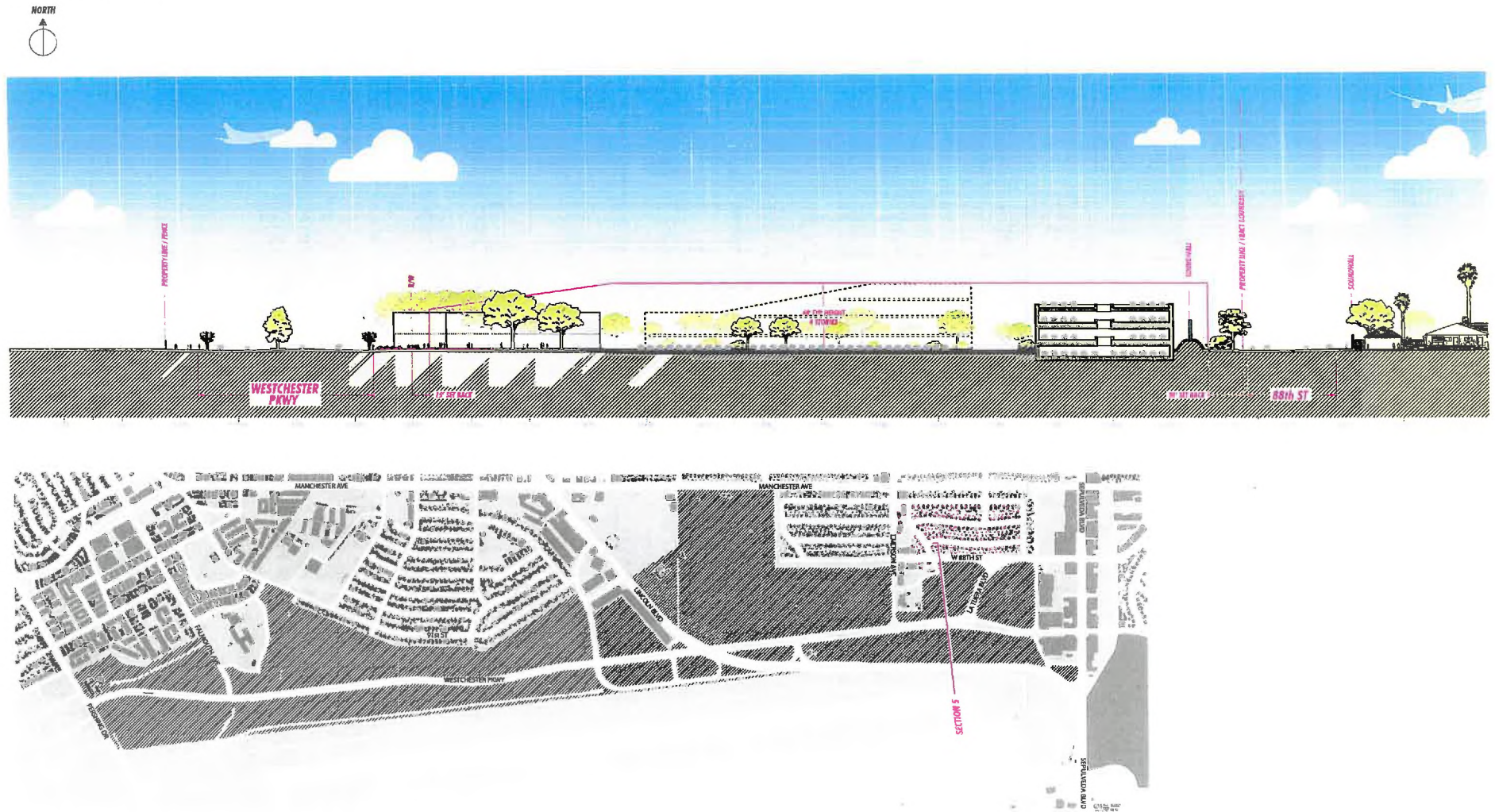




**FIGURE 03.3-5**

SECTION 05 / La Tijera West

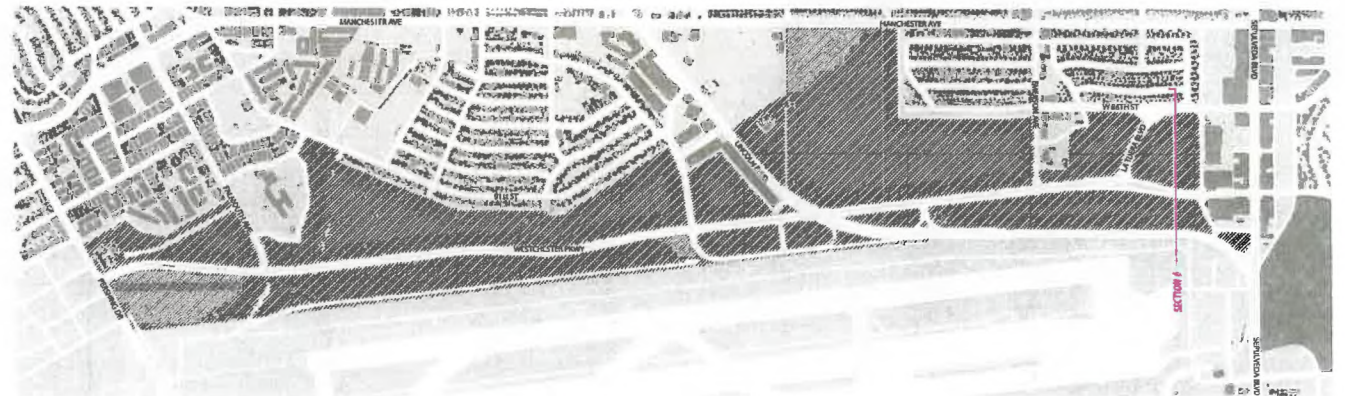
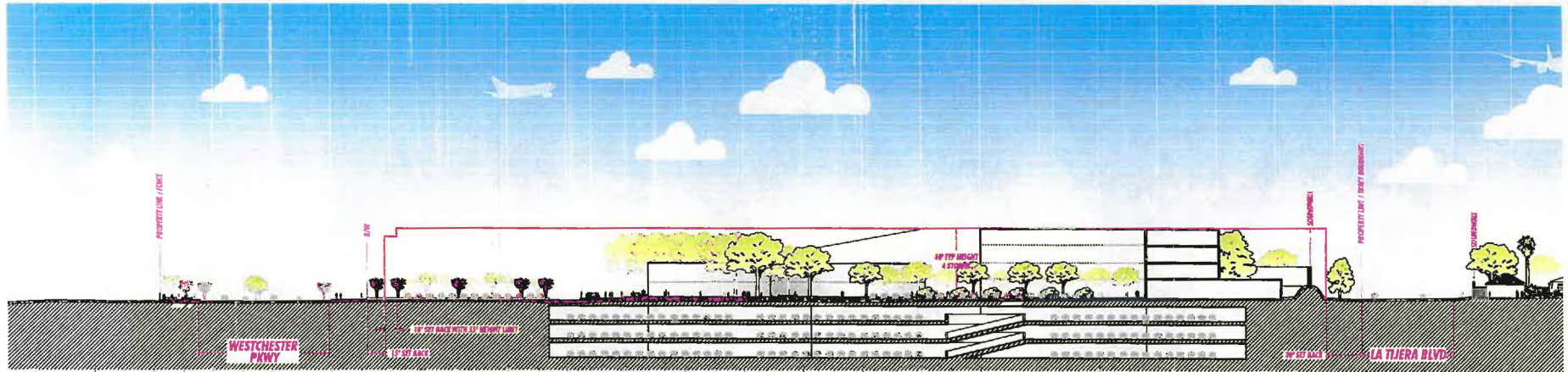
Development Scenario





**FIGURE 03.3-6**

SECTION 06 / Sepulveda Westway  
Development Scenario









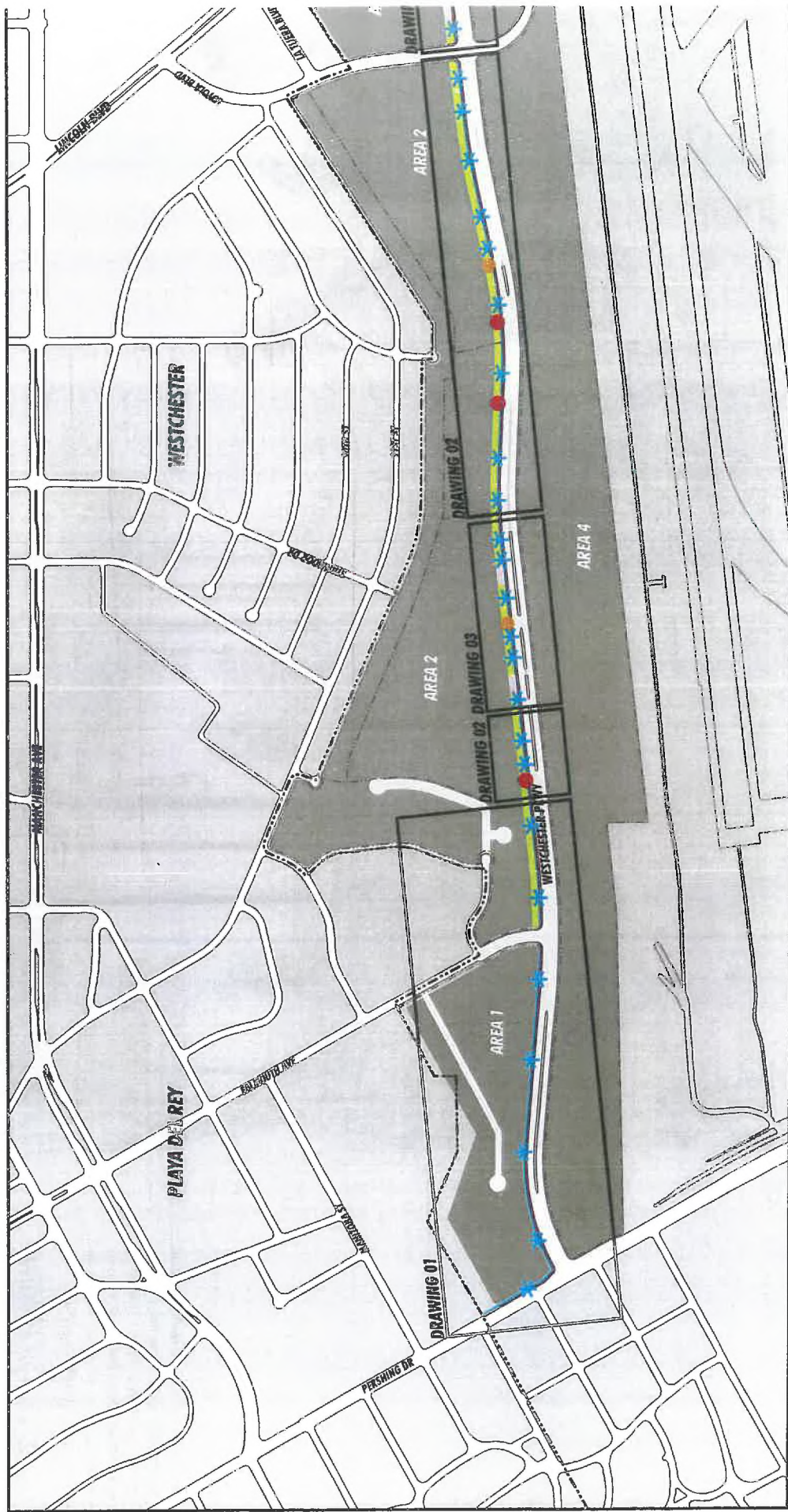


**FIGURE 03.4**

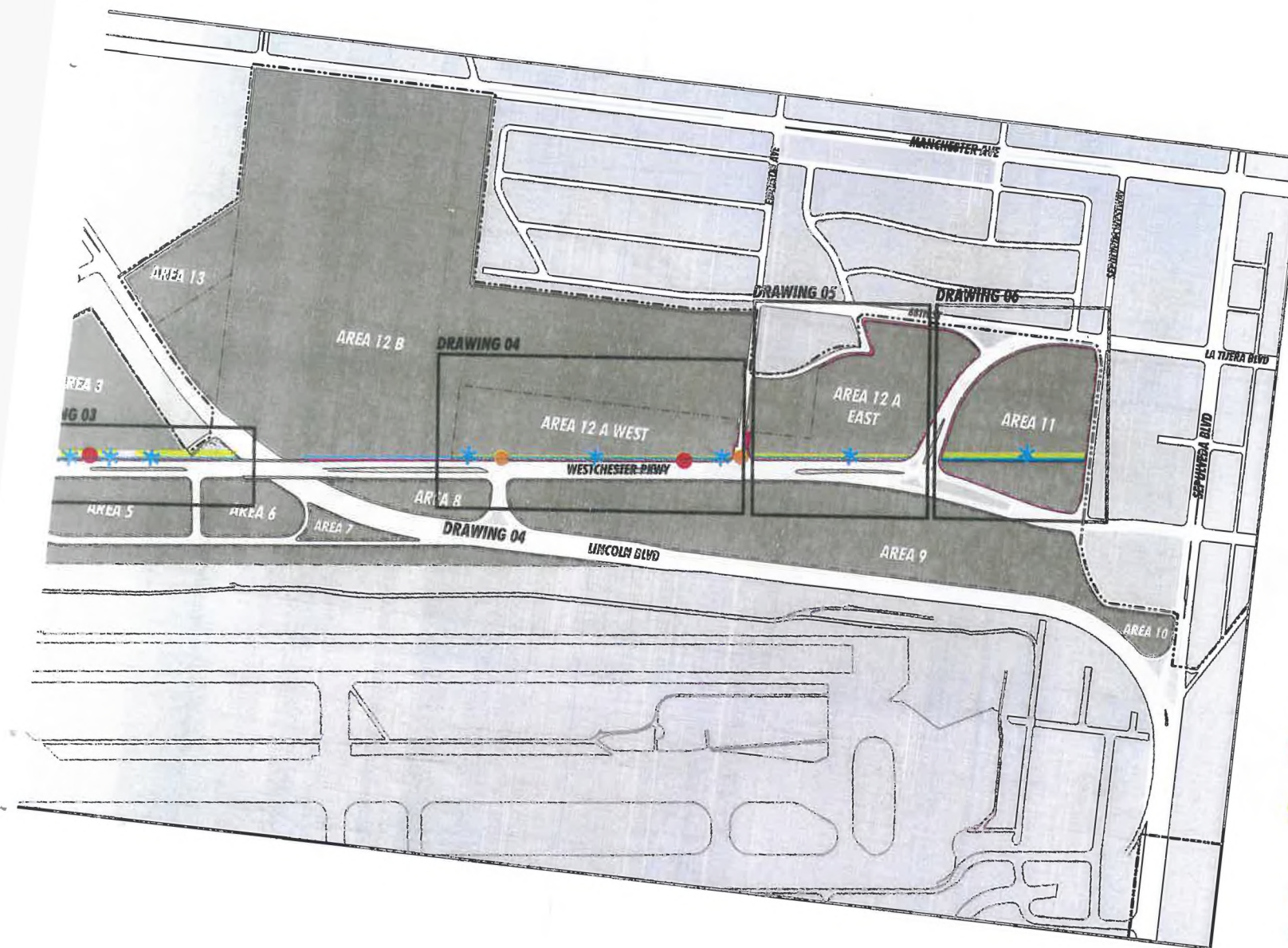
LAX NORTHSIDE CENTER - PASEO

This figure presents a conceptual illustration of the LAX Northside Center Paseo, including appropriate planting materials, and pedestrian amenities such as benches, trash receptacles, and pathway lighting.









**FIGURE 03.5**  
PASEO CONDITIONS & LOCATIONS

This figure illustrates the location and condition types that define the pedestrian accessible paseo in the LAX Northside Plan. It also provides a key for more detailed drawings presented in Figures 03.5-1 through 03.5-8.



### LEGEND

- EXISTING Sidewalk
- ADDITIONAL Sidewalk  
TOTAL AREA : 7,720 sq. ft.
- ADDITIONAL Decomposed Granite  
TOTAL AREA : 145,350 sq. ft.
- ADDITIONAL Space Remaining in Setback
- Landscaping
- Parking (Double Row)
- \* PLAZA Entry / RESPITE Area  
Additional Finish Material & Streetscape Amenities
- MAJOR Development Entry  
Signalized
- MINOR Development Entry  
Right-Turn only



**FIGURE 03.5-1**  
**AREA 1 and 2A**  
 Drawing 01  
 Development Scenario

PROPOSED ACTIVITIES



STROLLING



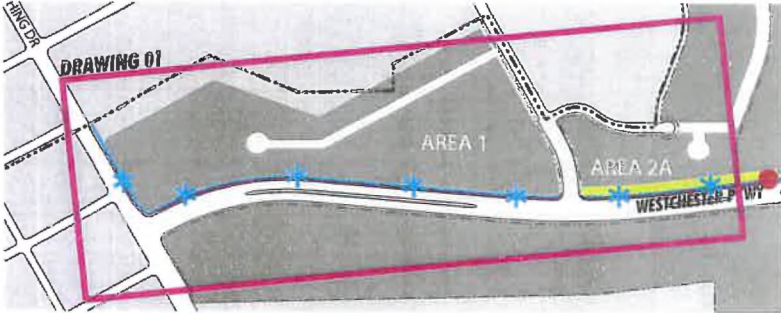
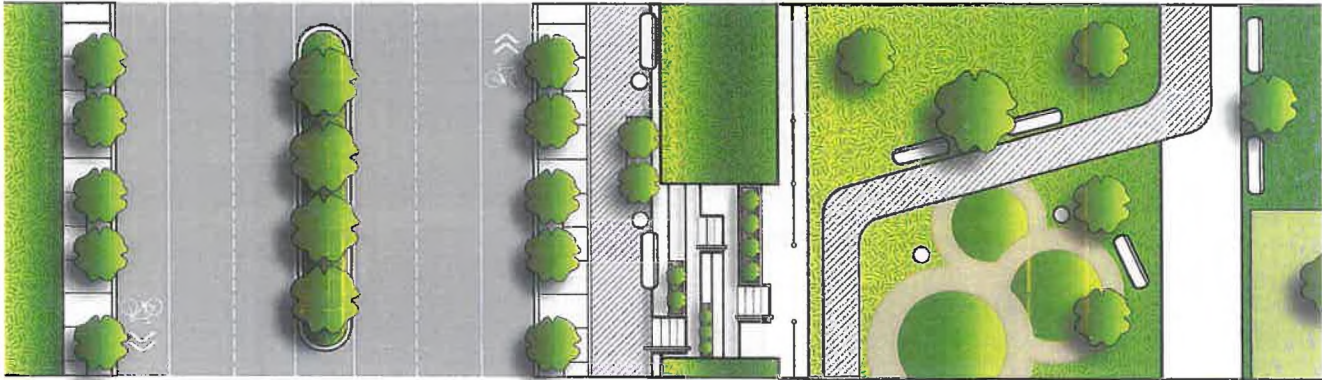
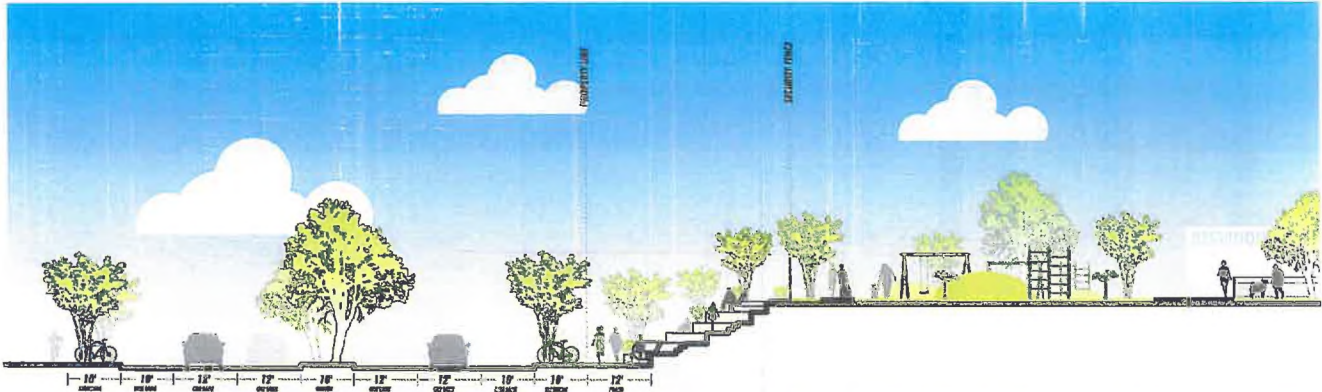
EXERCISE



OUTDOOR SEATING



ALTERNATIVE MOVEMENT





**FIGURE 03.5-2**  
**AREAS 2C, 2E, & 3**

Drawing 02

Development Scenario

PROPOSED ACTIVITIES



STROLLING



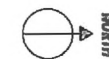
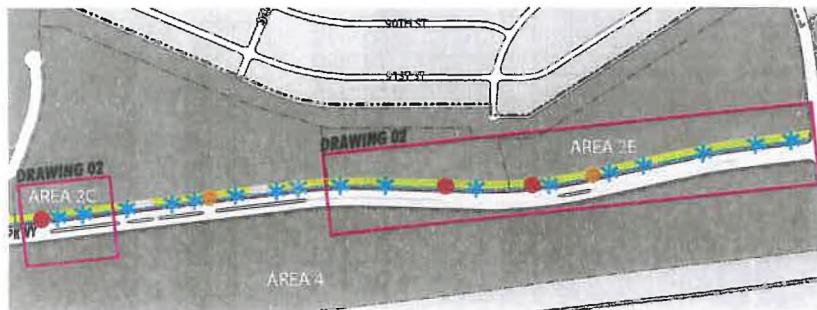
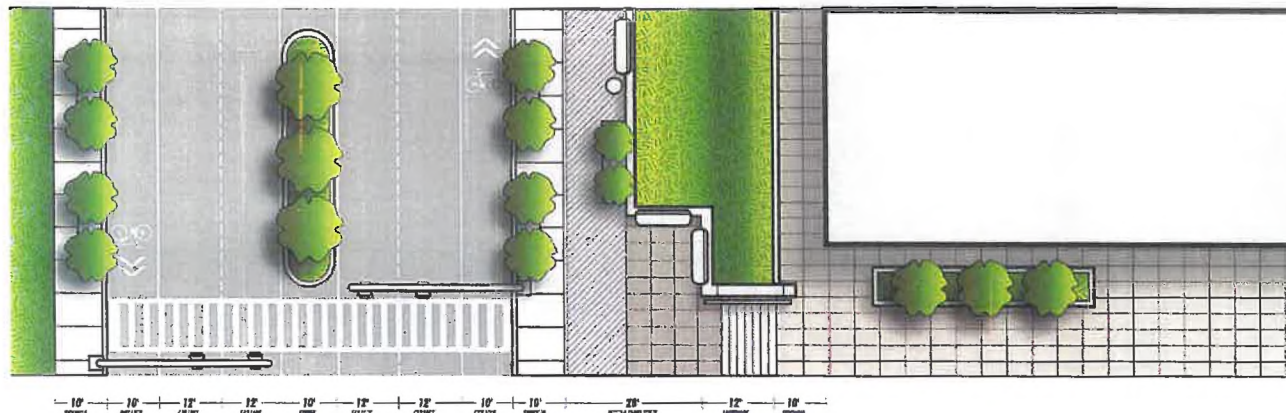
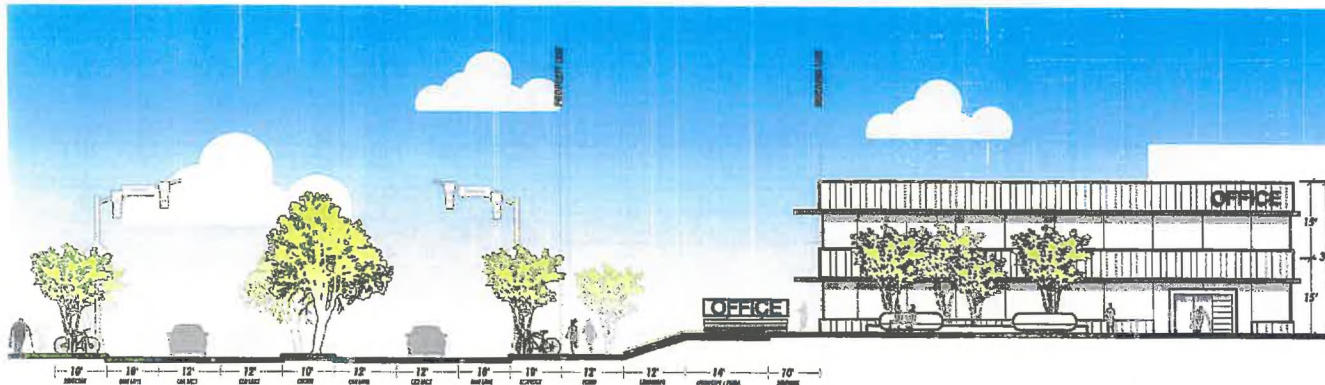
EXERCISE



OUTDOOR SEATING



ALTERNATIVE MOVEMENT





**FIGURE 03.5-3**  
**AREAS 2C, 2E, & 3**  
 Drawing 02 - Plaza Entry  
 Development Scenario

PROPOSED ACTIVITIES



BIKE RENTAL



SMALL PARKS



LARGER GROUPS



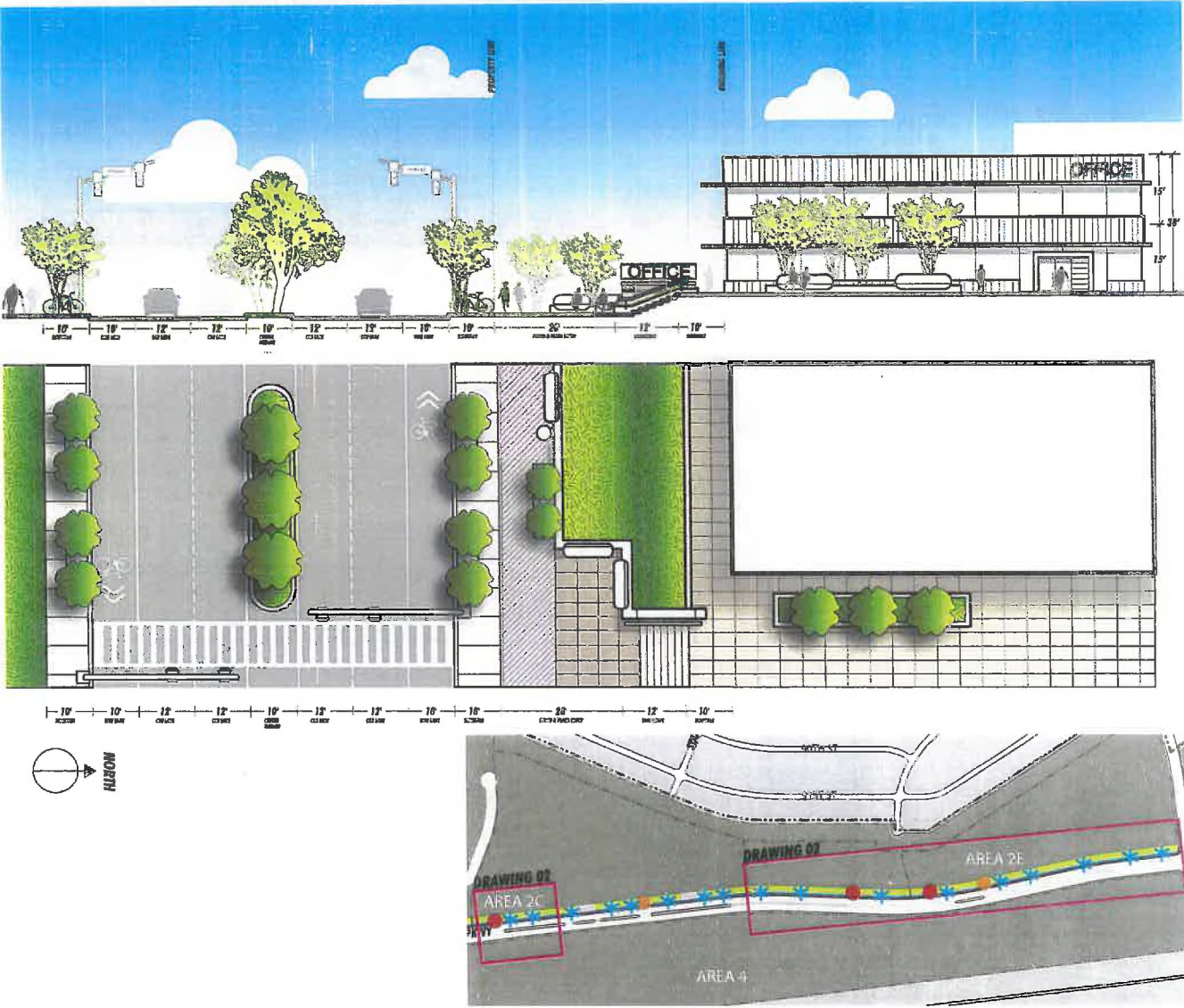
WATER FEATURES



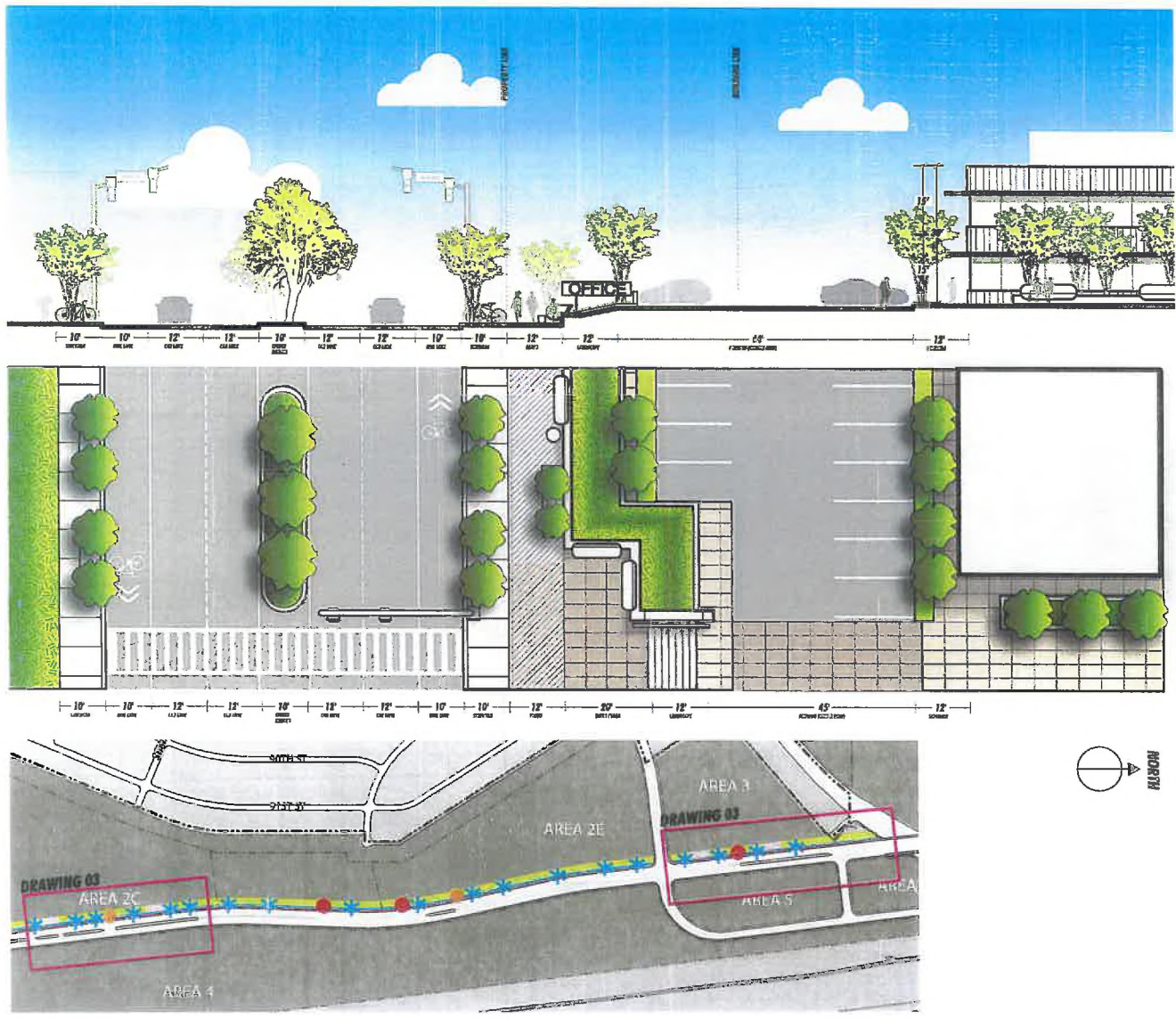
AIRPORT VIEWING



PUBLIC EVENTS







**FIGURE 03.5-4**

**AREAS 2C, 2E & 3**

Drawing 03 - Double Row of Parking  
Development Scenario

PROPOSED ACTIVITIES



STROLLING



EXERCISE



OUTDOOR SEATING



ALTERNATIVE MOVEMENT



**FIGURE 03.5-5**  
**AREAS 2C, 2E & 3**

Drawing 03 - Single Row Parking and Plaza Entry  
 Development Scenario

PROPOSED ACTIVITIES



BIKE RENTAL



SMALL PARKS



LARGER GROUPS



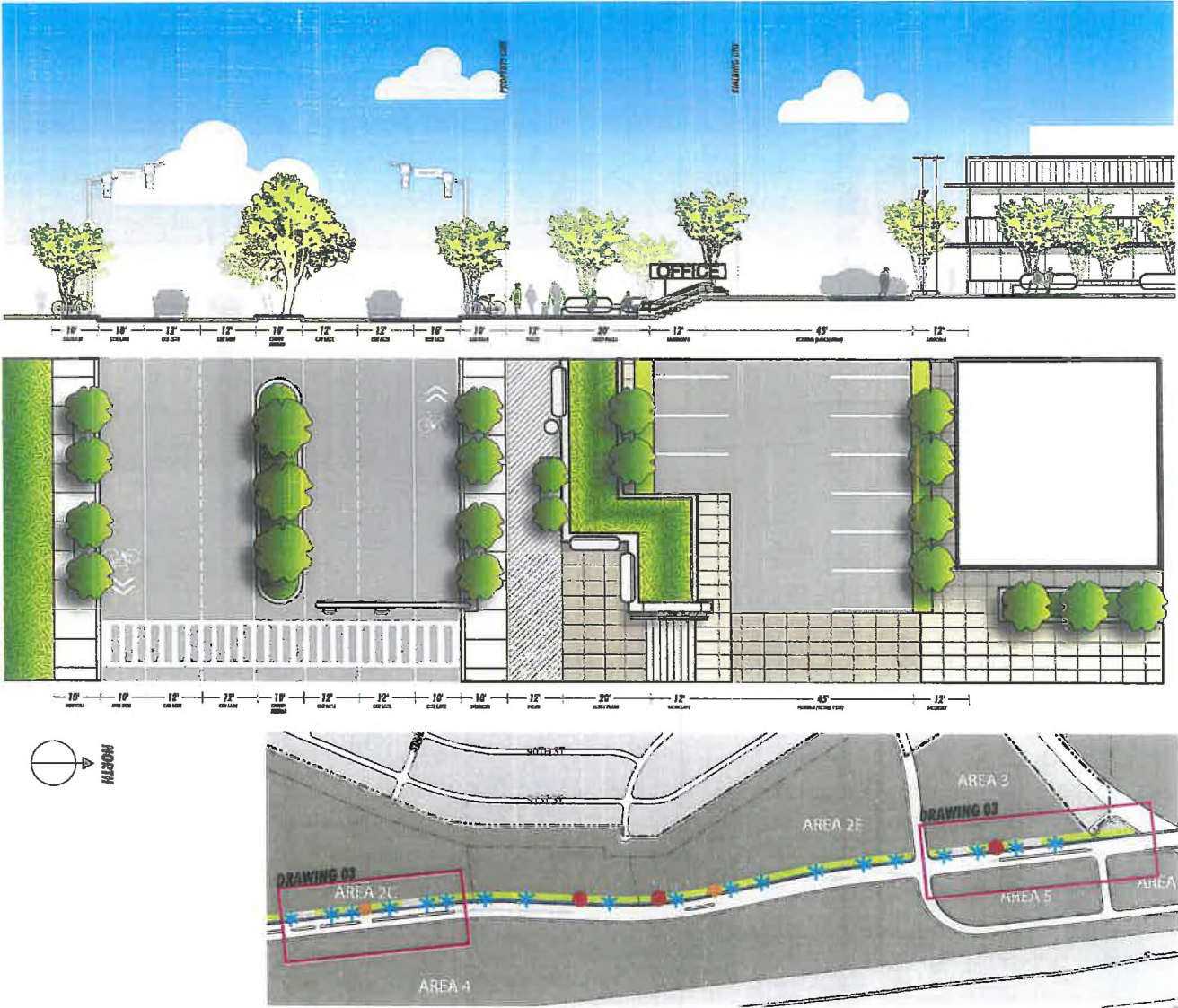
WATER FEATURES



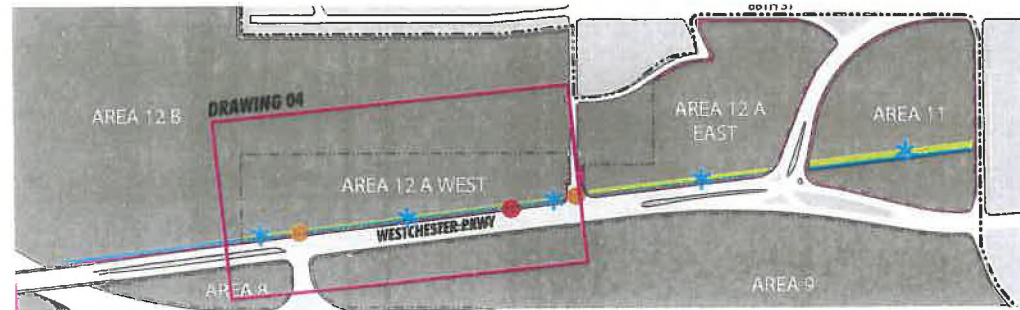
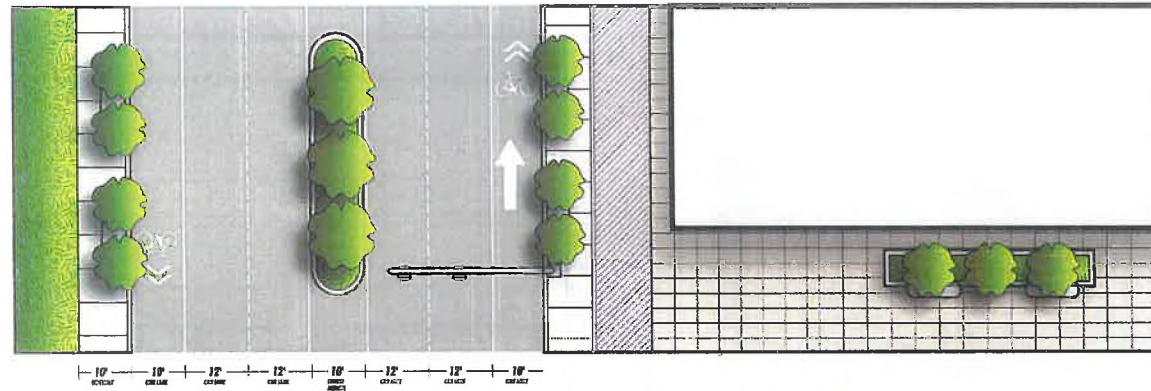
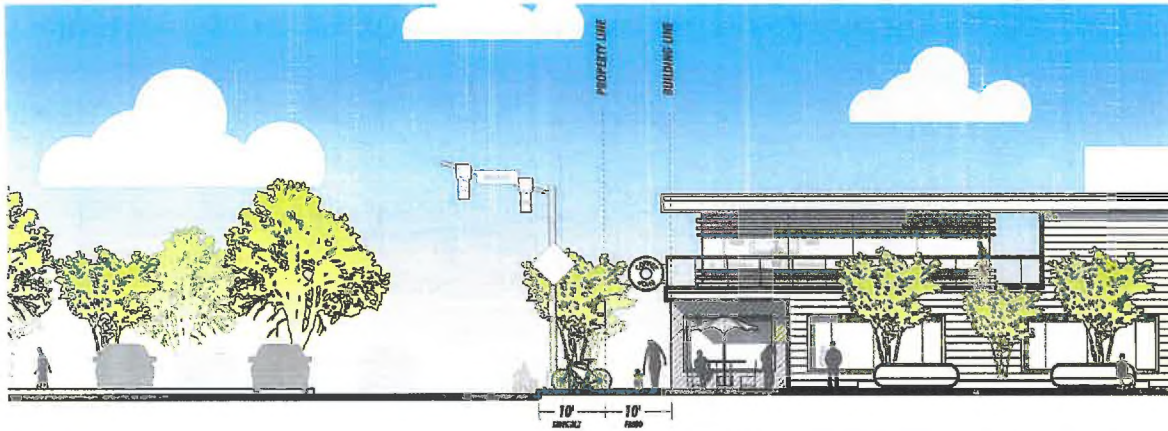
AIRPORT VIEWING



PUBLIC EVENTS







**FIGURE 03.5-6**

**AREA 12A West**

Drawing 04  
Development Scenario

PROPOSED ACTIVITIES



STROLLING



EXERCISE



OUTDOOR SEATING



ALTERNATIVE MOVEMENT

**FIGURE 03.5-7**  
**AREA 12A East**  
 Drawing 05  
 Development Scenario

PROPOSED ACTIVITIES



CAFE SPACE



SIDEWALK SALES



SMALL MARKETS



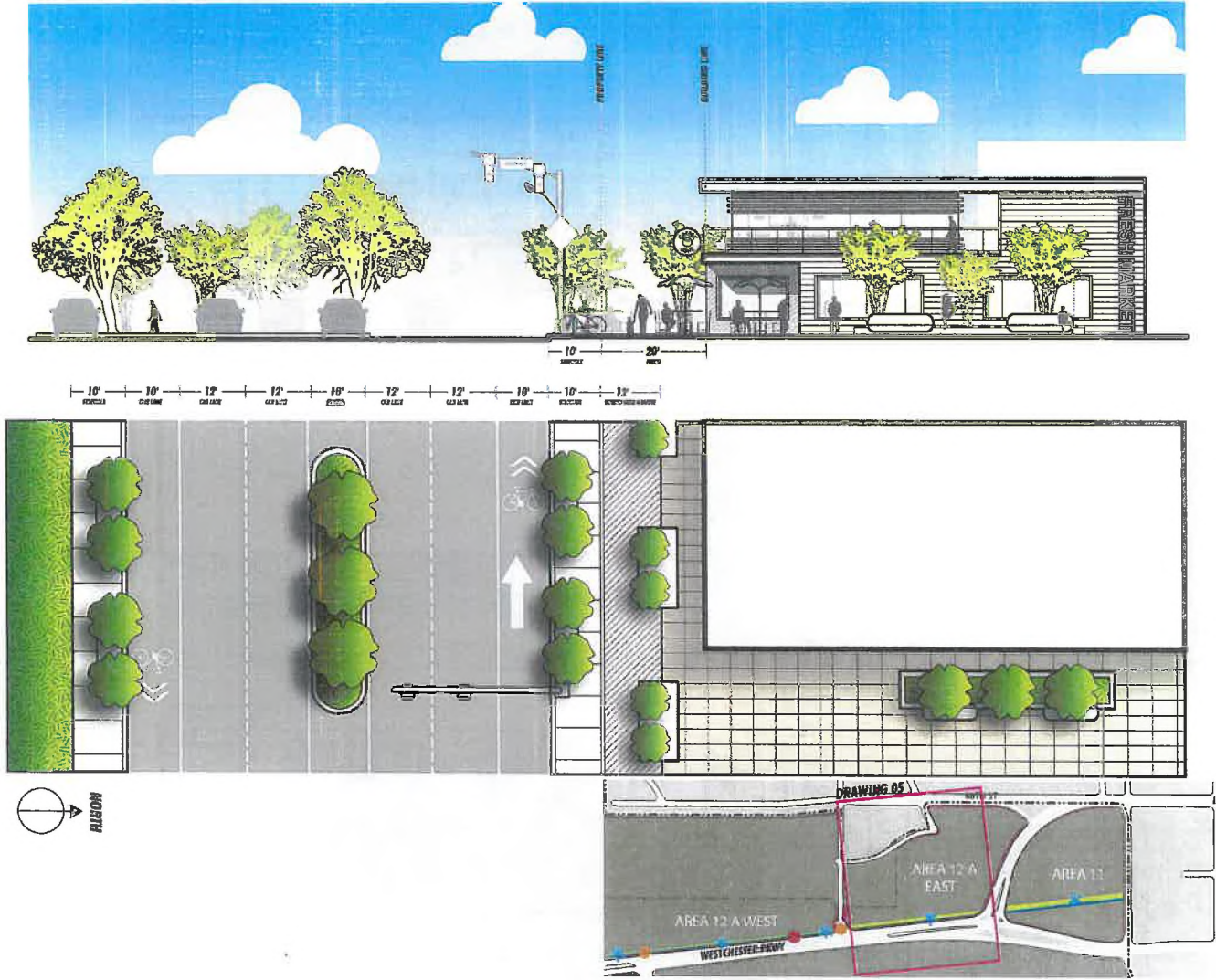
ADDITIONAL PLANTINGS



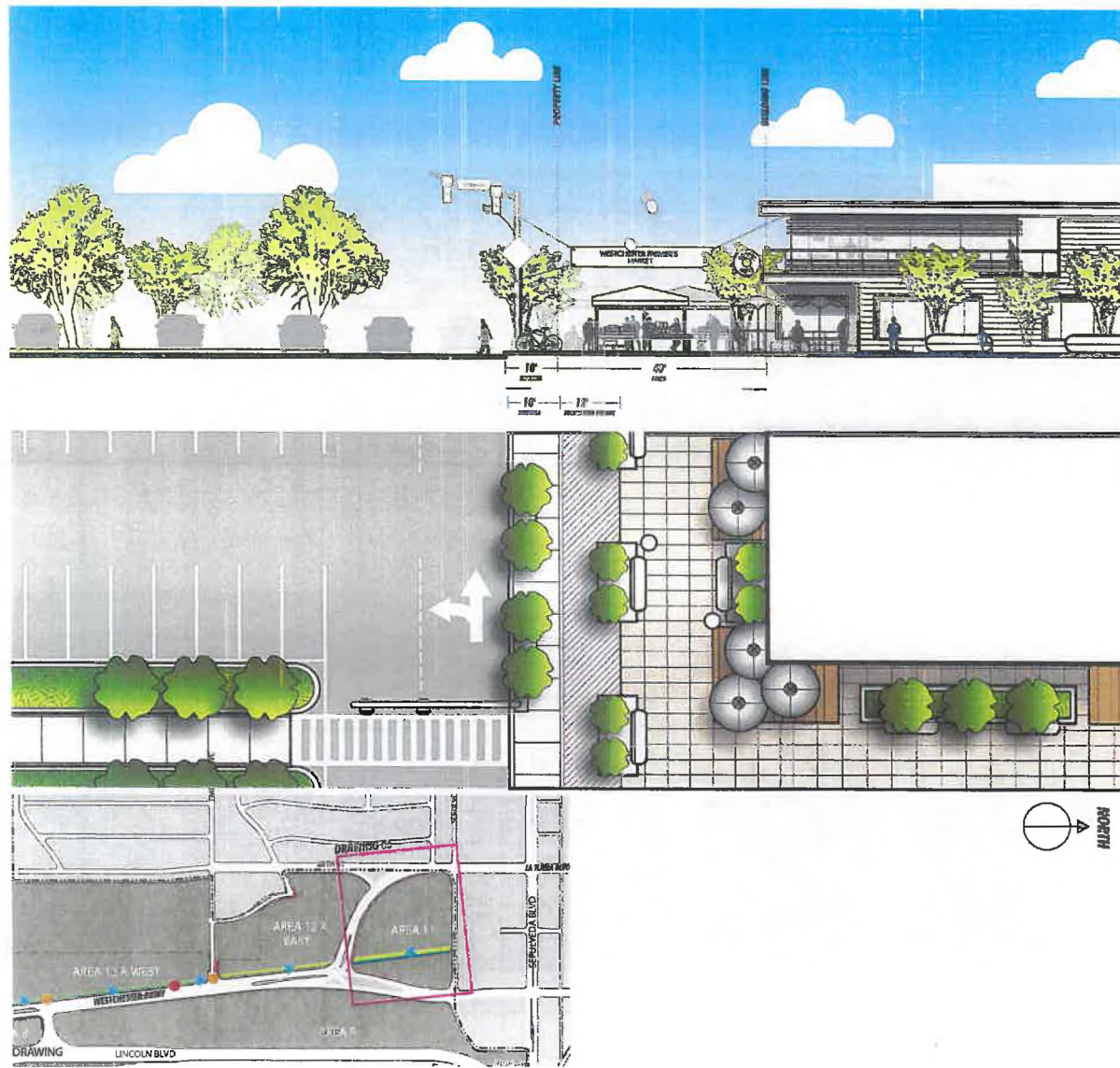
AIRPORT VIEWING



BICYCLISTS







**FIGURE 03.5-8**

**AREA 11**

Drawing 06

Development Scenario

**PROPOSED ACTIVITIES**



KIOSKS



BIKE RENTAL



SMALL PARKS



LARGER GROUPS



WATER FEATURES



FOOD TRUCKS

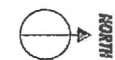
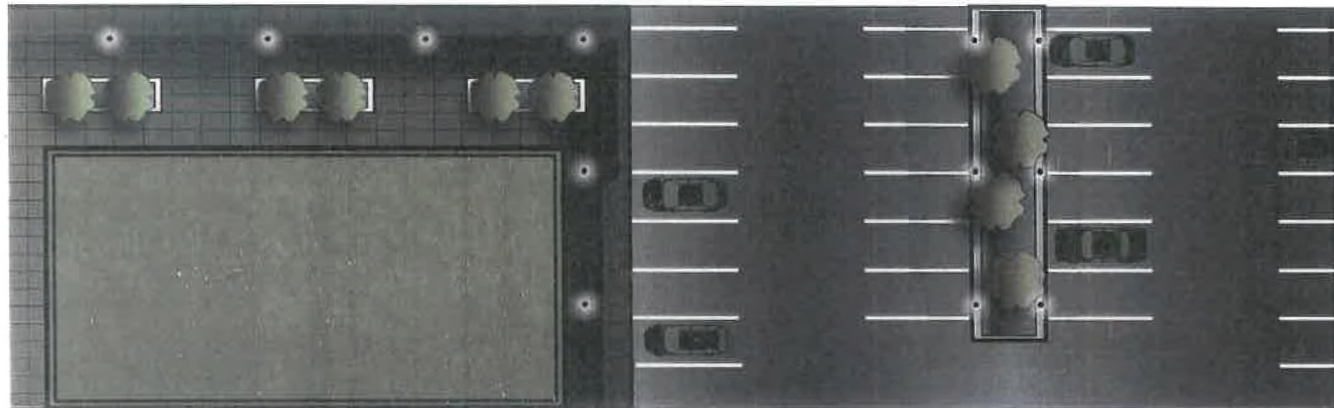


PUBLIC EVENTS







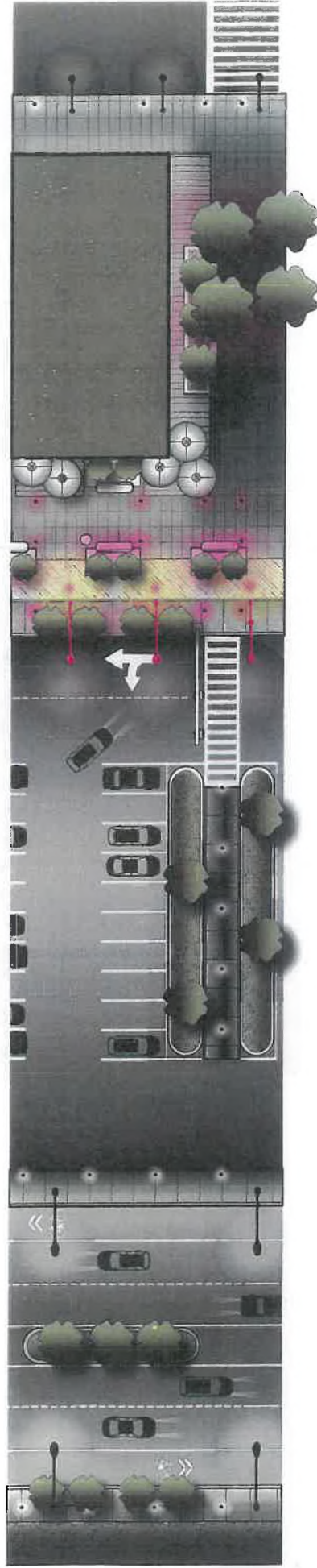


**FIGURE 03.6-1**

SITE LIGHTING - CAMPUS EXAMPLE

This Figure provides an example of site lighting in the LAX Northside Campus District. Site lighting is designed to prevent light spillover, enhance pedestrian awareness and safety,

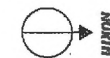
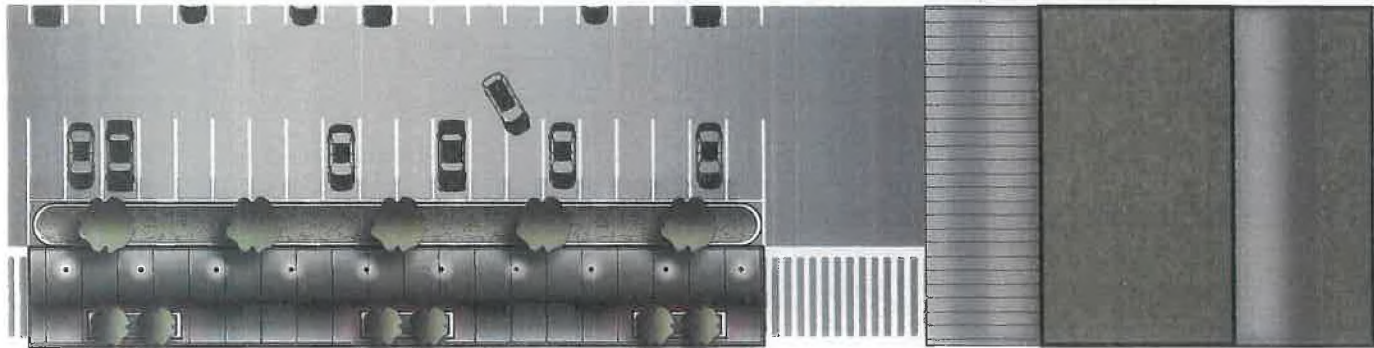






**FIGURE 03.6-2**  
SITE LIGHTING - CENTER EXAMPLE

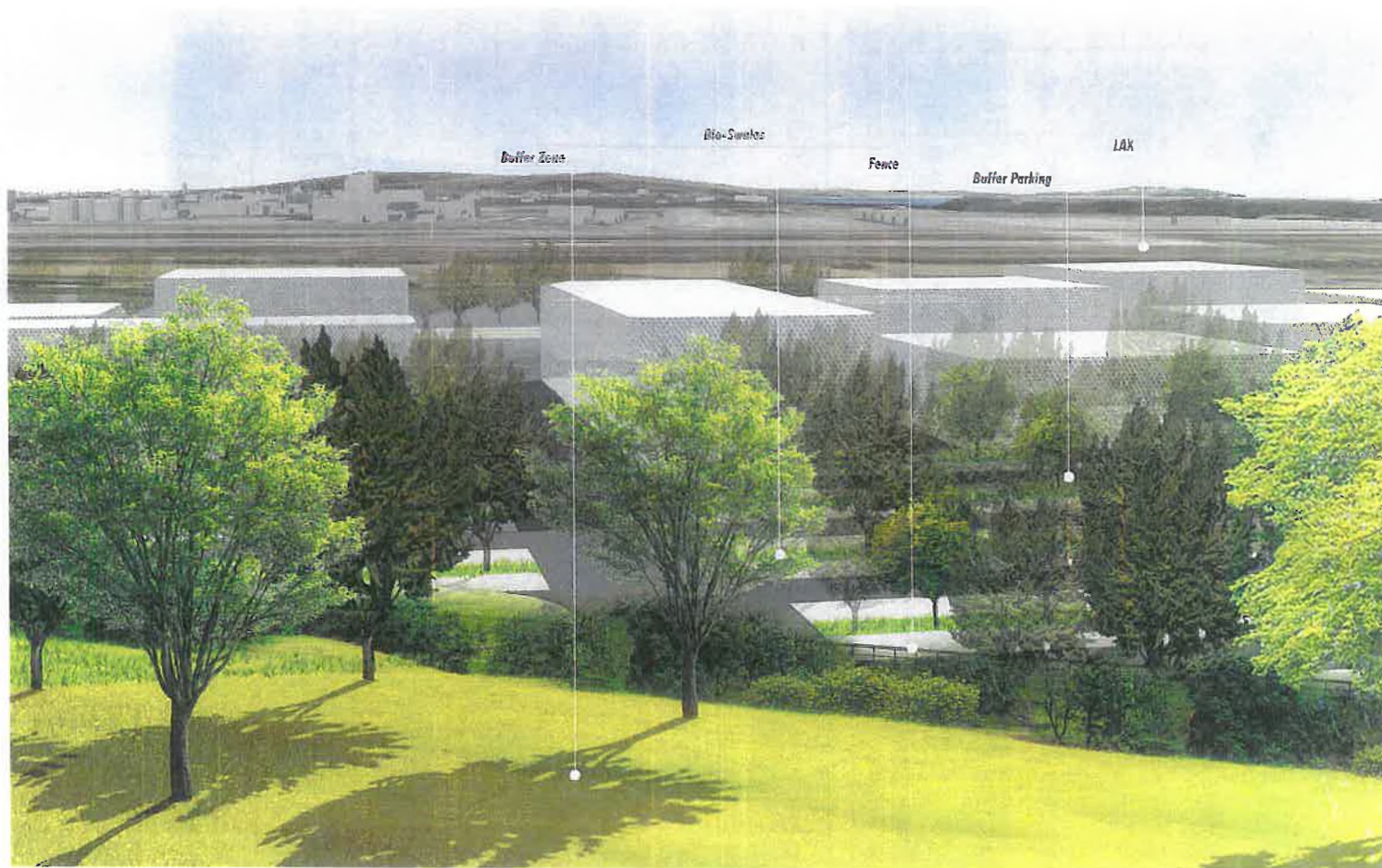
This Figure provides an example of site lighting in the LAX Northside Center District.



**FIGURE 03.7-1**

**LAX NORTHSIDE CAMPUS - OPEN  
SPACE**

This Figure presents a conceptual image for the LAX Northside Campus District Open Space overlooking the adjacent airfield, including appropriate planting materials for the specific location.







**FIGURE 03.7-2**

LAX NORTHSIDE CAMPUS - BUFFER

This Figure presents a conceptual image for the LAX Northside Campus District Buffer overlooking the area from the point of view of adjacent developments, including appropriate planting materials for the specific location.





# PART II

DESIGN GUIDELINES AND STANDARDS





# **PART II**

## **4 DESIGN REVIEW AND IMPLEMENTATION**

*Part II of this document contains the design guidelines and standards for Urban Design, Architecture, Landscape, the Paseo, and Signage and Graphics. Each section describes the overall design intent and contains specific design guidelines and standards to achieve the future vision of the LAX Northside as described in Part I of this document. This chapter describes how the LAX Northside Design Guidelines and Standards shall be implemented, summarizes the procedures for ensuring compliance, and contains the guidelines and standards to be used during project design review.*

### **AUTHORITY**

The Executive Director shall have the authority to review each project for compliance with all applicable provisions of the LAX Specific Plan and LAX Northside Design Guidelines and Standards. Except as provided in the LAX Specific Plan, no grading permit, foundation permit, building permit, use of land permit, or permit for a change of use shall be issued for any project on any lot located in whole or in part within the LAX Northside unless an approval has been issued pursuant to the procedures set forth in the LAX Specific Plan.

- No approval shall be issued for a project that would cause overall development in the LAX Northside to exceed the Site Development Standards of the LAX Specific Plan.
- No approval shall be issued unless the project complies with all applicable provisions of the LAX Specific Plan, including compliance with all applicable Project Design Features and Mitigation Measures and the LAX Northside Design Guidelines and Standards.

### **IMPLEMENTATION PROCEDURES**

Executive Director Approval shall review the applicable provisions of the LAX Specific Plan and determination of whether a project is in conformance with those regulations and these design guidelines and standards.

### **DESIGN REVIEW**

Part II of this document contains design guidelines as well as standards. As illustrated below, Design guidelines are recommendations that should be considered and are encouraged to be implemented. Design standards are requirements that shall be adhered to in order to achieve compliance with the LAX Northside Design Guidelines and Standards. Design standards are indicated in most instances with checkboxes, while guidelines have no corresponding checkbox, as indicated in the diagram at the end of this chapter.



#### APPLICATION MATERIALS

Applications for approval shall include, but not be limited to:

##### Floor Area Calculations

Every application for approval shall include a table that identifies the following:

- Proposed project building floor area.
- New total of developed building floor area within the LAX Northside.
- Total remaining allowed building floor area within the LAX Northside.
- New total developed building floor area for all areas within the LAX Northside.
- Total remaining allowed building floor area within the LAX Northside.

##### Trip Generation Calculations

Every application for approval shall include a table that identifies the following:

- Proposed project trip generation.
- New total trip generation within the LAX Northside.
- Total remaining allowed trip generation within the LAX Northside.

As illustrated below, Design Guidelines are recommendations that should be considered and are encouraged to be implemented. Design Standards are requirements that shall be adhered to in order to comply with the LAX Northside Design Guidelines and Standards.



### **REQUIRED**

SHALL be adhered to and indicated in most instances with a checkbox ☐



### **ENCOURAGED**

SHOULD or are ENCOURAGED to be included

# PART II

## 5 URBAN DESIGN

*“Urban design” refers to all spaces and infrastructure that provide character, identity and form to the overall built urban environment in the LAX Northside. The purpose of the urban design guidelines and standards is to establish a framework for the development of the built environment within the LAX Northside, and to shape the design of future projects. These guidelines and standards are intended to achieve compatibility with adjacent communities, while maintaining the flexibility needed to respond to market conditions and reflecting the latest best practices in sustainability.*

The urban design concepts contained herein differentiate the LAX Northside as a unique area within the Los Angeles region. By taking advantage of the LAX Northside’s location between Los Angeles International Airport and the existing business district in Westchester, the LAX Northside Design Guidelines and Standards support new

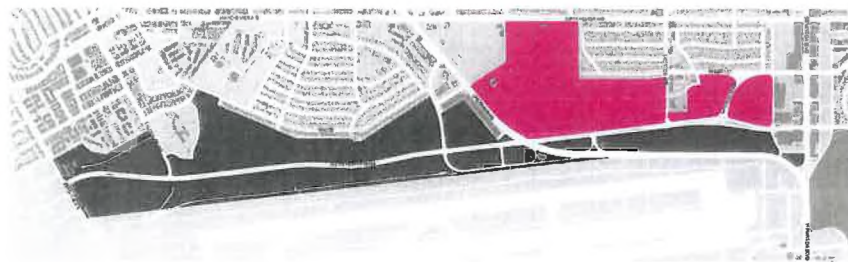
development that will provide a seamless transition and buffer adjacent to the Westchester and Playa Del Rey neighborhoods. The LAX Northside Design Guidelines and Standards address the sensitivity of neighboring uses by focusing activity and development along Westchester Parkway. This strategy will provide consistency in the design of the urban environment and will establish secure boundaries between future developments and existing neighbors.

In addition to defining urban design concepts, these guidelines and standards include sustainable practices as an integral aspect of the overall design direction. These design strategies focus on supporting active lifestyles and existing recreational activities. The LAX Northside facilitates a sustainable and healthy environment by nurturing active living through design and providing pedestrian-scaled infrastructure. The primary design elements that unify the project include:

1. Concentrating building density closest to Westchester Parkway to create a buffer from the adjacent neighbors and to establish a vibrant and active street frontage;
2. Preserving and enhancing existing recreational activities in the community through the creation of a pedestrian paseo along Westchester Parkway;
3. Creating a comfortable and active pedestrian experience along the length of the paseo, spacing development entrances at walkable distances, providing variable depths of space to accommodate a variety of active and passive activities, and providing pedestrian amenities throughout the project area;
4. Complementing the established linear character of the adjacent runways by planting a double row of trees across the LAX Northside to buffer future development; and
5. Using massing strategies that address the pedestrian scale.

Within the LAX Northside, three primary districts have been defined to help differentiate various design strategies.





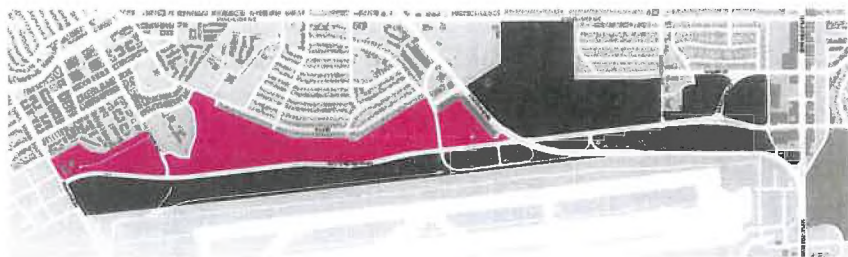
#### LAX NORTHSIDE CENTER DISTRICT

The LAX Northside Center District will be a low to mid-rise, retail and office environment extending from Sepulveda Westway to Lincoln Boulevard, incorporating Areas 11, 12A East and West, 12B (existing Westchester Golf Course) and 13. Vehicular access will be allowed primarily off of Westchester Parkway, with secondary access allowed along La Tijera Boulevard and Sepulveda Westway. Building setbacks and setbacks along Westchester Parkway and La Tijera Boulevard will accommodate a pedestrian environment that is consistent with the proposed paseo and promotes pedestrian activity along primary building frontages.

Pedestrians will access the development through plazas, connecting existing site conditions along Sepulveda Boulevard into the new developments. Transportation stations (for either bus or future light rail) are also accommodated in this mixed office and commercial use environment. In addition, future developments will provide bike racks and carpool parking to help promote alternative forms of transportation and trip reduction.

In Areas 11 and 12A East, buildings will frame a series of small, internal landscaped open spaces. In Area 11, building massing will be allocated to the periphery of the uniquely shaped parcel, providing massing along the edges of Westchester Parkway and La Tijera Boulevard. Along the La Tijera edge, the existing soundwall will be preserved and building frontages will be required to step back as height increases in order to provide privacy for adjacent neighbors. By locating building massing around the area's edges, the internal space of the parcel will be available for surface parking and pedestrian connections in necessary locations. Similar massing strategies will be used in Area 12A East as a means to reinforce activity along Westchester Parkway. In the northern portion of the area, a parking garage is allowed that will maintain privacy for adjacent neighbors through the location and articulation of the parking garage's facade. In Areas 12A West and 13, civic and community buildings along Westchester Parkway and Lincoln Boulevard will maintain pedestrian scaling through setbacks and height limits (20 to 60 feet).

Abundant annual blooms will provide color and attraction to the pedestrian environments of the Northside Center District. Landscape setbacks and general landscape materials in the LAX Northside Center District will be comprised of a fifty-fifty split between native and non-native species. Where landscape setbacks are required, privacy between adjacent neighbors will be preserved through the articulation of a planted edge. With the majority of space being designated for surface parking, planting materials will be designed to be capable of managing storm water and runoff, including through use of bioswales. These landscaping requirements in parking areas will help provide an additional buffering screen to adjacent neighbors and will help to define a sense of beauty throughout the district.



#### LAX NORTHSIDE CAMPUS DISTRICT

The LAX Northside Campus District will be a low-rise, low density office or research and development park extending from Lincoln Boulevard to Falmouth Avenue. Site access will be controlled and project entry points will become major design features along Westchester Parkway, incorporating signage and landscape elements. Along the north side of Westchester Parkway, buildings will maintain a relationship to the street, but will be diverse in terms of siting, design, and building clustering. Internal to the parcels, wider setbacks are required at major access points, while minimal building setbacks are required everywhere else creating a campus-like environment (see Figure 05.2 for specific setback requirements). Additionally, 65% of building square footage is required to be adjacent to Westchester Parkway. These design strategies reinforce a pedestrian scale within each development, while providing the opportunity for integration with the paseo.

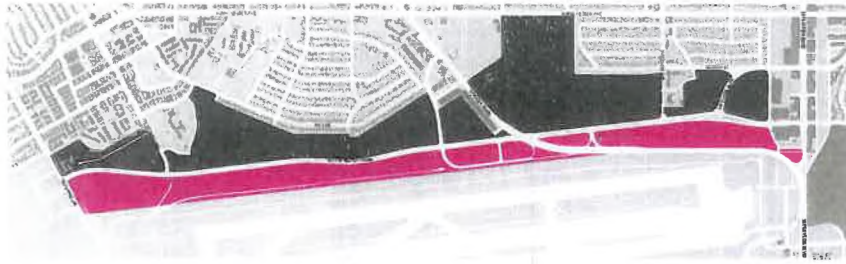
Grading strategies, building height limits (ranging from 45-60 feet), and density allocation requirements will limit the visual impact of the new developments on neighboring residences where possible. In Areas 2 and 3, grading

strategies will lower elevations for building frontages, providing a more accessible relationship with Westchester Parkway. In Area 1, existing grading will be preserved to separate planned open spaces from the busy nature of Westchester Parkway. In all areas, landscaping specific to the streetscape, setbacks, and campus environments will combine native and non-native materials that create a dynamic experience with annual blooms and vibrant colors articulated through a series of trees, shrubs and groundcover.

Landscape Buffers are required in two separate locations in the LAX Northside Campus District. Buildings, parking, and pedestrian access are prohibited in these areas. The Landscape Buffers will be secured on all of their sides to prevent access. A 100-foot Landscape Buffer is located on the northern property line in Parcel 2, and a 20-foot Landscape Buffer is located along the northwest property line in Parcel 1. These buffers will be planted primarily with locally-native trees, shrubs and ground cover, and when needed, will provide appropriate ground cover to control erosion. Existing trees will be preserved if they are compatible with the landscape material palettes in these guidelines and standards. The Landscape Buffer will function as a visual screen that physically separates the proposed land uses from the adjacent neighborhoods.

Recreation areas, which are open spaces designed to accommodate active and passive forms of recreation, including, but not limited to soccer, lacrosse, baseball, and dog parks shall be primarily allocated to Areas 1 and the western portion of Area 2. All recreation spaces will be secured with a perimeter fence and will operate with established hours of operation, and lighting will be designed to prevent light spillover. Parking will be provided for all recreation areas consistent with Los Angeles City Code requirements. Recreation areas will provide additional amenities such as ancillary buildings for storage, recreation centers, pedestrian pathways, and compliant access for handicapped individuals. Recreation areas will be planted with a required palette that is eighty (80) percent locally-native and twenty (20) percent non-native creating a landscape that is specific to the LAX Northside's location with the occasional pop of vibrant color and dynamic annual change.





#### LAX NORTHSIDE AIRPORT SUPPORT DISTRICT

The areas south of Westchester Parkway will be comprised of low-rise (30 foot height limit), light industrial structures, with the majority of building density located in Area 4. The existing site entrance and security checkpoint at the intersection of Falmouth Avenue and Westchester Parkway will be maintained, allowing a secured access point for employees. Grading strategies and landscape berms will be preserved as they exist today, limiting the visual presence of this area from the point of view of neighbors north of Westchester Parkway. Where applicable, additional grading and landscape berms may be introduced to further enhance these design ideas.

In addition to the existing landscape materials found within these areas, new materials will be introduced that are locally-native, drought tolerant, and require little to no maintenance. Landscape materials will be limited to shrubs and ground cover, and when applicable, existing materials that are compatible with these guidelines and standards will be preserved. In addition, newly introduced plant materials will be composed of eighty (80) percent

locally native and twenty (20) percent non-native species, creating a composite, low-lying landscape. Along existing security fence boundaries, replacement trees or shrubs may be introduced in the event of an existing tree dying or becoming damaged. These replacement trees have been selected to prevent illegal access into the airfield by having minimal branch strength and density.







**FIGURE 05.1**

LAND USE MAP

This Figure provides information specific to the location and distribution of land uses in the LAX Northside.



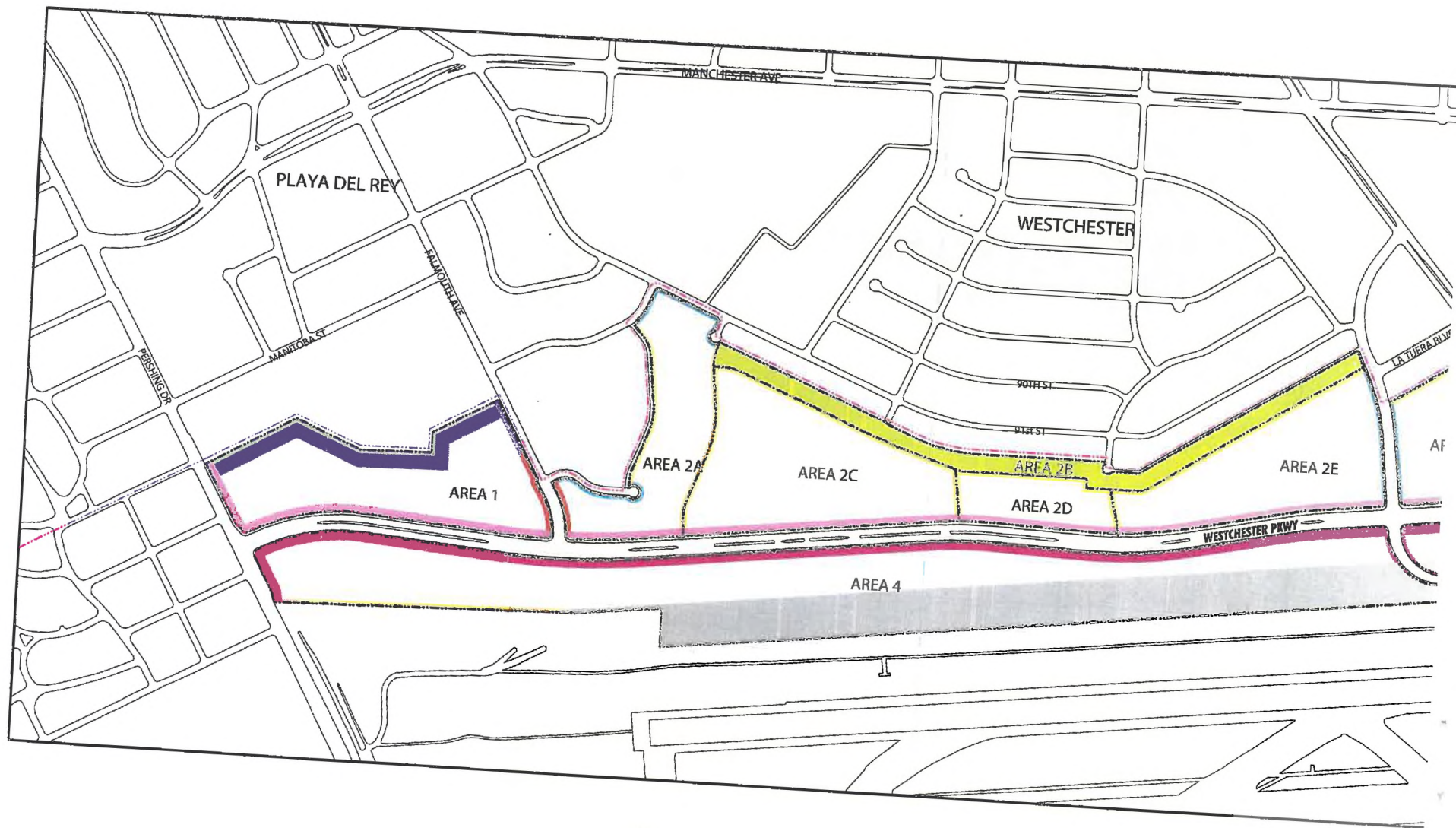
## LEGEND

### BOUNDARIES

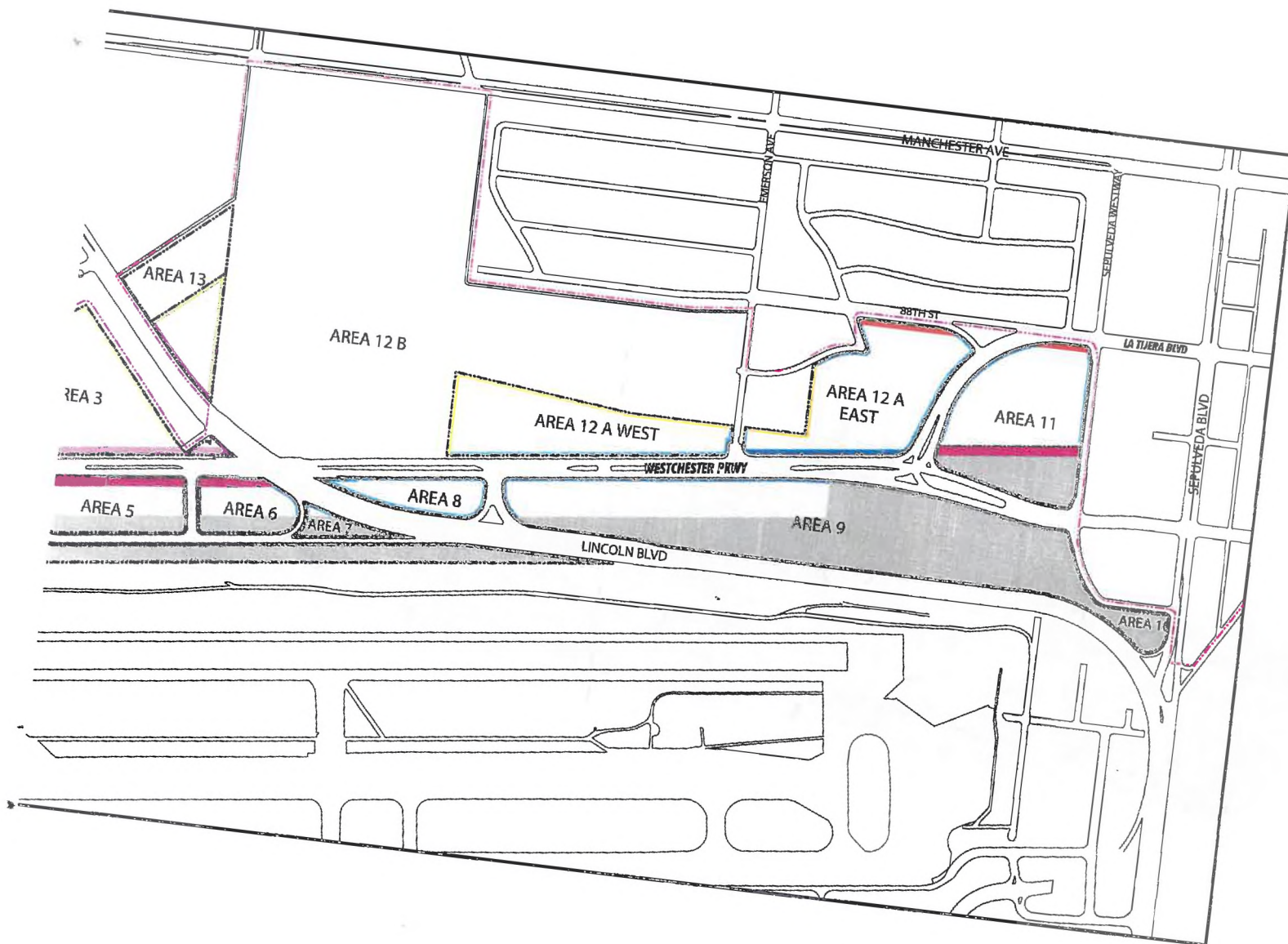
- PROJECT BOUNDARY
- AIRPORT PROPERTY BOUNDARY
- DISTRICT BOUNDARY

### PERMITTED LAND USES

- OFFICE, or RESEARCH and DEVELOPMENT USE (OR)
- COMMUNITY or CIVIC USE (CC)
- MIXED USE- COMMERCIAL (MU)
- AIRPORT SUPPORT (AS)
- RECREATION and OPEN SPACE (OS)
- BUFFER USES (B)







**FIGURE 05.2**  
BUILDING SETBACKS

This Figure provides information specific to the location of all regulated building setbacks in the LAX Northside.



### LEGEND

#### BOUNDARIES

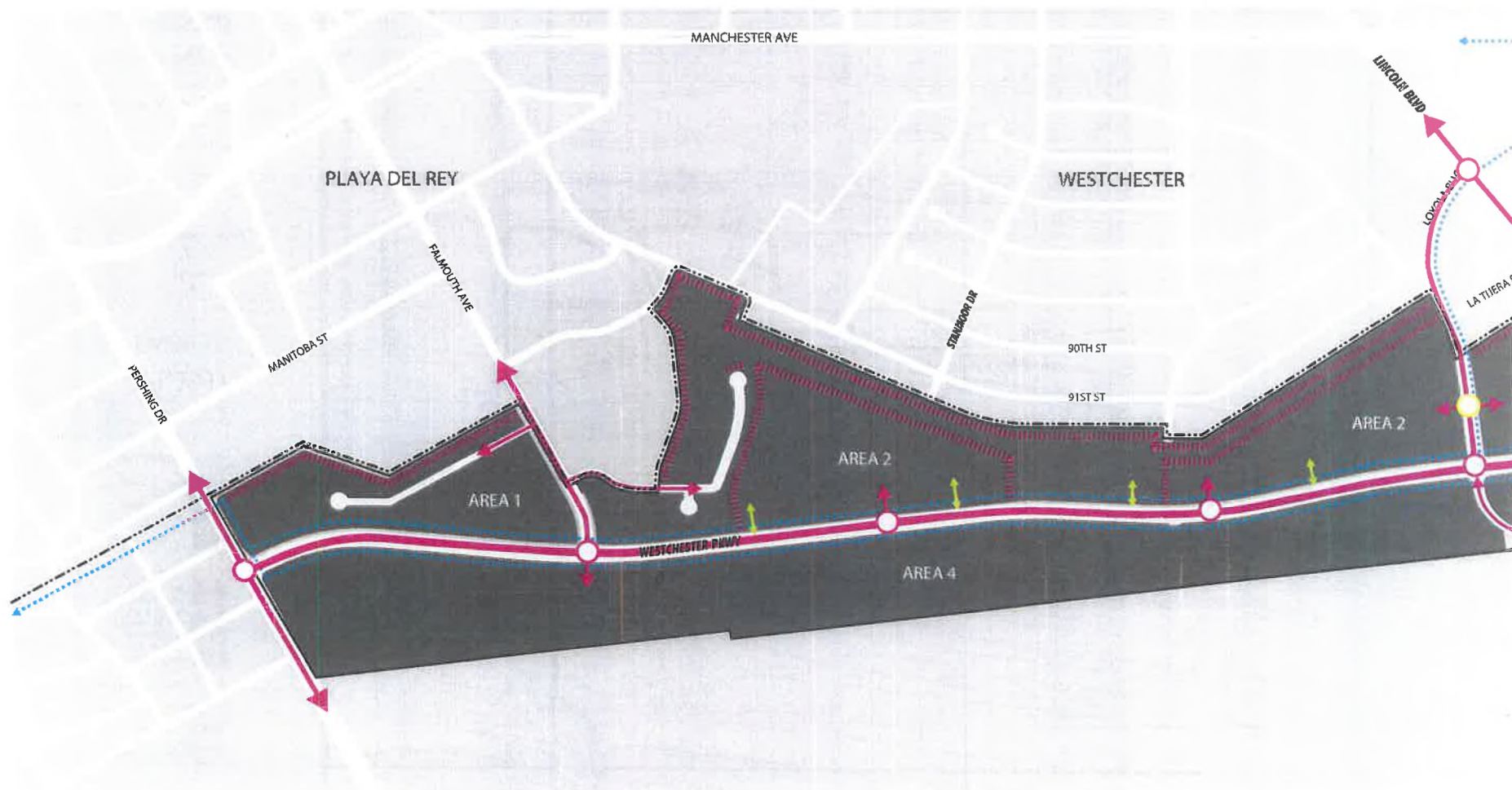
- AIRPORT PROPERTY BOUNDARY
- PROPERTY LINES

#### REQUIRED SETBACKS

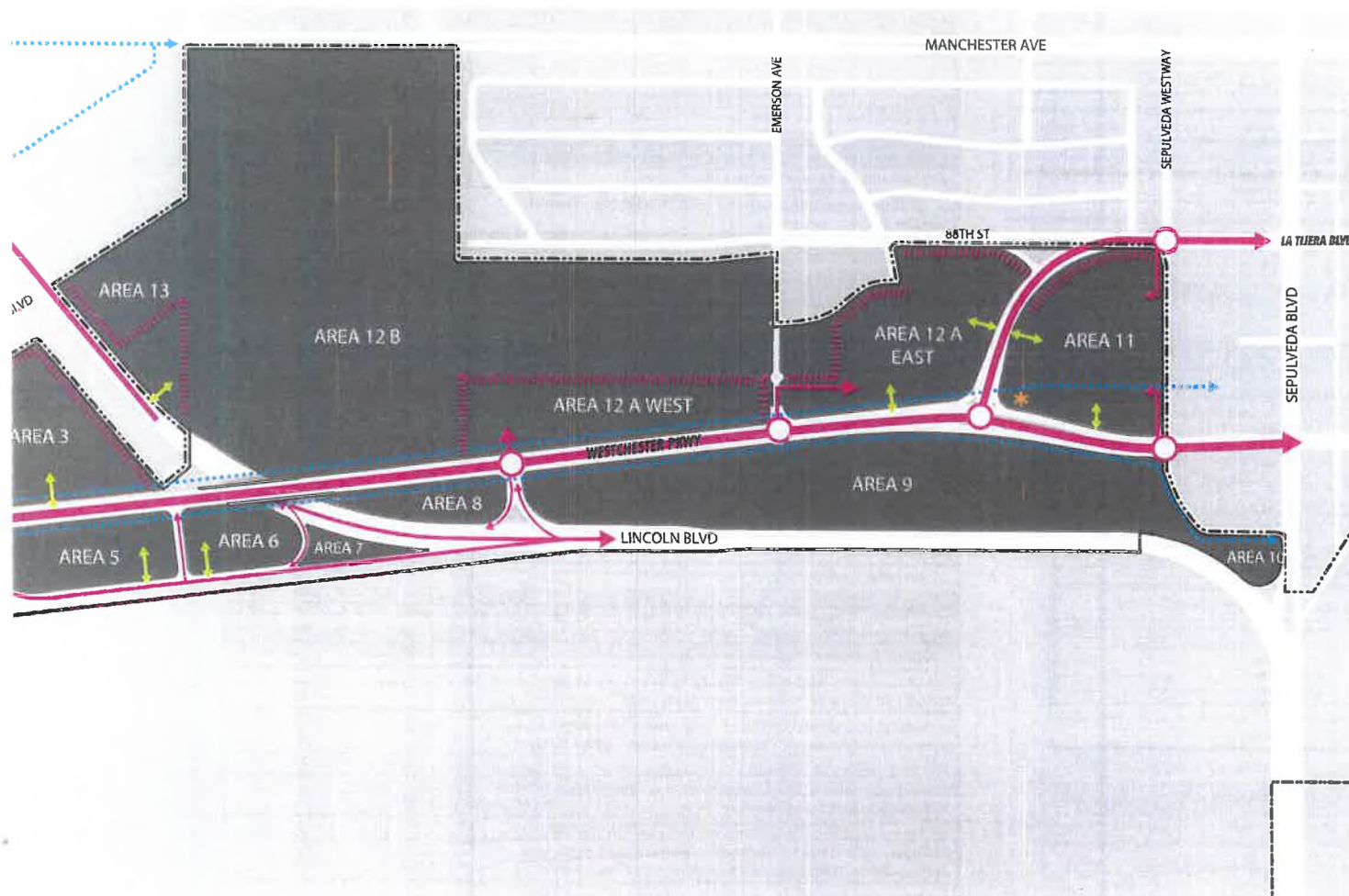
- 15' SETBACK
- 18' SETBACK
- 20' SETBACK
- 30' SETBACK
- 38' SETBACK
- 50' SETBACK
- 80' SETBACK
- LIMITED DEVELOPMENT AREA SETBACK

#### REQUIRED BUFFERS

- 20' BUFFER
- 100' BUFFER







**FIGURE 05.3**

**CIRCULATION & ACCESS**

This Figure depicts vehicular entrances, access points, and circulation paths.



## URBAN DESIGN GUIDELINES AND STANDARDS CHECKLIST

### 5 Urban Design Guidelines and Standards

All projects within the LAX Northside shall comply with the LAX Specific Plan and the standards and guidelines contained within this document. Additional information that illustrates and informs the overall concepts associated with the urban design approach can be found in Chapter 3: Vision.

#### 5.1 Land Use

- ☐ **A. Maximum Permitted Floor Area**  
The maximum total permitted Floor Area within the LAX Northside shall not exceed 2,320,000 square feet.
- ☐ **B. Vehicle Trip Cap**  
The maximum permitted total daily vehicle trips generated by the LAX Northside shall not exceed 23,635 trips.
- ☐ **C. Project Land Use**  
The LAX Northside shall be developed with the land uses as defined by Section E and G below. These land uses shall be developed in the Districts, as shown on Figure 05.1, the Land Use Map.
- ☐ **D. Designation of Sub-Areas**  
The LAX Northside contains three Districts and 15 Areas, as shown on the District Map, Figure 01.1. The Districts are designated as: LAX Northside Center District, LAX Northside Campus District, and LAX Northside Airport Support District. The Areas are designated as Area: 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12A East, 12A West, 12B, and 13.
- ☐ **E. Floor Area**  
The LAX Northside shall be developed with the development capacity for each district as defined in the following table:

MAXIMUM FLOOR AREA PER DISTRICT	
District	Building Square Footage
LAX Northside Center District	645,000
LAX Northside Campus District	1,075,000
LAX Northside Airport Support District	600,000

Table 05.1.1

- ☐ **F. Transfer and Equivalency Program**  
Transfers of floor area between all Areas within a District are permitted, however, transfers between LAX Northside Districts is prohibited. In no event shall the maximum permitted floor area within the LAX Northside exceed 2,320,000 square feet of floor area. Transfers and equivalencies shall conform to the Land Use Equivalency Program (Section 5.3).
- ☐ **G. Permitted Land Uses**  
Permitted land uses are indicated with "X" for each Area in the

PERMITTED LAND USE TABLE			
LAND USE	AREAS/DISTRICT		
	1-3 Campus	4-10 Airport Support	11-13 Center
<b>Buffer Uses (B)</b>			
a. Undeveloped, landscape buffer areas	X		X
b. Underground infrastructure and utilities, with limited related surface structures	X		
c. Ancillary uses and structures related to the primary permitted uses	X		
<b>Community or Civic Uses (CC)</b>			
a. Non-profit businesses or institutions that serve the local community	X		X
b. Outside recreation related to non-profit institutions such as swimming pools and athletic courts/fields	X		X
c. City, county or state government buildings including senior center, police stations, fire stations and libraries	X		X
d. Cultural institutions such as performing arts or museums	X		X
e. Other community-serving uses	X		X
f. Non-profit club or lodge	X		X
g. Underground infrastructure and utilities, with limited related surface structures	X		X
h. Ancillary uses and structures related to the primary permitted uses	X		X
i. Farmer's market	X		X
<b>Office, Research &amp; Development (OR)</b>			
a. Community and Civic (CC) land uses	X		
b. Office, general business or professional	X		
c. Medical or dental office, including surgery center, outpatient services, primary care clinic, and pharmacy	X		
d. Research and development including office, engineering, showroom, laboratory. Limited test and assembly of not-for-sale prototypes is permitted	X		
e. Business college, professional or scientific school or college, not including trade schools involving shop work, or the repair or maintenance of machinery or equipment	X		
f. Media, post-production, motion picture, or broadcast studio, without transmission towers	X		
g. Animal care, kennel, or boarding facility	X		
h. Underground infrastructure and utilities, with limited related surface structures	X		



PERMITTED LAND USE TABLE			
LAND USE	AREAS/DISTRICT		
	1-3 Campus	4-10 Airport Support	11-13 Center
i. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14	X		
j. Farmer's market	X		
k. Ancillary uses and structures related to the primary permitted uses	X		
<b>Recreation and Open Space (OS)</b>			
a. Golf course	X		X
b. Athletic fields, outdoor athletic courts	X		X
c. Public shade structures, picnic areas and rest rooms	X		X
d. Dog park	X		X
e. Below grade storm water treatment facilities	X		X
f. Underground infrastructure and utilities, with limited related surface structures	X		X
g. Farmer's market	X		X
h. Ancillary uses and structures related to the primary permitted uses	X		X
<b>Mixed Use - Commercial (MU)</b>			
a. Banks or financial institutions			X
b. Retail, including merchandise sales and local services such as bakery, barber shop, beauty shop, book store, stationery store, software or computer store, toy store, clothes cleaners, tailors, florist or gift shop, real estate, hardware or appliance store or jewelry store			X
c. Professional office			X
d. Restaurants, including sit down, quick serve, and drive through			X
e. Hotel, including related restaurants, services and parking			X
f. Transit station	X	X	X
g. Medical or dental office, including surgery center, outpatient services, primary care clinic and pharmacy			X
h. Animal medical clinic			X
i. Parking	X		X
j. Wholesale stores			X
k. Auto repair or service			X
l. Auto fueling stations			X

PERMITTED LAND USE TABLE			
LAND USE	AREAS/DISTRICT		
	1-3 Campus	4-10 Airport Support	11-13 Center
m. Farmer's market			X
n. Underground infrastructure and utilities, with limited related surface structures			X
o. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14			X
p. Ancillary uses and structures related to the primary permitted uses			X
<b>Airport Support Uses (AS)</b>			
a. Maintenance and repair shops		X	
b. Indoor storage and warehouses		X	
c. Exterior storage		X	
d. Administrative offices		X	
e. Radars and surveillance facilities	X	X	X
f. Utilities and utility-related structures		X	
g. Construction material temporary storage		X	
h. Recycling sorting and storage		X	
i. Parking		X	
j. Underground infrastructure and utilities, with limited related surface structures		X	
k. All of the uses permitted in the M2 Zone, as specified in LAMC Section 12.19		X	
l. Ancillary uses and structures related to the primary permitted uses		X	
<b>Additional Permitted Uses</b>			
Alcohol sale, subject to a Conditional Use Permit	X		X

#### H. Prohibited Land Uses

The following land uses shall be prohibited in the LAX Northside.

- a. Residential, or dwelling units of any kind
- b. K-12 education
- c. A retail store over 100,000 gross square feet of floor area
- d. Auto dealerships
- e. Adult businesses as defined in LAMC 12.70
- f. Massage parlors as defined in LAMC 12.70
- g. Parking as a primary use, except in Airport Support and Mixed Use-Commercial permitted use categories
- h. Hazardous materials testing
- i. Aircraft under power

#### 5.2 Development Regulations

##### 5.2A Building Heights

Buildings shall be developed in compliance with the height standards identified below and contained within Table 05.2A.1:

1. Heights shall be measured from finished Grade, as defined in Section 12.03 of the Planning and Zoning Code.

Table 05.2A.1 BUILDING HEIGHT	
Area	Maximum Height
<b>LAX Northside Center</b>	
Area 11 and 12A East	60'
Area 12A West	20'
Area 13	45'
<b>LAX Northside Campus</b>	
Area 3	60'
Areas 1, 2A, 2C, 2D, 2E	45'
<b>LAX Northside Airport Support</b>	
All Areas	30'

##### 5.2B Building Stepbacks

1. In Area 11, buildings located adjacent to the 88th Street and La Tijera property line shall be stepped back by one foot for each additional foot of height above 15'.

##### 5.2C Building Setbacks

Buildings shall be developed in compliance with the following setback standards.

1. Buildings shall be developed in compliance with the Building Setback standards as shown the Building Setbacks Map (Figure 05.2) and Table 05.2C.1
2. No building or portion of a building is permitted within the Building Setback, except architectural features as defined herein.
3. No parking is permitted within the Building Setbacks in the LAX Northside Center District.
4. No walls or fences are permitted within the Building Setback along Westchester Parkway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue or Pershing Drive.
5. Plaza spaces, outdoor eating areas, and enhanced pedestrian connections are permitted within the Building Setback.
6. Landscaped areas within Building Setbacks shall be landscaped in accordance with the Landscape Zone Map and Palettes established in Chapter 7.
7. Architectural features such as canopies, awnings, and architectural overhangs are permitted to extend beyond the face of the building into the public right-of-way, provided they do not impede any streetscape trees or other streetscape elements.

TABLE 05.2C.1 BUILDING SETBACKS	
Area	Required Setback
Area 1, 2, and 3 at Westchester Parkway	38 feet
Area 1 west boundary	38 feet
Area 1 north boundary	80 feet
Area 1 east boundary at Falmouth Avenue	30 feet
Area 2 west boundary at Falmouth Avenue	30 feet
Area 2 between OS and OR land uses	20 feet
Area 2 between OR, CC and B land uses	20 feet
Area 2 east boundary at Loyola Avenue	15 feet
Area 2 north boundary	15 feet
Area 3 west boundary at Loyola Avenue	15 feet
Area 3 east and north boundaries	20 feet
Area 13 west boundary at Lincoln Boulevard	15 feet
Area 13 north and east boundaries	20 feet
Area 12A West north and west boundaries	20 feet
Area 12A West south and east boundaries	15 feet
Area 12A East west at Emerson Avenue, north and west along 88th Place, and east boundaries	15 feet
Area 12A East south boundary	18 feet
Area 12A East north and west boundary with existing use	20 feet
Area 12A East north at 88th Street	30 feet
Area 11 east and west boundaries	15 feet



TABLE 05.2C.1 BUILDING SETBACKS	
Area	Required Setback
Area 11 north boundary	30 feet
Area 11 south boundary	50 feet
Area 4 west boundary	50 feet
Area 4 southwest boundary	20 feet
Areas 4 through 6 north boundary	50 feet
Areas 5 through 10 east and west boundaries	15 feet
Areas 7 through 10 north boundaries	15 feet
Areas 4 through 10 south boundaries at airfield	0 feet

#### 5.2D Site Access

Vehicular access location and design shall conform with the following standards and guideline. Site access requirements are also illustrated in Figure 05.3 Circulation and Access.

- |                          |  |
|--------------------------|--|
| <input type="checkbox"/> | 1. Vehicular access is prohibited from Lincoln Boulevard, Pershing Drive, and all the local streets along the north edge of the LAX Northside, including locations at Rayford and Stanmoor Drives, excluding the existing golf course on Manchester Avenue.            |
| <input type="checkbox"/> | 2. Reciprocal ingress and egress access shall be provided for all adjacent properties within the LAX Northside. This requirement may be waived by due to extreme site constraints or unforeseen conditions.  |
| <input type="checkbox"/> | 3. Minor intersections shall include a right turn only entry way into developments that do not require a signalized entrance way. Primary access drives (Major Intersections), shall include a signalized intersection that allows for both right and left turn entry. |
|                          | 4. Primary access drives along Westchester Parkway should be limited to enhance traffic flow and to reduce the disruption of the landscaping, pedestrian recreation paths, and Westchester Parkway medians.  |

#### 5.2E Parking

Required parking spaces shall conform to standards set forth in the provisions of LAMC Section 12.21.A.4 and surface parking lots shall be landscaped in accordance with the following standards.

- |                          |   |
|--------------------------|---|
| <input type="checkbox"/> | 1. A minimum of one tree for every four parking spaces shall be provided. Trees should be sized at 24-inch box or larger at the time of installation and remaining landscaped area shall contain understory planting.       |
| <input type="checkbox"/> | 2. Landscape islands and landscape fingers containing trees shall be a minimum of six feet in width.  |
| <input type="checkbox"/> | 3. Any portion of the parking area not used for parking, loading drive aisles, or pedestrian connectivity shall be landscaped.  |
| <input type="checkbox"/> | 4. Parking stalls shall be paved with permeable pavers or porous paving materials. Drive aisles and primary and secondary entrance roadways are excluded from this requirement.   |
| <input type="checkbox"/> | 5. Parking areas shall be designed to mitigate stormwater in compliance with the City of Los Angeles' Low Impact Development Ordinance, as amended.   |
| <input type="checkbox"/> | 6. Landscaping within parking areas shall be protected from encroaching vehicles by concrete curbing or raised planting areas. Curb cuts shall be provided to allow stormwater drainage into landscape islands and fingers. |

#### 5.2F Building Location

Buildings or structures shall be developed in compliance with the following standards.

- |                          |   |
|--------------------------|---|
| <input type="checkbox"/> | 1. Buildings within Areas 2, 11, and 12A shall front Westchester Parkway, La Tijera, and Sepulveda Westway setbacks.  |
| <input type="checkbox"/> | 2. Buildings within Areas 2C and 2E shall be located with a minimum of 65 percent of the proposed project ground floor area located within 250 feet of the Westchester Parkway property line. |
| <input type="checkbox"/> | 3. Ancillary buildings shall not front Westchester Parkway, Sepulveda Westway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue or Pershing Drive.                                      |
| <input type="checkbox"/> | 4. Parking structures shall not front Westchester Parkway.  |

#### 5.2G Pedestrian and Bicycle Orientation

All Areas fronting Westchester Parkway, La Tijera, and Sepulveda Westway are designated as "pedestrian oriented." The pedestrian circulation system shall connect buildings, streets, parking areas, and public transit stops to create an environment that supports public transportation, carpools, biking, and other forms of transportation. The following development standards, in addition to the development standards set forth above, shall apply to all primary buildings. These standards shall not apply to ancillary buildings.

- |                          |   |
|--------------------------|---|
| <input type="checkbox"/> | 1. A direct pedestrian connection designated by distinct landscaping and paving materials shall be provided between parking areas and the buildings they serve.   |
| <input type="checkbox"/> | 2. Retail or restaurant uses shall provide bicycle parking within a minimum of one hundred (100) feet of each primary building entrance.  |
| <input type="checkbox"/> | 3. Office or research buildings shall provide bicycle parking within a minimum of two hundred (200) feet of an employee entrance.   |
| <input type="checkbox"/> | 4. A clearly-marked pedestrian connection designated by distinct landscaping and paving materials shall be provided between the primary building entrance of buildings and the paseo. The maximum distance between such pedestrian connections and the paseo shall be no more than one hundred (100) feet within the LAX Northside Center District and three hundred (300) feet within the LAX Northside Campus District. |
| <input type="checkbox"/> | 5. Bicycle parking shall be provided consistent with Section 12.21.A.16 of the Los Angeles Municipal Code, as amended.  |

#### 5.2H Landscape Buffers

Landscape Buffers have been identified as Area 2B and the 20-foot Landscape Buffer on the northern boundary of Area 1. Landscape Buffers shall be developed in compliance with the following standards.

- |                          |  |
|--------------------------|--|
| <input type="checkbox"/> | 1. Pedestrian access is prohibited, except for maintenance.  |
| <input type="checkbox"/> | 2. Landscaped Buffers shall be landscaped in accordance with the Landscape Zones and Required Palettes established in Chapter 7.   |
| <input type="checkbox"/> | 3. Plantings shall be dispersed evenly throughout Landscape Buffers and shall not be limited to the perimeter.   |
| <input type="checkbox"/> | 4. A 10-foot high fence shall secure the perimeter of a Landscape Buffer identified in the Land Use Map, Figure 05.1. Fence color shall complement proposed landscaping. Examples of appropriate fencing are presented at the end of this Section. |
| <input type="checkbox"/> | 5. Trees planted within the Area 1 Buffer shall be planted to minimize obstruction of views from adjacent residences.  |

## 5.2I Utilities and Service Areas

Utilitarian elements and loading/service areas shall conform to the following standards, with the exception of the LAX Northside Airport Support District which is excluded from these requirements.

<input type="checkbox"/>	1. All utility service equipment, including but not limited to meters, vaults, sprinkler risers, vacuum breakers, and all service and trash areas shall be screened from neighboring properties and public right-of-way and shall be located away from major pedestrian routes and outdoor seating areas. These areas shall be screened by landscape materials including trees, shrubs, and ground cover and/or fences or walls designed to conform to the standards outlined within this document.
<input type="checkbox"/>	2. No materials, supplies or equipment, including trucks or other motor vehicles (excluding company vehicles for passenger use) shall be stored on-site unless located inside a closed building or structure or screened from public view.
<input type="checkbox"/>	3. Service areas shall be designed to minimize automobile/pedestrian conflicts.
<input type="checkbox"/>	4. Roof mounted equipment shall be screened at a minimum equal to the height of the equipment, using similar materials and colors as the primary building.
<input type="checkbox"/>	5. Walls designed to screen utilitarian equipment shall be a maximum of six (6) feet in height, measured from finish grade.
<input type="checkbox"/>	6. Loading areas shall be accommodated entirely on-site.
<input type="checkbox"/>	7. Loading docks and doors for areas dedicated to loading shall not be visible from a public street.
<input type="checkbox"/>	8. Ancillary buildings shall be built with permanent materials that relate in style and finish to the primary buildings with which they are associated.
<input type="checkbox"/>	9. Trash and recycling storage areas shall be located to the rear or sides of a building and shall be screened from public view with walls, berms, or landscaping.
<input type="checkbox"/>	10. Trash enclosures and loading areas shall be designed using similar materials and colors as the primary buildings with which they are associated.
<input type="checkbox"/>	11. Recycling bins shall be screened.
<input type="checkbox"/>	12. Functional building elements, such as roof scuppers and vents shall not be visible from a public street.
<input type="checkbox"/>	13. Sheet metal vents, pipe stacks, and flashing shall be similar in finish and color to the adjacent roof or wall material.

## 5.2J Walls and Fences

Fences and walls shall conform to the following standards.

<input type="checkbox"/>	1. Walls and fences are discouraged along interior lot lines, except where Landscape Buffers or demonstrated security needs are required.
<input type="checkbox"/>	2. Recreation Areas shall be secured with an eight (8) foot tall fence and provide limited and controlled access to the general public.
<input type="checkbox"/>	3. Fences and walls not associated to Recreation or Buffer areas shall have a maximum height of eight (8) feet measured from the finished grade. A six (6) foot wide planting strip shall be located adjacent to walls and fences and shall include shrubs, vines and ground cover identified in Chapter 7.
<input type="checkbox"/>	4. Solid fences or walls shall be designed with both sides articulated with similar or complementary materials and colors as the primary building with which they are associated.
<input type="checkbox"/>	5. Chain link fencing (with or without slats), corrugated metal, and barbed/razor wire is prohibited within the Northside Center and Campus Districts.
<input type="checkbox"/>	6. Long expanses of walls (50 feet or greater) shall be broken up with projections or recessed elements, landscape pockets, or changes in materials.
<input type="checkbox"/>	7. Where a wall or fence is located adjacent to a public right-of-way, a minimum six (6) feet landscaped setback shall be provided.

## 5.2K Site Lighting

The following lighting standards apply:

<input type="checkbox"/>	1. Glare or light trespass is prohibited on any adjacent streets, or within any adjacent properties.
<input type="checkbox"/>	2. Lighting mounted above ten (10) feet from finished grade shall incorporate a full cut-off shield fixture.
<input type="checkbox"/>	3. Lighting shall be provided in parking areas, near access drives, pedestrian pathways or crosswalks, and internal vehicular circulation areas.
<input type="checkbox"/>	4. The parking lot illumination level shall achieve a uniformity ratio of 3 to 1 (average to minimum) with a maintained average of 1 foot candle and minimum of .3 foot candle.
<input type="checkbox"/>	5. Service area lighting shall be contained within the service yard boundaries and enclosure walls. No light spillover shall occur outside the service area.
<input type="checkbox"/>	6. Pedestrian area lighting, including outdoor plazas, entry ways or other common areas shall achieve a uniformity ratio of 3 to 1 average to minimum, with an average illumination of .60 foot candles and minimum of .18 foot candles
<input type="checkbox"/>	7. Pedestrian walking areas, such as the paseo or parking lot walkways, shall require point to point lighting at a minimum of twenty (20) feet between each point with no specific illumination levels required. Lighting shall clearly identify the pedestrian walking zone and direction of travel.





### ***Landscape Buffer Fencing***

LAX Northside Center District

Area 1 and Area 2b

These images depict the appropriate details and functional requirement specified for the fences that secure Buffer areas.



### 5.3 Land Use Equivalency Program Guidelines

The LAX Northside would be developed by a variety of individual applicants over a period of years, and may not eventually have the exact amount of development of each land use described in the LAX Northside Plan Update Environmental Impact Report. In order to maintain flexibility, it is necessary to provide a mechanism to reduce development of one type of land use in favor of increasing development of another.

Because the LAX Northside's various land uses generate trips at different rates, substitutions of square footage<sup>1</sup> between land uses would change the number of trips expected to be generated. The LAX Northside Plan Update Environmental Impact Report analyzed the maximum development "envelope"- measured by the number of peak hour trips it generates- to ensure that all potentially significant traffic impacts were identified. The land use equivalency program defines factors by which developed square footage can be adjusted between Areas within Districts. By applying these factors, these adjustments can be made such that no additional significant traffic impacts would result.

#### Land Use Equivalency

The land use equivalency factors are calculated by dividing the trip generation rate of a particular land use by the respective rates of each other land use. Table 5.3.1 summarizes the factors for each of the six proposed trip-generating land uses based on the trip generation estimates from Table 11 in the LAX Northside Plan Update Draft EIR Traffic Study. Table 5.3.1 summarizes factors based on daily, morning peak hour, and afternoon peak hour trip generation. However, in practice, the afternoon peak hour factors are used to simplify the land use equivalency calculation. Table 5.3.2 summarizes the afternoon peak hour equivalency factors on their own.

#### Application of Factors

The land use equivalency factors allow conversion of square footage from one land use to another. The row headers on the left side of Table 5.3.2 show the initial land use and the column headers at the top show the new land use. Equivalency factors are chosen by matching the row of the initial land use with the column of the new land use. If a particular land use is being reduced by a known amount, the equivalency factor should be multiplied by the amount of that reduction to calculate the allowable increase of the new land use. If the goal is to increase a particular land use by a known amount, then the amount of that new land use should be divided by the equivalency factor to determine how much of the original land use must be reduced. For example, if reducing the amount of research and development space by 5,000 square feet (sf) in favor of developing additional office space, the appropriate factor would be 0.71 (transferring from R&D to office). A reduction of 5,000 sf of R&D would translate to an increase of 3,550 sf of office in its place ( $5,000 \text{ sf} \times 0.71 = 3,550 \text{ sf}$ ). If reducing the amount of community/civic space by 10,000 sf in favor of adding additional airport support uses, the ratio is

2.46. A reduction of 10,000 sf of community/civic space would translate to additional LAX facilities housing 25 employees ( $10,000 \text{ sf} \times 2.46 = 24.6$  employees, rounded up to 25).

If desiring to build two extra open space and recreation playing fields by reducing retail space, the ratio is 0.13, which should be divided by the number of fields. The increase of two fields would require the reduction of 15,385 sf of retail space ( $2 \text{ fields} / 0.13 = 15.385 \text{ ksf} = 15,385 \text{ sf}$ ). If desiring to increase the amount of community/civic space by 5,000 sf by reducing R&D, the ratio is 0.65 and a total of 7,692 sf of R&D space must be reduced ( $5,000 \text{ sf} / 0.65 = 7,692 \text{ sf}$ ).

Therefore, the two equations are:

1.  $[\text{Amount of Reduced Land Use}] \times [\text{Equivalency Factor}] = [\text{Amount of New Land Use}]$
2.  $[\text{Amount of New Land Use}] / [\text{Equivalency Factor}] = [\text{Amount of Reduced Land Use}]$

<sup>1</sup>Two of the six land uses generate trips based on other metrics than square footage. Open Space and Recreation are based on the number of fields, and Airport Support are based on the number of employees anticipated. For practical reasons, the term "square footage" is used when describing land use transfers in this document, but transfers can be made between land uses using whichever metric is unique to that land use (e.g., some square footage of office space could be transferred for some number of employees at LAX facilities).



Table 05.3.1 Land Use Equivalency Matrix LAX Northside Trip Generation Equivalency																			
From this land use ↓	To this land use →	Open Space and Recreation (fields)			Community or Civic (ksf)			Office (ksf)			Research and Development (ksf)			Airport Support (employees)			Mixed Use-Commercial (ksf)		
		Daily	AM	PM	Daily	AM	PM	Daily	AM	PM	Daily	AM	PM	Daily	AM	PM	Daily	AM	PM
Open Space and Recreation		1.00	1.00	1.00	3.28	0.91	15.01	7.80	1.04	16.59	10.32	1.43	23.23	35.67	10.94	36.91	2.37	2.00	7.92
Community or Civic		0.30	1.10	0.07	1.00	1.00	1.00	2.38	1.15	1.11	3.14	1.57	1.55	10.87	12.02	2.46	0.72	2.20	0.53
Office		0.13	0.96	0.06	0.42	0.87	0.90	1.00	1.00	1.00	1.32	1.37	1.40	4.57	10.49	2.23	0.30	1.92	0.48
Research and Development		0.10	0.70	0.04	0.32	0.64	0.65	0.76	0.73	0.71	1.00	1.00	1.00	3.46	7.65	1.59	0.23	1.40	0.34
Airport Support		0.03	0.09	0.03	0.03	0.08	0.41	0.22	0.10	0.45	0.29	0.13	0.63	1.00	1.00	1.00	0.07	0.18	0.21
Mixed Use-Commercial		0.42	0.50	0.13	1.38	0.45	1.90	3.29	0.52	2.10	4.35	0.71	2.93	15.03	5.47	4.66	1.00	1.00	1.00
Note: Community/Civic, Office, and R&D uses include 5% transit credit. Mixed Use-Commercial includes 30% pass-by credit.																			

Table 05.3.2 Land Use Equivalency Matrix LAX Northside Trip Generation Equivalency- PM Peak Hour Only							
From this land use ↓	To this land use →	Open Space and Recreation (fields)	Community or Civic (ksf)	Office (ksf)	Research and Development (ksf)	Airport Support (employees)	Mixed Use- Commercial (ksf)
Open Space and Recreation		1.00	15.01	16.59	23.23	36.91	7.92
Community or Civic		0.07	1.00	1.11	1.55	2.46	0.53
Office		0.06	0.90	1.00	1.40	2.23	0.48
Research and Development		0.04	0.65	0.71	1.00	1.59	0.34
Airport Support		0.03	0.41	0.45	0.63	1.00	0.21
Mixed Use-Commercial		0.13	1.90	2.10	2.93	4.66	1.00
Note: Community/Civic, Office, and R&D uses include 5% transit credit. Mixed Use-Commercial includes 30% pass-by credit.							

# PART II

## 6 ARCHITECTURAL DESIGN

"Architecture" as described in this portion of the guidelines and standards refers to all buildings that provide character, identity, and form to the built environment in the LAX Northside. The purpose of the architectural design guidelines and standards is to establish a framework for the design and articulation of buildings within the LAX Northside, defining the standards for how the built environment will look and the character it will capture. These guidelines and standards are intended to achieve compatibility with adjacent communities and uses while maintaining the flexibility needed to respond to individual identities and the latest best-practices in building design and sustainability.

### ARCHITECTURE DESIGN GUIDELINES AND STANDARDS CHECKLIST

#### 6 Architecture Design Guidelines and Standards

The guidelines and standards provided within this chapter articulate the design expectations for the LAX Northside Campus District and LAX Northside Center District and are intended to be used in tandem with the Urban Design Guidelines located in Chapter 5 along with the vision and direction provided within the rest of this document. The guidelines and standards address the minimum requirement for creating quality development.

##### 6.1 Building Form

One of the key aspects to manage development within the LAX Northside is the reduction of impacts on adjacent uses, such as the working airfield and surrounding residential and commercial communities.



1. Building facades within 150 feet of neighboring residences shall be located to maximize privacy associated with abutting homes and shall incorporate two or more of the following strategies:

Buildings shall be oriented to limit direct views into neighborhood homes or sensitive use spaces, such as the golf course, day care or existing condominium facilities.

Off-set windows on walls adjacent to a neighboring residences to prevent direct views into neighboring windows.

Utilize clerestory windows, translucent glass, and/or vision glass beginning in elevation for the second story or higher at a minimum of four (4) feet from finish floor to prevent direct sight lines into neighbors' windows and livable outdoor spaces.

Use landscaping to provide a buffer or screening between properties.



2. No building facade shall extend more than eighty (80) feet in length without variations in the wall surface through setbacks or changes in the wall plane. Variations at a minimum must be four (4) foot offset horizontally.



3. Two or more of the following design strategies shall be used to reduce the perceived height, bulk, and massing of the building:

Variation in the vertical wall in locations in excess of item 2 above.

Variation in parapet or roof by more than two (2) feet for every forty (40) feet.

Variation of roof types, or alternating roofs and parapets.

Variation of facade material, so that no material is more than 35% of the total facade area, including glazing.



<input type="checkbox"/>	4. Vertical circulation elements (stairs and elevators) shall be designed as an integral part of the overall architecture of the building and shall complement its massing and form.
<input type="checkbox"/>	5. Minor surface detailing shall not be used as a substitute for distinctive building massing. Minor surface detailing includes score lines or changes in color, rather than a change or relief in the wall plane.
<input type="checkbox"/>	6. The ground floor shall be differentiated from upper floors through changes in massing, architectural relieve, or other strategies.

#### 6.2 Facade Articulation and Materials

Architectural details should be used to enhance buildings by adding color, shadows, and interesting forms. They should not, however, be used as a substitute for genuine building massing. This is particularly important on frontages facing Westchester Parkway, La Tijera Boulevard, and internal to the project area where buildings are oriented toward parking and primary vehicular access areas within the retail and office environments.

<input type="checkbox"/>	1. Building massing shall be broken down into smaller units, with vertical and horizontal queues to promote pedestrian scale.
<input type="checkbox"/>	2. Mirror or reflective surfaces shall not be primary building materials.
<input type="checkbox"/>	3. Architectural details should be consistent with the proportions and scale of the building(s).
<input type="checkbox"/>	4. All building facades should be treated with an equal level of detail and articulation.

#### 6.3 Roofs

Roof design and mechanical equipment screening are important design features. Integrating full roof forms and elements, green roofs, and the application of a painted finish can provide opportunities to improve the visual quality of roofs.

<input type="checkbox"/>	1. Roof parapets shall be articulated with details including, but not limited to precast treatments, continuous banding, or projecting cornices, lintels, caps, corner details, or variety in pitch (for example, articulated, sculptural roof lines/forms).
<input type="checkbox"/>	2. Roof parapets shall not appear "tacked on" and shall convey a sense of permanence. Where tower or vertical elements are proposed, parapets shall wrap to create the appearance of a complete structure.
<input type="checkbox"/>	3. All roof mounted mechanical equipment shall be screened to the height of the equipment. Line of sight screening is not acceptable. <ul style="list-style-type: none"> <li>o Buildings with flat or low-pitched roofs shall incorporate parapets, pitched facades, or architectural elements designed to screen roof mounted mechanical equipment.</li> <li>o Screening shall be architecturally compatible in color, shape, size, and material with the primary building and shall be carefully integrated into the overall building design.</li> </ul>
<input type="checkbox"/>	4. Roof access shall be provided from the interior of the building. Exterior roof access ladders are prohibited.
<input type="checkbox"/>	5. Roof surfaces shall be light in color.
<input type="checkbox"/>	6. Green roofs are encouraged.

#### 6.4 Parking Structures

<input type="checkbox"/>	1. Parking structures shall be designed and sited to reduce visual impact from public view and neighboring residential development.
<input type="checkbox"/>	2. The following strategies shall be considered when siting and designing a parking structure: <ul style="list-style-type: none"> <li>o Minimize visual and lighting impacts on neighboring properties.</li> <li>o Vehicle ramps within the interior of the structure to limit headlight exposure.</li> <li>o Utilize exterior screen systems for planting and vegetation, or additional architectural articulation to improve aesthetic quality.</li> <li>o Provide additional accent or façade articulation at vehicular and pedestrian entries to the garage.</li> </ul>
<input type="checkbox"/>	3. All lighting within and on the roof of the parking structure shall be shielded so that the light sources are not visible from adjacent property or rights-of-way.
<input type="checkbox"/>	4. A ten foot minimum landscape strip shall be provided at the base of the structure where pedestrian or vehicle access is not provided.
<input type="checkbox"/>	5. Naturally ventilated parking is encouraged in order to minimize mechanical ventilation.
<input type="checkbox"/>	6. Elevators and stairs of parking structures should be highlighted architecturally, so visitors can easily find and access these entry points.
<input type="checkbox"/>	7. Signage and wayfinding should be provided within the parking structure architecture.

#### 6.5 Pedestrian Amenities and Infrastructure

<input type="checkbox"/>	1. Pedestrian amenities shall be selected to complement the overall character of the development and adhere to the following objectives: <ul style="list-style-type: none"> <li>o Furnishings shall be attractive, functional, durable and easy to maintain.</li> <li>o Amenities shall promote safe, visually pleasing, and comfortable pedestrian environments.</li> </ul>
<input type="checkbox"/>	2. Trash receptacles, benches, bollards, planters and bike racks shall be located in areas with high pedestrian activity such as pedestrian walkways, project entry plazas and building entrances, seating areas, and transit stops.
<input type="checkbox"/>	3. Incorporate features such as white markings, signage, and lighting at crosswalks so that pedestrian crossings are visible to moving vehicles during the day and at night.
<input type="checkbox"/>	4. Pave sidewalks with pavers, concrete, or other safe, non-slip materials to create a distinctive pedestrian environment and, for crosswalks, to visually and physically differentiate these from vehicle travel lanes and promote continuity between pedestrian sidewalks.

#### 6.6 Building Lighting

Building lighting shall be designed and placed to limit impacts on adjacent properties or disrupt the function of the airfield. The quality of light, level of light as measured in foot-candles, and the type of bulb or source shall be carefully addressed. Lighting levels shall not be so intense as to draw attention to the flow or glare of the project site. Lighting shall incorporate current energy-efficient fixtures and technology.

<input type="checkbox"/>	1. Lighting shall be designed to provide ambience, safety, and security without unnecessary spillover or glare onto adjacent properties. Indirect wall lighting or "wall washing" and overhead down lighting may be used to help reduce light trespass into adjacent properties.
<input type="checkbox"/>	2. Spot lighting or glare from any site lighting shall be shielded from adjacent properties and directed at a specific object or target area.
<input type="checkbox"/>	3. Building light fixtures shall be designed or selected to be architecturally compatible with the main structure.
<input type="checkbox"/>	4. When security lighting is necessary, it shall be recessed, hooded, and located to illuminate only the intended area. Off-site glare and light trespass shall be prevented.
<input type="checkbox"/>	5. Exposed bulbs are prohibited.

#### 6.7 Stormwater Management

All areas shall integrate Low Impact Design (LID) best practices into projects to promote and facilitate water conservation.

<input type="checkbox"/>	1. Site development shall comply with all applicable Regional Water Quality Control Board and County of Los Angeles regulations for water quality and quantity including preparation of a Standard Urban Stormwater Mitigation Plan (SUSMP) with Operation and Maintenance Guidelines.
<input type="checkbox"/>	2. Natural vegetation and native and/or drought tolerant plants shall be planted in parking lot islands and other landscaped areas where feasible.
<input type="checkbox"/>	3. Natural drainage systems shall be utilized to the maximum extent feasible.
<input type="checkbox"/>	4. Impervious area shall be minimized.
<input type="checkbox"/>	5. Non-structural Best Management Practices shall be used unless they are infeasible in which case the infeasibility shall be documented and structural Best Management Practices are implemented.
<input type="checkbox"/>	6. Stormwater shall be pre-treated prior to infiltration or discharge from site.



Bike Racks



Bollards



Trash Receptacles





Planters



Benches



Benches

## Appropriate Pedestrian Amenities

LAX Northside Center and LAX Northside Campus Districts

The following images provide examples of sturdy, durable, and attractively designed pedestrian amenities. These images do not specify the specific items, but capture the spirit of the project in their articulation and represent input from community workshops.

# PART II

## 7 LANDSCAPE DESIGN

*“Landscape” as described in this portion of the design guidelines and standards refers to all plant materials that provide character, identity and form to the natural environment in the LAX Northside. These items include streets and landscape setbacks, the variety and placement of selected plant materials, walkways and the paseo, signage, lighting, site furnishings, and arrangement of major functional elements including development entries, parking lots, buildings, service areas, and other locations throughout the LAX Northside. The objective of the Landscape Design guidelines and standards is to ensure that landscaping as a design element will help create, convey, and reinforce the overall character of the LAX Northside, even while project architecture and the design of building sites may vary in type, size, style and location.*

The landscape design guidelines support the overall development concepts of the three Districts: the LAX Northside Center District, the LAX Northside Campus District, and the LAX Northside Airport Support District. For airport operational reasons, it is critical to prevent future interactions between wildlife and the working airfield. An overall landscape concept has been established to achieve this goal. This concept presents a hybrid landscape that, much like a gradient, provides non-native planting strategies along Westchester Parkway, a mix of non-native and native plantings in the development zones and parking areas, and a full native planting palette for all areas that exist along the northern property lines, adjacent to the residential communities.

The landscape guidelines and standards have been organized around seven areas that exist within the three Districts of the LAX Northside. These areas have been selected to help focus specific plants from the overall planting palette into appropriate locations. The seven areas are 1) Landscape Setbacks, 2) Paseo and Streetscapes, 3) Airport Support, 4) Surface Parking, 5) Recreation, 6) Parking and Development, and 7) Urban Tree Line. The following provide the conceptual direction for these areas:

### *Landscape Setback Zone*



Landscape setbacks are used primarily to screen development from neighbors and differentiate boundaries along property lines. These areas, depending on their location within the LAX Northside, will consist of drought tolerant, low maintenance and foot traffic durable materials that provide options for trees, shrubs and groundcover. The palette will combine fifty (50) percent non-native and fifty (50) percent native plant materials.

### *Paseo and Streetscapes Zones*



The palette will primarily be evergreen and non-native, allowing a consistent visual appeal year round, in addition to being drought tolerant and non-invasive. The palette will combine seventy (70) percent non-native and thirty (30) percent native plant materials.

### *Airport Support Zone*



Plantings will be limited within the Airport Support zone due to its proximity to the adjacent airfield. Most plant material will be groundcover and shrubs, and limited trees. This zone will combine eighty (80) percent native and twenty (20) percent non-native plant materials.



#### Landscape Buffer Zone

The buffers will consist of one hundred (100) percent locally-native, drought tolerant plant materials intended to reduce undesirable impacts while requiring limited to no maintenance.

#### Recreation Zone

Primarily, all recreation areas will be designated in Area 1 and the western portion of Area 2. These areas are intended to be open space areas that require specific and particular groundcover for active playing fields, and intensive uses, such as dog parks and running paths. The planting palette for this area type is drought tolerant, non-invasive and will require frequent maintenance due to its use. The palette will favor locally native species and will combine eighty (80) percent native and twenty (20) percent non-native plant materials.

#### Parking and Development Zone

One of the largest landscaped areas within the LAX Northside area will be the surface parking areas required for each development. The planting palette for these areas will consist of a hybrid mix of sixty (60) percent native plants and forty (40) percent non-native, and it is recommended that the trees, shrubs and groundcover options be compatible with storm water management systems, such as bioswales or permeable paving systems.

#### Urban Tree Line

One of the most distinguished design features presented for the LAX Northside is the introduction of a continuous line of trees running along Westchester Parkway. This line of trees will run the entire length of the LAX Northside and will provide an edge through which development frontages engage and interact. This row of trees is intended to be planted with a single tree species, the Aleppo Pine (*Pinus halpensis*) that is an evergreen species known for its low maintenance, capability for slender but tall growth in a conical form, and vibrant light green needles. This defining line will help create an identity for the LAX Northside, while buffering visual and audible impacts from future developments on adjacent neighbors.

Tables 07.2-1 through 07.2-7 detail the permitted plants for each planting zones, and estimated growth is provided for trees. All species allowed in the Landscape Buffer Zone are native, while the other zones allow a mix of native and non-native species.

## LANDSCAPE DESIGN GUIDELINES AND STANDARDS CHECKLIST

### 7 Landscape Design Guidelines and Standards

#### 7.1 Landscape Design

- ☐ 1. Landscaped areas shall be planted in accordance with the Landscape Zones established in this chapter.
- ☐ 2. Plant materials are restricted to those specified in the following plant palettes and shall be located within the zones identified on the Landscape Zone map, Figure 7.1.
- ☐ 3. Landscapes are required to achieve the following percentage breakdown in their overall composition. These percentages are required on a project by project basis.

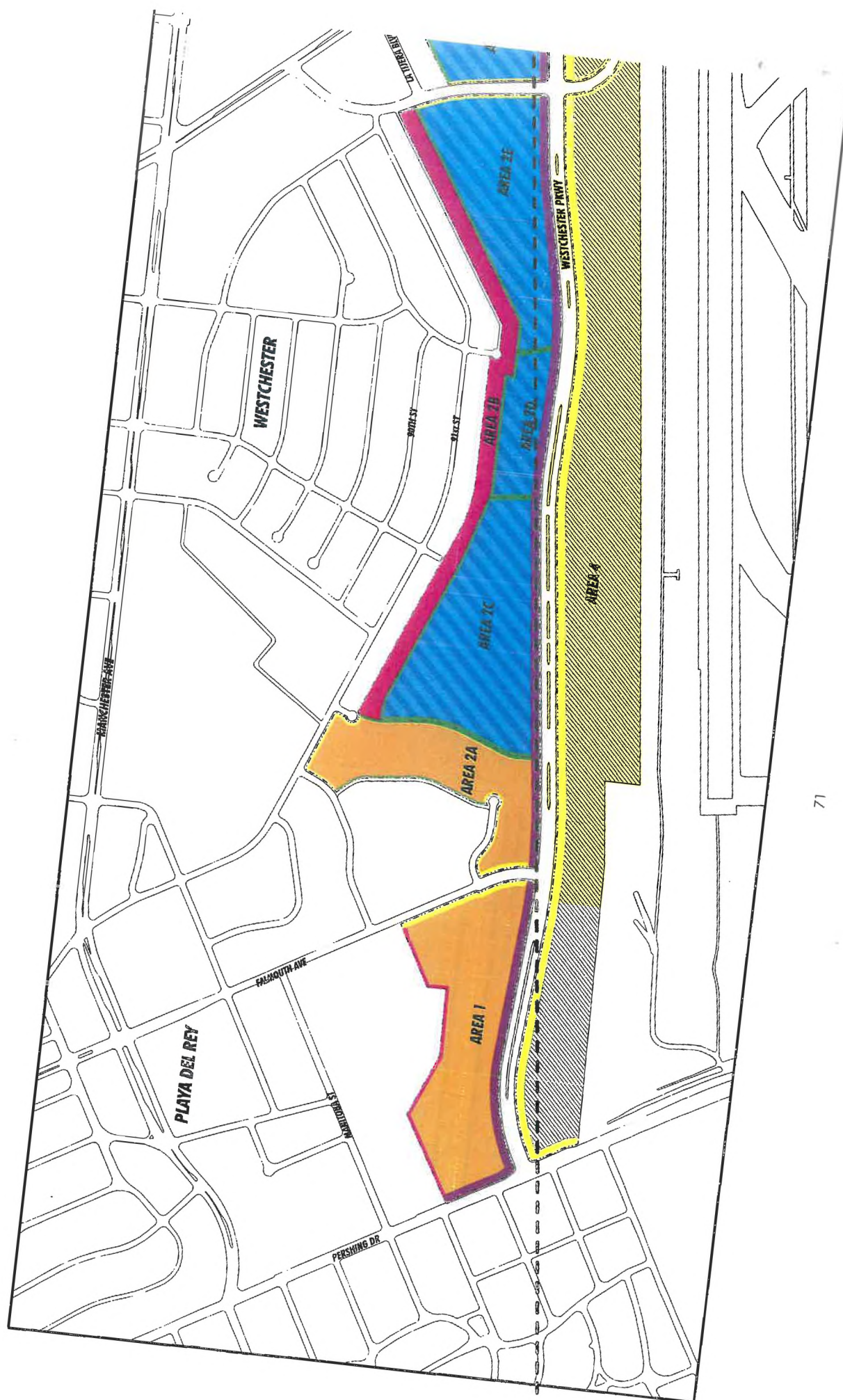
Planting Zone	Native (%)	Non-Native (%)
A. Paseo / Streetscapes	30	70
B. Landscape Buffers	100	
C. Landscape Setbacks	50	50
D. Parking and Developments	60	40
E. Recreation	80	20
F. Airport Support	80	20

- ☐ 4. Trees and large shrubs shall be planted at a spacing of two times the full growth radius. For example, a tree that grows to 30' canopy shall be planted 60' on center.
- ☐ 5. Casting of seeds for lawns, such as with hydro-seeding, is prohibited.
- ☐ 6. Mulch should be used underneath all planted materials to promote weed control and water conservation.
- ☐ 7. Planted areas should be equipped with automatic irrigation systems and conform to the City of Los Angeles' conservation requirements.

#### 7.2 Site Maintenance

These maintenance guidelines shall apply to individual parcel developers and are applicable to all zones and districts within the LAX Northside.

- ☐ 1. Areas not used for structures, walkways, paved driveways, or storage areas shall maintain a well-kept landscaped condition and according to the maintenance specifications to be provided by the lessee.
- ☐ 2. All trees and large shrubs shall be regularly maintained in order to have a thin and open canopy.
- ☐ 3. All trees replaced within the medians and right-of-ways shall adhere to the species specified in this document and as stated below:
  - a. Any tree replaced in the medians of Westchester Parkway shall be replaced with a *Arbutus 'Marina'*, also known as the Marina Strawberry Tree.
  - b. Any tree replaced along the right-of-way on Westchester Parkway or La Tijera Boulevard shall be replaced with a *Metrosideros excelsa*, also known as the New Zealand Christmas Tree.







**FIGURE 07.1**

**PLANTING ZONES**

This Figure provides information specific to the planting zones in the LAX Northside.



- LANDSCAPE SETBACK ZONES
- PASEO ZONES
- STREETSCAPE ZONES
- RECREATION ZONES
- LANDSCAPE BUFFER ZONES
- STORM WATER MITIGATION (BIOSWALES)
- PARKING and DEVELOPMENT ZONES
- AIRPORT ZONE
- URBAN TREE LINE

TABLE 07.2-1 // TREES

Estimated growth &gt; 30' H

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
	Casuarina cunninghamiana	River She-Oak	70'X30'	X						X			
X	Cupressus arizonica	Arizona Cypress	40'X20'	X					X	X			
X	Cupressus guadalupensis	Guadalupe Cypress	50'X15'	X					X	X			
X	Lyonothamnus floribundus	Catalina Ironwood	50'X35'	X	X	X			X	X	X		
	Melaleuca quinquenervia	Punk Tree	40'X25'	X	X	X				X	X		
	Pinus halpensis	Aleppo pine	60'x40'										X
	Pinus pinea	Italian Stone Pine	75'X50'	X	X	X				X			
X	Pinus torreyana	Torrey Pine	70'X40'	X	X	X				X			

TABLE 07.2-2// SMALL TREES &amp; LARGE SHRUBS

Estimated growth &lt; 30' H

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
	Agonis flexuosa	Pepermint tree	30'x30'	X	X	X				X	X		
	Arbutus 'Marina'	Marina Strawberry Tree	25'x40'	X	X	X				X	X		
	Arbutus unedo	Strawberry Tree	30'x30'	X	X	X				X	X		
X	Atriplex lentiformis lentiformis	Saltbush	15'x10'				X	X		X	X	X	
	Banksia ericifolia	Heath Banksia		X						X			
	Banksia integrifolia	Coast Banksia	30'x30'	X	X	X				X	X		
	Banksia praemorsa	Cut-leaf Banksia		X	X					X			
	Butia Capitata	Pindo Palm or Jelly Palm	20'X12'										
	Cassia leptophylla	Gold Medallion Tree	25'X20'	X	X	X				X	X		
X	Ceanothus arboreus	Feltleaf or Catalina Ceanothus	15-25'x12'	X	X	X	X	X		X	X		
X	Cercis occidentalis	Western Redbud	15'x10'	X	X					X	X	X	
	Chitalpa tashkentensis	Chitalpa	30'x30'	X	X	X				X	X		
	Cordyline australis	Grass Palm	VARx30'	X	X	X				X		X	
	Dodonaea viscosa and cultivars	Hopseed Bush	18'x12'	X						X	X		
	Dracaena Draco	Dragon Tree	VARx25'	X	X	X				X	X		
	Eleagnus pungens	Silverberry		X	X	X				X	X		
X	Garrya elliptica	Coast silk-tassel		X	X	X	X			X	X		
	Hakea suaveolens	Sweet Hakea		X	X	X				X	X		
X	Heteromeles arbutifolia	Toyon	15'x15'	X	X	X			X	X	X		



TABLE 07.2-2 // SMALL TREES &amp; LARGE SHRUBS

Estimated growth &lt; 30' H

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
				1	2	3	4	5	6	7	8	9	10
	Laurus nobilis	Sweet bay	30'x20'	X	X	X	X			X			
	Leptospermum laevigatum	Australian Tea Tree	25'x25'	X	X	X				X			
	Leucospermum cordifolium	Nodding Pincushion		X	X	X				X	X		
	Lysiloma watsonii	Feather Bush	25'x25'	X	X	X				X	X		
	Maytenus phyllanthoides	Mangle Dulce, Sweet Mangrove		X	X	X				X	X		
	Melaleuca armillaris		15'x30'	X						X			
	Melaleuca elliptica	Granite Honey Myrtle	18'x15'	X						X			
	Melaleuca ericifolia	Swamp Paperbark	18'x24'	X						X			
	Melaleuca linariifolia	Flax Leaf Paperbark	30'x25'	X	X	X				X	X		
	Melaleuca hypericifolia	Hillock Bush		X	X	X				X	X		
	Melaleuca nesophila	Pink Melaleuca	18'x20'	X	X	X				X	X		
	Metrosideros excelsa	New Zealand Christmas Tree	30'x30'	X	X	X				X	X		
	Metrosideros collina 'springfire'	Lehua	18'x18'	X	X	X				X	X		
X	Myrica californica	Pacific Wax Myrtle	15-30' H	X						X			X
	Myrsine africana	African Boxwood		X	X	X	X			X	X		
	Parkinsonia x 'Desert Museum' (Cercidium x)	Desert Museum Palo Verde	25'x25'	X	X	X				X	X		
	Protea spp	Protea		X	X	X				X	X		
X	Salix exigua	Narrow-leaf Willow											X
X	Salix lasiolepis	Arroyo Willow											X
X	Simmondsia chinensis	Jojoba		X	X	X				X	X		

TABLE 07.2-3 // SHRUB &amp; PERENNIALS

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
				1	2	3	4	5	6	7	8	9	10
X	Acalypha californica	California copperleaf		X						X	X	X	
X	Arctostaphylos catalinae	Catalina Manzanita		X	X	X				X	X	X	
X	Arctostaphylos edmundsii	Little Sur Manzanita		X	X	X				X	X	X	
X	Arctostaphylos glandulosa	Eastwood Manzanita		X	X	X				X	X	X	
X	Arctostaphylos glauca 'Los Angeles'	Los Angeles Big Berry Manzanita		X	X	X				X	X	X	
X	Arctostaphylos hookeri	Hooker's Manzanita		X	X	X				X	X	X	
X	Arctostaphylos hybrids, cultivars	Manzanita		X	X	X				X	X	X	
X	Arctostaphylos insularis	Island Manzanita		X	X	X				X	X	X	

TABLE 07.2-3 // SHRUB &amp; PERENNIALS

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
X	Arctostaphylos morroensis	Park View' Manzanita, Morro Manzan		X	X	X		X	X	X			
X	Arctostaphylos nummularia sensitiva	Glossyleaf Manzanita		X	X	X		X	X	X			
X	Arctostaphylos osoensis (A. cruzensis)	SLO Valley Manzanita		X	X	X		X	X	X			
X	Arctostaphylos pacifica x	San Bruno Carpet		X	X	X		X	X	X			
X	Arctostaphylos pajaroensis Brother James	Brother James Manzanita.		X	X	X		X	X	X			
X	Arctostaphylos pumila	Sandmat Manzanita		X	X	X		X	X	X			
X	Arctostaphylos purissima	La Purisima Manzanita		X	X	X		X	X	X			
X	Arctostaphylos rudis	Sand Mesa Manzanita		X	X	X		X	X	X			
X	Arctostaphylos uva-ursi	Bear Berry, Kinnikinnick		X	X	X		X	X	X			
	Agapanthus africanus	Lily Of The Nile		X	X	X	X		X	X	X	X	
X	Artemisia californica	California Sagebrush						X	X	X	X		
X	Artemisia douglasiana	Mugwort						X	X	X	X	X	
	Asteriscus sericeus	Canary Island Daisy		X	X	X			X	X			
X	Astragalus trichopodus var. lonchus	Santa Barbara Milk Vetch							X	X	X		
X	Atriplex canescens	Four-wing Salt Bush							X	X	X		
X	Atriplex nummularia	Saltbush or Sand Mat							X	X	X		
X	Baccharis pilularis spp.	Coyote Bush		X	X	X	X	X	X	X	X		
X	Calliandra californica	Red Baja Fairy Duster		X	X	X			X	X			
X	Ceanothus foliosus	Wavy Leaf Mountain lilac		X	X	X	X	X	X	X	X		
X	Ceanothus gloriosus	Mountain lilac		X	X	X	X	X	X	X	X		
X	Ceanothus griseus	Carmel Ceanothus		X	X	X	X	X	X	X	X		
X	Ceanothus hearstiorum	San Simeon Ceanothus		X	X	X	X	X	X	X	X		
X	Ceanothus maritimus	Bluff California Lilac		X	X	X	X	X	X	X	X		
X	Ceanothus thyrsifolius	Coastal Mountain lilac		X	X	X	X	X	X	X	X		
	Centaurea cineraria	Velvet Centaurea, Dusty Miller		X	X	X			X	X			
	Cercocarpus betuloides	Mountain mahogany		X	X	X			X	X	X		
	Chamelaucium uncinatum	Wax Flower		X	X	X				X	X		
	Cistus spp.	Rock rose		X	X	X			X	X			
X	Coreopsis gigantea	Giant coreopsis		X	X	X		X	X	X			
X	Coreopsis maritima	Sea Dahlia		X	X	X		X	X	X			
	Correa spp.	Australian Fuchsia		X	X	X				X	X		
X	Dendromecon harfordii	Channel Island Bush Poppy		X	X	X	X	X	X	X	X		
X	Dendromecon rigida	Tree poppy, Bush Poppy		X	X	X	X	X	X	X	X		
	Dianella spp.	Dianella		X	X	X			X	X			
	Echium candicans	Pride of Madeira		X	X	X			X	X			
X	Encelia californica	California Coast Sunflower		X	X	X	X	X	X	X	X		
X	Encelia farinosa	Brittlebush or Incienso		X	X	X	X	X	X	X	X		



TABLE 07.2-3 // SHRUB &amp; PERENNIALS

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
X	<i>Encelia ventorum</i>	Baja Bush Sunflower		X	X	X	X	X	X	X	X	X	
X	<i>Epilobium canum</i>	California fuchsia		X	X	X	X	X	X	X	X	X	
	<i>Eremophila hygrophana</i>	Blue bells		X	X	X					X	X	
	<i>Eremophila x</i>	Emu Bush		X	X	X					X	X	
X	<i>Ericameria laricifolia</i>	Turpentine Bush		X	X	X					X	X	
X	<i>Erigeron glaucus</i>	Seaside daisy		X	X	X	X	X		X	X	X	
	<i>Erigeron karvinskianus</i>	Santa Barbara Daisy		X	X	X	X	X			X	X	
	<i>Eriogonum parvifolium</i>	Coast Buckwheat		X	X	X	X	X	X	X	X	X	
X	<i>Eriophyllum nevinii</i>	Island snow flake, Catalina Silverlace		X	X	X	X	X	X	X	X	X	
X	<i>Erysimum insulare ssp. suffrutescens</i>	Island Wallflower								X			
	<i>Euphorbia rigida</i> (E. biglandulosa)	Gopher Plant		X	X	X					X	X	
X	<i>Galvezia speciosa</i>	Island Snapdragon		X	X	X	X	X	X	X	X	X	
	<i>Gaura lindheimeri</i>	Gaura		X	X	X					X	X	
	<i>Gaillardia</i>	Blanket flower		X	X	X					X	X	
	<i>Grevillea spp</i>	Grevillea		X	X	X					X	X	
X	<i>Grindelia spp</i>	Gum plant		X				X	X	X	X	X	X
	<i>Hamelia patens</i>	Texas Firecracker		X	X	X					X	X	
	<i>Havardia pallens</i> (Pithecellobium Pallens)	Haujillo		X	X	X					X	X	
X	<i>Hazardia cana</i>	San Clemente Island Hazardia		X				X	X	X	X	X	X
	<i>Helichrysum italicum</i>	Licorice Plant		X	X	X					X	X	
X	<i>Heuchera sanguinea</i>	Coralbells		X	X	X	X				X	X	
X	<i>Iris douglasiana</i> and PCH hybrids	Douglas Iris		X	X	X	X	X	X	X	X	X	
X	<i>Isocoma menziesii</i> var. <i>menziesii</i>	Coast Golden Bush								X			
X	<i>Isomeris arborea</i>	Bladderpod								X			
	<i>Ilex vomitoria</i> 'Stokes'	Stokes Yaupon		X	X	X					X	X	X
	<i>Juniperus procumbens</i>	Spreading Juniper		X	X	X					X	X	
	<i>Juniperus rigida conferta</i>	Shore Juniper		X	X	X					X	X	
	<i>Justicia spicigera</i>	Mexican Honeysuckle		X	X	X					X	X	
X	<i>Lavatera assurgentiflora</i>	Island Tree Mallow								X	X	X	
X	<i>Lepechinia calycina</i>	Pitcher sage		X	X	X				X	X	X	
X	<i>Lepechinia fragrans</i>	Fragrant Pitcher Sage		X	X	X				X	X	X	
X	<i>Lotus scoparius scoparius</i>	Deerweed								X	X		
X	<i>Lupinus arboreus</i> var. <i>arboreus</i>	Yellow Tree Lupine		X	X	X	X	X	X	X	X	X	
X	<i>Lupinus chamissonis</i>	Dune lupine		X				X	X	X			
X	<i>Lycium californicum</i>	California Box Thorn						X	X	X	X	X	
X	<i>Mimulus aurantiacus</i>	Sticky Monkeyflower		X	X	X	X	X	X	X	X	X	
X	<i>Mimulus clevelandii</i>	Cleveland's Monkey Flower		X	X	X	X	X	X	X	X	X	

TABLE 07.2-3// SHRUB &amp; PERENNIALS

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
				1	2	3	4	5	6	7	8	9	10
X	<i>Oenothera elata hookerii</i>	Hooker's Evening Primrose		X					X	X	X	X	
	<i>Pelargonium tomentosum</i>	Peppermint-scented Geranium		X	X	X				X	X		
X	<i>Penstemon</i> spp	Penstemon		X	X	X	X		X	X	X		
	<i>Phlomis fruticosa</i>	Jerusalem Sage		X	X	X				X	X		
	<i>Phormium tenax</i>	New Zealand Flax		X	X	X				X	X		
X	<i>Polypodium californicum</i>	California polypody fern		X	X	X				X	X		X
	<i>Rhaphiolepis</i> spp	Hawthorn		X	X	X				X	X		
X	<i>Rhus integrifolia</i>	Lemonade Berry		X	X	X			X	X	X		
	<i>Rhus laurina</i>	Laurel Sumac		X	X	X			X	X	X		
	<i>Rosmarinus</i> spp.	Rosemary		X	X	X	X			X	X		
	<i>Rusellia equisetiformis</i>	Coral Fountain		X	X	X				X	X		
	<i>Rusellia</i> x	Coral Fountain cultivars		X	X	X				X	X		
X	<i>Salvia apiana</i>	White Sage		X	X	X	X		X	X	X		
X	<i>Salvia brandegeei</i>	Brandegee's Sage		X	X	X	X		X	X	X		
X	<i>Salvia clevelandii</i>	Cleveland Sage		X	X	X	X		X	X	X		
X	<i>Salvia columbariae</i>	Chia		X	X	X	X		X	X	X		
	<i>Salvia leucantha</i>	Mexican Bush Sage		X	X	X			X	X	X		
X	<i>Salvia leucophylla</i>	Purple Sage		X	X	X	X		X	X	X		
X	<i>Salvia millifera</i>	Black Sage		X	X	X	X		X	X	X		
X	<i>Salvia munzii</i>	San Miguel Mtn or Munz's Sage		X	X	X	X		X	X	X		
X	<i>Salvia spathacea</i>	Hummingbird Sage		X	X	X	X		X	X	X		
	<i>Salvia</i> spp	Sage, Non native spp		X	X	X				X	X		
	<i>Senna oliogophylla</i>	Outback Cassia		X	X	X				X	X		
	<i>Tecoma stans</i>	Yellow bells		X	X	X				X	X		
	<i>Tecoma</i> hybrids/cultivars			X	X	X				X	X		
	<i>Teucrium chamaedrys</i>	Germander		X	X	X				X	X		
	<i>Teucrium cosnii</i>	Majorcan Germander		X	X	X				X	X		
	<i>Teucrium fruticans</i>	Bush Germander		X	X	X				X	X		
	<i>Teucrium x lucidrys</i>	Germander		X	X	X				X	X		
X	<i>Trichostema lanatum</i>	Woolly Blue Curls or Romero		X	X	X			X	X	X		
	<i>Westringia fruticosa</i>	Coast Rosemary		X	X	X				X	X		



TABLE 07.2-4 // GROUNDCOVER &amp; VINES

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
X	<i>Abronia maritima</i>	Red sand verbena		X	X	X	X	X	X	X	X		
X	<i>Abronia umbellata</i>	Beach Sand Verbena		X	X	X	X	X	X	X	X		
X	<i>Abronia villosa</i>	Desert sand verbena		X	X	X	X	X	X	X	X		
X	<i>Achillea millefolium</i>	Yarrow		X	X	X	X	X	X	X	X		
	<i>Ambrosia pumila</i>	San Diego Ambrosia						X					
	<i>Antigonon leptopus</i> 'Baja Red'	Queen's Wreath											
X	<i>Armeria maritima</i>	Thrift, Sea Pink		X	X	X	X	X	X	X	X		
	<i>Armeria douglasiana</i>	Mugwort		X	X	X				X	X		
X	<i>Aster chilensis</i>	Coast Aster or California Aster		X	X	X			X	X	X		
	<i>Asteriscus maritimus</i>	Gold coin		X	X	X				X	X		
	<i>Asteriscus sericeus</i>	canary Island daisy		X	X	X				X	X		
X	<i>Atriplex barklayana</i>	Dwarf Saltbush						X	X	X	X		
X	<i>Atriplex leucophylla</i>	Beach Saltbush						X	X	X	X		
	<i>Bougainvillea</i> spp.	Bougainvillea		X	X	X				X	X		
X	<i>Calystegia macrostegia</i>	Island Morning Glory		X	X	X	X	X	X	X	X		
	<i>Cissus</i> spp.	Grape Ivy		X	X	X				X	X		
	<i>Distictis buccinatoria</i>	Blood-red trumpet vine		X	X	X					X	X	
X	<i>Eriophyllum confertiflorum</i> var. <i>confertiflorum</i>	Golden Yarrow		X	X	X	X	X	X	X	X		
X	<i>Fragaria chiloensis</i>	Beach Strawberry		X	X	X	X	X	X	X	X	X	
	<i>Gazinia rigens</i>	Trailing Gazinia		X	X	X				X	X		
	<i>Geranium incanum</i>	Carpet Geranium		X	X	X				X	X		
	<i>Hardenbergia comptoniana</i>	Lilac Vine, Native wisteria		X	X	X				X	X		
	<i>Hardenbergia violacea</i>	Purple Vine Lilac		X	X	X				X	X		
	<i>Hibbertia scandens</i>	Guinea Gold Vine		X	X	X					X	X	
	<i>Hylocereus undatus</i>	Pitaya, Dragonfruit			X	X				X	X		
	<i>Kniphofia uvaria</i>	Hot Poker		X	X	X				X	X		
	<i>Lantana montevidensis</i>	Purple Trailing Lantana		X	X	X	X			X	X		
	<i>Lantana</i> spp.	Lantana		X	X	X				X	X		
	<i>Lavandula</i> spp.	Lavender		X	X	X				X	X		
X	<i>Lessingia filaginifolia</i>	California aster		X	X	X	X	X	X	X	X		
X	<i>Leptodactylon californicum</i>	Prickly Phlox							X	X			

TABLE 07.2-4 // GROUNDCOVER &amp; VINES

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
				1	2	3	4	5	6	7	8	9	10
X	<i>Limonium californicum</i>	Sea Lavender, Marsh Rosemary							X	X	X	X	
	<i>Lobelia laxiflora</i>	Loose Flowered Lobelia		X	X	X				X	X		
	<i>Lonicera japonica</i>	Japanese Honeysuckle		X	X	X				X	X		
X	<i>Lotus heermannii</i> var. <i>heermannii</i>	Heermann's Bird's Foot Trefoil					X	X	X				
X	<i>Monardella linoides</i>	Willow Mint						X	X				
	<i>Muehlenbeckia complexa</i>	Wire Vine, Mattress Vine		X	X	X				X	X		
X	<i>Oenothera caespitosa</i>	White Evening Primrose		X	X	X				X	X		
	<i>Osteospermum</i> spp	Trailing african daisy		X	X	X				X	X		
	<i>Parthenocissus quinquefolia</i>	Virginia Creeper		X	X	X				X	X		
	<i>Plecotachys serpyllifolia</i>	Dwarf Plecotachys, S. African Whit		X	X	X				X	X		
X	<i>Rosa minutifolia</i>	Baja wild rose						X					
	<i>Santolina chamaecyparissus</i>	Lavender Cotton		X	X	X				X	X		
X	<i>Sidalcea malviflora</i>	Checkerbloom						X					
X	<i>Senecio flaccidus</i> var. <i>douglasii</i>	Butterweed, Bush groundsel						X					
	<i>Sphagneticola trilobata</i>	Yellow Dot		X	X	X				X	X		
X	<i>Tanacetum camphoratum</i>	Camphor Dune tansy						X					
	<i>Thymus</i> spp.	Thyme		X	X	X				X	X		
	<i>Thunbergia alata</i>	Black-eyed Susan		X	X	X				X	X		
	<i>Thunbergia gregorii</i>	Orange Clock Vine		X	X	X				X	X		
	<i>Zephyranthes</i> x	Rain Lily		X	X	X				X	X		

TABLE 07.2-5 // HERBACIOUS &amp; BULB-LIKE

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
				1	2	3	4	5	6	7	8	9	10
X	<i>Allium haematochiton</i>	Red-Skinned Onion		X			X	X	X				
X	<i>Asclepias speciosa</i>	Butterfly weed		X	X	X	X	X	X				
X	<i>Calochortus catalinae</i>	Catalina mariposa lily		X	X	X		X	X	X			
X	<i>Camissonia cheiranthifolia</i>	Beach evening primrose		X	X	X		X	X	X			
	<i>Drimiopsis maculata</i>	African Hosta		X	X	X			X	X			
X	<i>Eschscholzia caespitosa</i>	Foothill Poppy		X	X	X	X	X	X	X			
X	<i>Eschscholzia californica</i> var. <i>maritima</i>	California Poppy		X	X	X	X	X	X	X			
X	<i>Ratibida columnifera</i>	Mexican Hat		X	X	X			X	X			



TABLE 07.2-6// SUCCULENTS &amp; SOD

ALL coastal adapted, non invasive species are acceptable.

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
	Aeonium spp			X	X	X				X	X		
	Agave spp.	Agave		X	X	X				X	X		
X	Agave shawii	Shaw agave		X	X	X			X	X	X		
	Aloe spp.	Aloe		X	X	X				X	X		
	Bulbine frutescens	NCN		X	X	X				X	X		
	Cereus hildmannianus (C. peruvianus)	Hedge Cactus		X	X	X				X	X		
	Delosperma litorale	White trailing iceplant		X	X	X				X	X		
X	Dudleya spp.	Dudleya		X	X	X			X	X	X		
	Echeveria spp	Hens and Chicks		X	X	X				X	X		
X	Euphorbia misera	Cliff Spurge		X	X	X			X	X	X		
	Furcraea foetida	Green Aloe											
	Furcraea gigantea	False Agave, Mauritius Hemp		X	X	X				X	X		
	Hesperaloe funifera	Giant Hesperaloe		X	X	X				X	X		
	Hesperaloe parviflora	Hesperaloe		X	X	X				X	X		
X	Jaumea carnosa	Jaumea		X	X	X			X	X	X	X	
	Kalanchoe spp	NCN		X	X	X				X	X		
	Manfreda maculosa	Manfreda, Texas Tuberose		X	X	X				X	X		
X	Opuntia littoralis	Prickly Pear		X	X	X				X	X		
	Portulacaria afra	Elephant's Food		X	X	X				X	X		
	Senecio spp.	Chalksticks		X	X	X				X	X		
X	Yucca baccata	Banana Yucca		X	X	X			X	X	X		
	Yucca gloriosa	Spanish dagger		X	X	X			X	X	X		
	Yucca pallida	Pale Leaf Yucca		X	X	X			X	X	X		
	Yucca rigida	Mexican Blue Yucca		X	X	X			X	X	X		
	Yucca rostrata	Beaked Yucca		X	X	X			X	X	X		
	Yucca rupicola	Twisted Leaf Yucca		X	X	X			X	X	X		
X	Yucca whipplei	Chaparral Yucca		X	X	X			X	X	X		
X	Preservation Mix by S & S seeds				X	X				X	X		
X	Native Mow Free by S & S seeds				X	X				X	X		
	NO-MOW (fescue)				X	X				X	X	X	
	GN-1 hybrid Bermuda									X			
	Tifway 419									X			
	Kakua									X			

TABLE 07.2-7 // GRASSES, SEDGES &amp; RUSHES

ALL coastal adapted, non invasive species are acceptable.

NATIVE	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
X	Agrostis exarata	Spike Bent Grass		X	X	X		X	X	X			
X	Agrostis pallens (A. snadiegoensis)	San Diego Bent Grass		X	X	X		X	X	X			
	Agrostis palustris	Creeping Bent Grass		X	X	X		X	X	X			
	Agrostis tenuis	Colonial Grass		X	X	X		X	X	X			
	Carex glauca	Blue sedge		X	X	X			X	X	X		
X	Carex praegracilis	Dune sedge		X	X	X		X	X	X	X		
	Chondropetalum elephantium	Large cape rush		X	X	X		X	X	X	X		
	Chondropetalum tectorum	Small cape rush		X	X	X		X	X	X	X		
	Cynodon dactylon	Bermuda grass		X	X	X		X	X	X			
	Dasyllirion quadrangulatum (D. longissimum)	Toothless Desert Spoon		X	X	X		X	X	X			
	Dasyllirion wheeleri	Grey Desert Spoon		X	X	X		X	X	X			
	Dasyllirion texanum	Green Desert Spoon		X	X	X		X	X	X			
X	Distichlis spicata	Salt grass						X	X	X	X		
X	Festuca californica var. parishii	California Fescue		X	X	X		X	X	X	X		
X	Festuca rubra	Red fescue		X	X	X	X	X	X	X	X		
X	Hordeum intercedens	Little Barley						X	X	X			
	Isolepis nodosa	Knobby Club Rush		X	X	X			X	X	X		
X	Juncus patens	Wire grass		X	X	X		X	X	X	X		
X	Leymus condensatus	Giant wild rye		X	X	X		X	X	X			
	Leymus triticoides	creeping wild rye		X	X	X			X	X			
	Lolium multiflorum	Annual Ryegrass		X	X	X		X	X	X			
	Lolium perenne	Perennial Ryegrass		X	X	X		X	X	X			
	Lomandra spp and hybrids	Mat Rush		X	X	X	X		X	X	X		
	Poa annua	Annual Bluegrass		X	X	X		X	X	X			
X	Poa secunda var. juncifolia	Pine Bluegrass		X	X	X	X	X	X	X			
X	Poa secunda var. secunda	One-sided Bluegrass		X	X	X	X	X	X	X			
	Poa trivialis	Rough-stalked Bluegrass		X	X	X	X	X	X	X			
	Nolina nelsonii	Blue Nolina		X	X	X			X	X			
	Ophiopogon spp.	Mondo grass		X	X	X			X	X	X		
	Zoysia matrella	Manila Grass		X	X	X			X	X			
	Zoysia tenuifolia	Korean Grass		X	X	X			X	X	X		





Urban Tree Line - Aleppo Pine



Westchester Parkway Median - Morina Strawberry Tree



Westchester Right-of-Way - New Zealand Christmas Tree

## ***Landscape Materials & Their Locations***

LAX Northside Center and LAX Northside  
Campus Districts

These images show examples of  
recommended plantings.



Parking Lot Bioswales - Jaumea



Parking Lot Bioswales - Arroyo Willow



Parking Lot Bioswales - Western Redbud



Buffer & Open Space - Coast Buckwheat



Buffer & Open Space - California Poppy



Buffer & Open Space - Beach Evening Primrose

# PART II

## 8 PASEO & PUBLIC REALM

"Public realm" as described in this portion of the guidelines and standards refers to the extended right-of-way introduced in the LAX Northside that will accommodate the introduction of a continuous paseo experience connecting Sepulveda Boulevard with Pershing Drive along Westchester Parkway. The paseo will provide character, identity and form to the built environment in the LAX Northside. The purpose of the public realm and paseo guidelines and standards is to establish a framework for the design and articulation of active and passive recreation within the LAX Northside, defining the guidelines and standards for how this pedestrian infrastructure will look and the character it will capture. These guidelines and standards include restrictions intended to achieve compatibility with adjacent communities and uses, such as the airport, while maintaining the flexibility needed to respond to individual identities and the latest best-practices in recreation design and sustainability.

### PUBLIC REALM AND PASEO DESIGN GUIDELINES AND STANDARDS CHECKLIST

#### 8 Public Realm and the Paseo Guidelines and Standards

The public realm and paseo shall conform to the following standards.

##### 8.1 Path Dimensions and Locations

Paving shall consist of stabilized decomposed granite in the following depths and locations :

<input type="checkbox"/>	1. A minimum of twelve (12) feet between the existing sidewalk within the 50-foot building setback located in Area 11 along the Runway Protection Zone (RPZ) boundary.
<input type="checkbox"/>	2. A minimum of twelve (12) feet between the existing sidewalk and the 18-foot building setback located in Area 12A East along Westchester Parkway.
<input type="checkbox"/>	3. A minimum of twelve (12) feet between the existing sidewalk and the 15-foot building setback located in Area 12A West along Westchester Parkway.
<input type="checkbox"/>	4. A minimum of twelve (12) feet between the existing sidewalk and the 38-foot building setback located along Westchester Parkway in Areas 1, 2 and 3.
<input type="checkbox"/>	5. A minimum of twelve (12) feet between the existing sidewalk and the 38-foot building setback located along Westchester Parkway and Pershing Drive in Area 1.

##### 8.2 Streetscapes

The public realm streetscape includes only the pedestrian accessible paseo and does not apply to existing right-of-way along Westchester Parkway.

Where sidewalks are being introduced, in particular Area 11, they shall be ten (10) feet wide and shall be designed to the standards set forth by the City of Los Angeles.

<input type="checkbox"/>	1. All tree wells shall have root barriers to prevent material deterioration of the sidewalks and recreation paths.
<input type="checkbox"/>	2. All soil in tree wells shall be finished with a minimum of two (2) inches of decomposed granite that is not stabilized.



### 8.3 Street Furnishings

Street furniture elements include bench seating, bollards, planters, trash receptacles, and bike and newspaper racks located in the public right-of-way at locations such as bus shelters, street intersections, transit stations, and public plazas where high numbers of pedestrians commonly congregate or where entrances are provided to developments.

General objectives are as follows: 1) To provide street furniture and amenities that are functional, durable, and easy to maintain; 2) to provide street furniture which provides access and ease of use for handicapped persons; and, 3) to provide amenities to help promote safe, visually pleasing, and comfortable pedestrian environments.

#### 8.3A Bench Seating

<input type="checkbox"/>
--------------------------

1. Benches shall be located along walkways, with a maximum distance of one thousand (1,000) feet between each seating area. In addition, various configurations and seat types shall be located in appropriate quantities to respond to user needs at transit stations, retail environments, bus shelters, street intersections, and public plazas.

2. Benches should be durable and sturdy, with attractive design.

#### 8.3B Bike Racks

<input type="checkbox"/>
--------------------------

1. Bike racks shall be located along walkways, near building entrances, intersections, transit stations, bus shelters, and any other pedestrian gathering areas. Spacing shall be at a maximum distance of one thousand (1,000) feet and in clusters of three (3).

2. Bike racks should be durable and sturdy, with attractive design.

#### 8.3C Lighted Bollards (excluding safety bollards)

<input type="checkbox"/>
<input type="checkbox"/>

1. Lighted bollards shall be located at street intersections where they will be used to define the boundary between pedestrian and vehicular zones. Lighted bollards may also be used to delineate pedestrian walkways.

2. Lighted bollard spacing shall be at a minimum distance of twenty (20) feet along both sides of the paseo.

#### 8.3D Planters

<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

1. Planters shall be used in conjunction with other street furniture, such as benches, bollards, or trash receptacles. Planters shall be located in areas where pedestrians gather.

2. Planters shall not exceed 36 inches in height.

3. Unless maintained on a regular schedule, all planters shall be irrigated.

4. Planters shall be planted with materials selected from the list of acceptable plants specified for the Paseo and Streetscape Zones located in sections 07.3 and 07.4 of this document.

#### 8.3E Trash Receptacles

<input type="checkbox"/>
--------------------------

1. Trash receptacles shall be located along pedestrian walkways, near parcel entry plazas, seating areas, transit stops, public plazas, and other pedestrian gathering areas.

#### 8.3E Trash Receptacles, continued

<input type="checkbox"/>
<input type="checkbox"/>

2. Trash receptacle spacing shall not exceed a distance of one thousand (1,000) feet and shall be placed adjacent to benches and planters.

3. All trash receptacles shall be covered.

# PART II

## 9 SIGNAGE & GRAPHICS

"Signage" as described in this portion of the design guidelines and standards refers to all graphic information that conveys location or direction and provides character, identity and form to the built environment in the LAX Northside. The purpose of the signage and graphics design guidelines and standards is to establish a framework for the design and character of signage for tenants and the public realm of the LAX Northside. This means defining the guidelines and standards for size, location, and material for signage. This Chapter includes the guidelines and standards to achieve compatibility with adjacent uses, while maintaining the flexibility to respond to individual identities.

### SIGNAGE AND GRAPHICS DESIGN GUIDELINES AND STANDARDS CHECKLIST

#### 9 Signage and Graphics Design Guidelines and Standards

Signage and graphics shall comply with the following standards.

##### 9.1 Address Sign

- |                          |   |
|--------------------------|---|
| <input type="checkbox"/> | 1. Address signs shall be required for all tenants. Address signs include any sign that is used to communicate the numerical or alphanumeric identification of a given business or development's address on a street. |
|--------------------------|---|

##### 9.2 Tenant Identification Sign

"Tenant Identification Sign" refers to a wall sign that is limited to a company logo, generic type of business, or the name of a business or building.

- |                          |  |
|--------------------------|--|
| <input type="checkbox"/> | 1. A maximum of two (2) illuminated identification signs on two separate elevations of the building are allowable.   |
| <input type="checkbox"/> | 2. Signage shall be located on building frontages and primary entry facades.   |
| <input type="checkbox"/> | 3. Illumination brightness of signs shall be restricted to no greater than two foot candles above ambient lighting, measured at the property line of the nearest residentially zoned property.   |
| <input type="checkbox"/> | 4. Surface brightness of all translucent materials shall be consistent in all components of the sign.  |
| <input type="checkbox"/> | 5. All conduits, lamps or transformers specific to the operation of the sign shall be entirely concealed within the sign.  |
| <input type="checkbox"/> | 6. Tenant signage may not project above the building, as in the manner of common billboards. However, tenants are allowed to use the flat roof surfaces for signage and advertisements as long as the signage and/or advertisement is not visible to adjacent residential properties.  |
| <input type="checkbox"/> | 7. An exposed light source (neon, incandescent) is prohibited.   |
| <input type="checkbox"/> | 8. Signs employing animated components, moving/flashing or blinking lights, exposed raceways, exposed ballast boxes or transformers, unedged or uncapped plastic letters or letters with no returns and exposed fastenings, luminous-vacuum formed type plastic letters, and sandblasted wood type construction shall be prohibited from use in the LAX Northside. |
| <input type="checkbox"/> | 9. Visible sign manufacturer's names, stickers, stamps or decals are prohibited on any sign or graphic.  |
| <input type="checkbox"/> | 10. Simulated materials (i.e., wood grained plastic laminate, etc.) are prohibited as a primary sign surface.  |



#### 9.2 Tenant Identification Sign, Continued

<input type="checkbox"/>	11. Each Identification Sign shall not exceed four (4) feet six (6) inches in height in the LAX Northside Center and three (3) feet in height in the LAX Northside Campus Districts.
<input type="checkbox"/>	12. Signs shall not overlap or directly impact the clarity of architectural features, such as mullions or window breaks.
<input type="checkbox"/>	13. A maximum of two tenant identification signs shall be placed on a building, a maximum of one per façade can be illuminated. If a retail building is free standing, an additional identification monument sign is permitted.
<input type="checkbox"/>	14. One monument sign is permitted at the site entrance of a recreational use and shall not exceed four (4) feet six (6) inches in height, measured from the finish grade.

#### 9.3 Temporary Signage

"Temporary Signage," as defined in these guidelines refers to all forms of signage that are temporary in their use, such as construction related signage (walls/barricades, entries, etc.), lease signs, flags, banners and pennants.

<input type="checkbox"/>	1. Lease signs attached to buildings shall be limited to the ground floor.
<input type="checkbox"/>	2. A maximum of three flag poles for advertisement flags will be allowed for each building, and flag poles should be no taller than 30 feet.
<input type="checkbox"/>	3. Flags shall be no larger than 6' x 9'.
<input type="checkbox"/>	4. Banners and pennants shall only be permitted in interior court areas and not visible from public right-of-ways.
<input type="checkbox"/>	5. Special occasion banners or pennants shall be temporarily erected.
<input type="checkbox"/>	6. Temporary signs shall be submitted to the reviewing authority for approval prior to their installation.
<input type="checkbox"/>	7. No temporary sign shall be visible for more than a consecutive 30-day term.

#### 9.4 Maintenance Guidelines

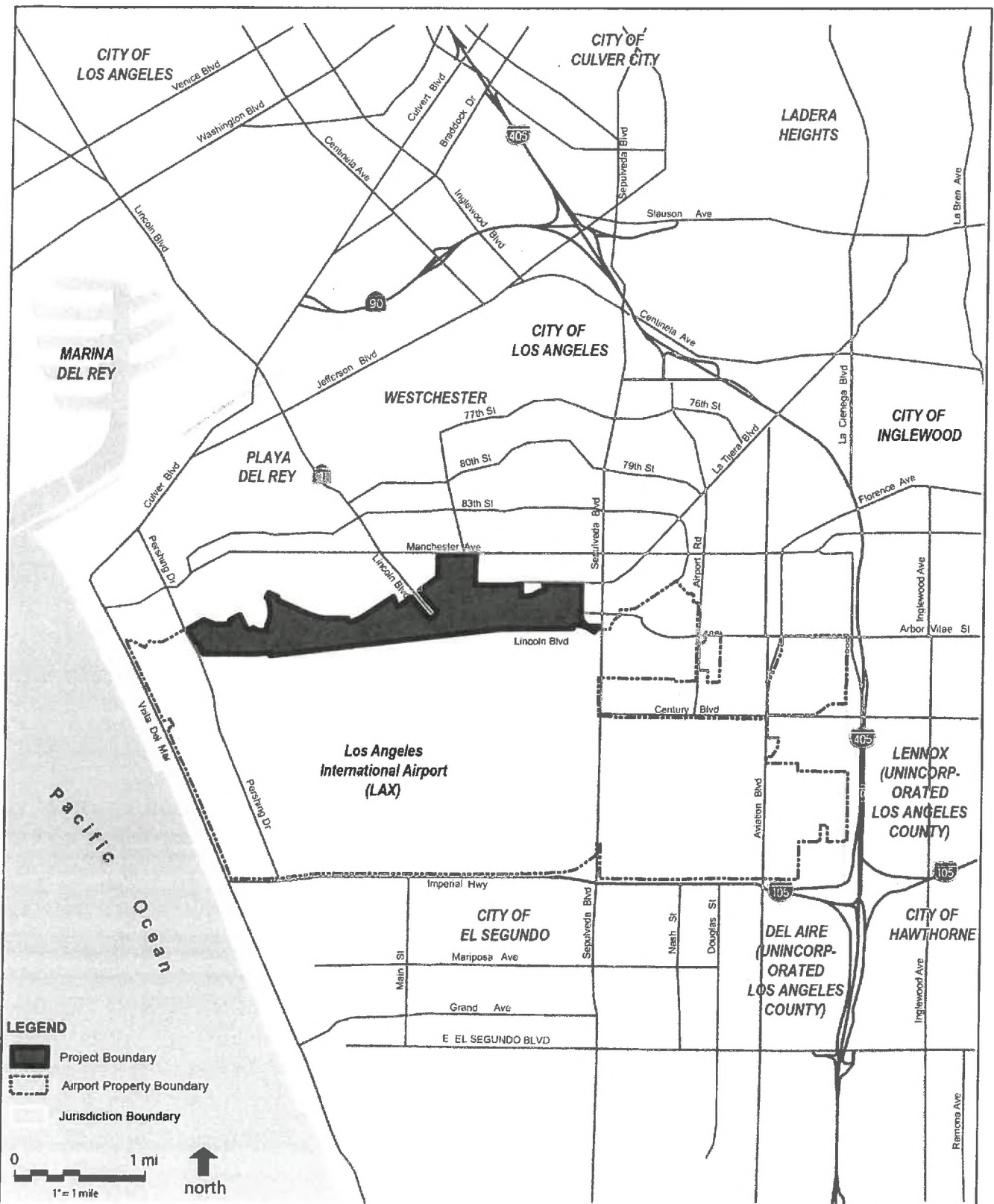
These maintenance guidelines shall apply to individual parcel developers and are applicable to all Districts of the LAX Northside.

1. Every sign should be maintained in a clean safe and good working condition, including the replacement of defective parts, defaced or broken faces, lighting and other acts required for maintenance of the sign.
2. Sign display surfaces should be kept neatly painted or finished at all times.
3. The base of any sign erected on the ground should be kept clear of weeds, rubbish or other combustible material at all times.
4. All signage shall be removed, or the face of said signs should be removed and replaced with blank panels painted to match adjacent background colors within ninety (90) days of a close of business.









Source: LAWA 2011; ESRI Maps and Data - March 2014; Prepared by: URS Corporation, 2014

EXHIBIT C1

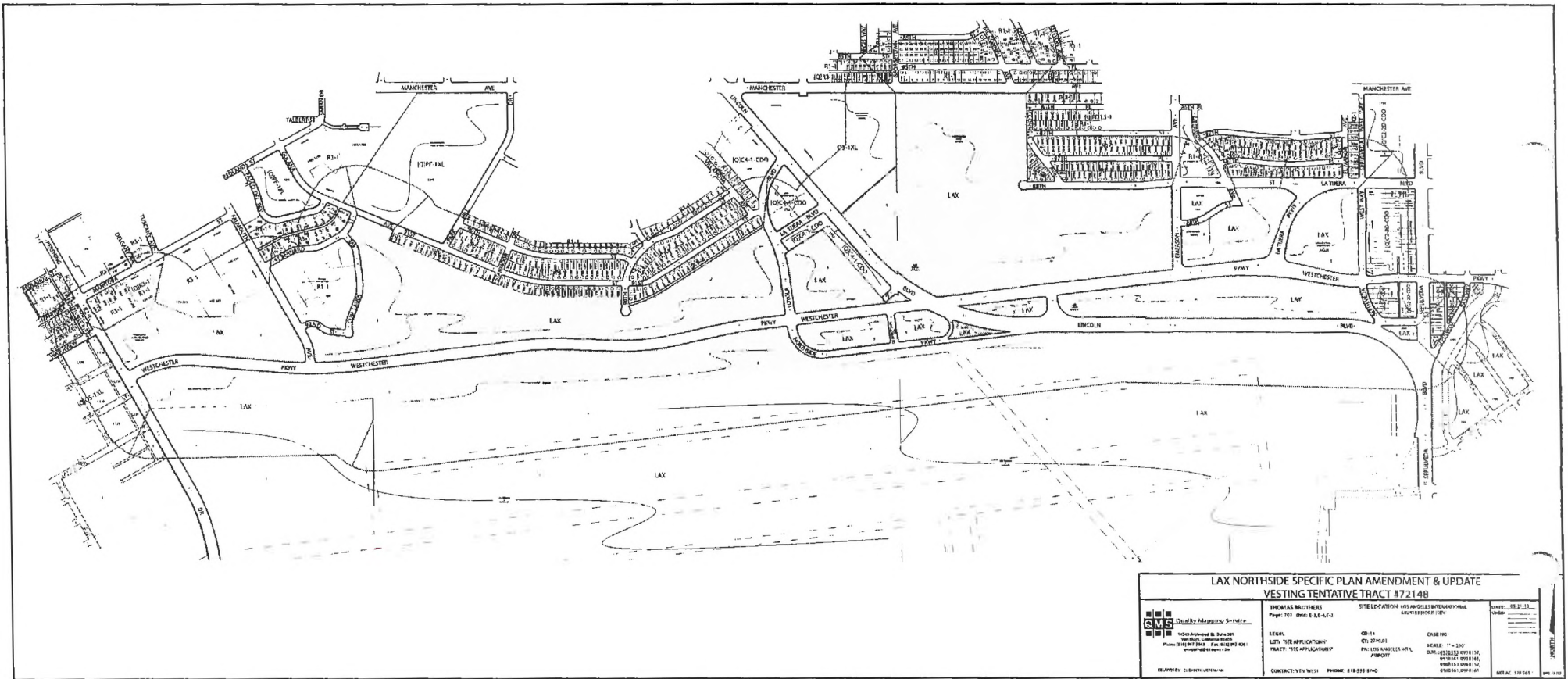
VICINITY MAP





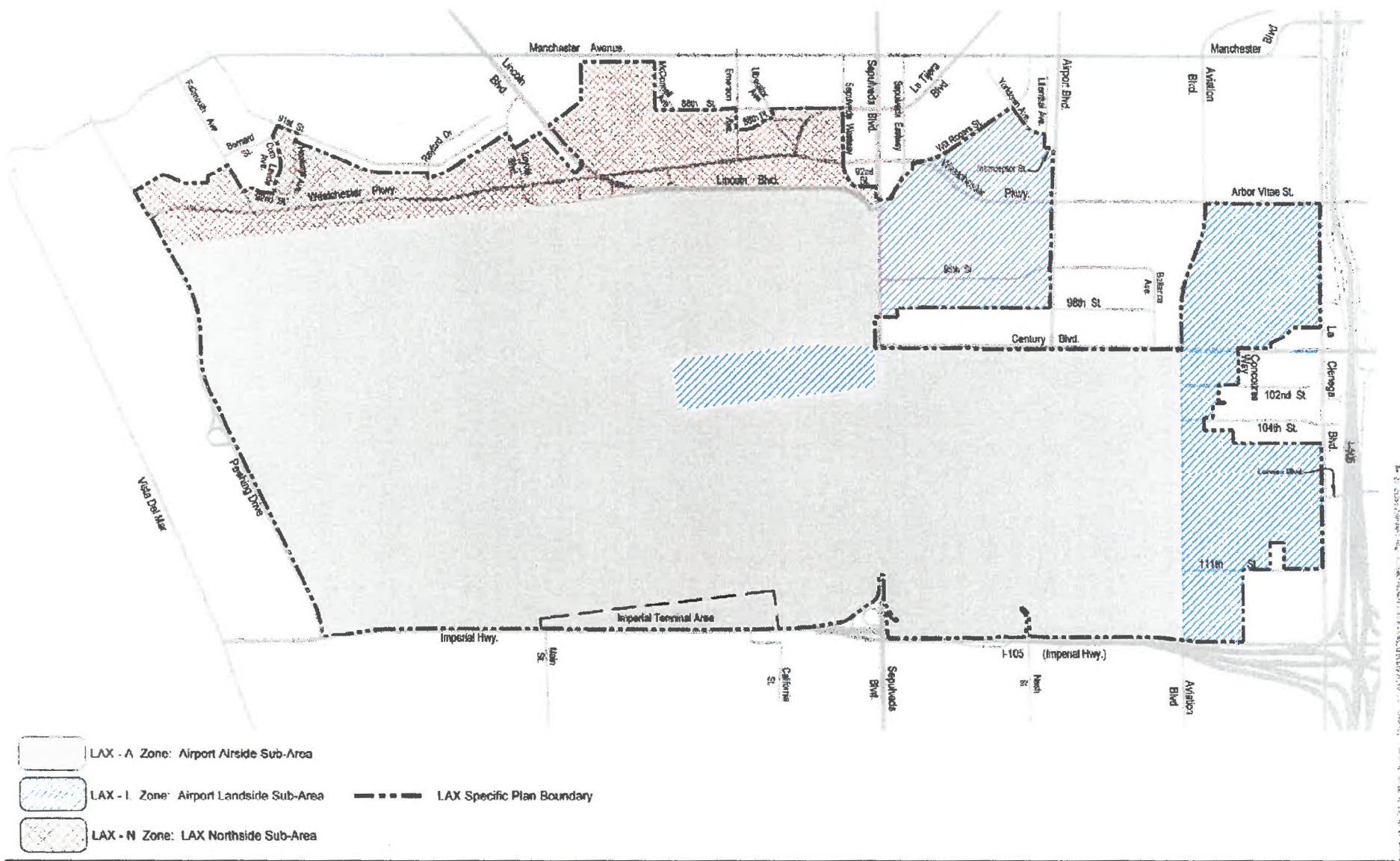
# EXHIBIT C2

## RADIUS MAP









# EXHIBIT C3 SPECIFIC PLAN MAP

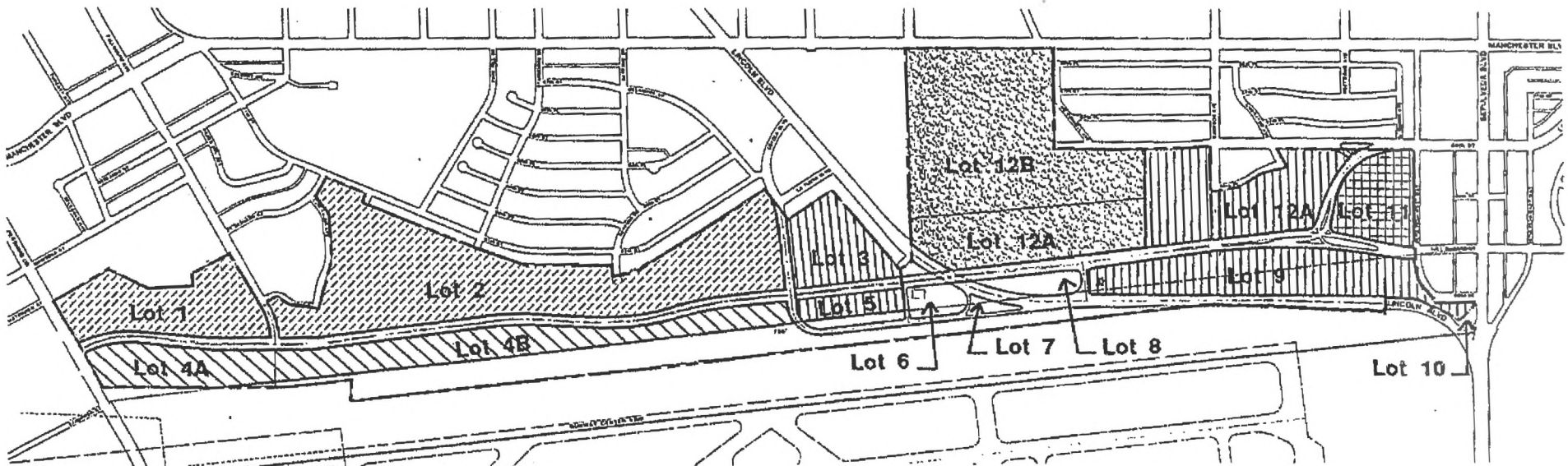
Specific Plan Sub-Areas



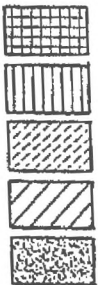


# EXHIBIT C4

## EXISTING LAND USE MAP



- Mixed Use: Hotel / Office / Retail
- Commercial: Hotel / Office / Retail
- Research Park
- Airport Related Uses
- Golf Course





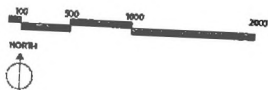








MANCHESTER AVE







# **EXHIBIT E**

**LETTERS RECEIVED FROM THE PUBLIC**







Karen Hoo &lt;karen.hoo@lacity.org&gt;

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## Notice Of Public Hearing

4 messages

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Earl <earlsmitley7@msn.com>

Wed, Mar 4, 2015 at 4:46 PM

To: karen.hoo@lacity.org

Cc: JoAnn Smitley &lt;jsmitley@visitationschool.org&gt;

Ms Hoo,

I am writing to you as you are listed as the Staff Contact for the Department Of City Planning for the LAX Airport Plan.

I am a resident of Westchester since 1968, living at the same address the entire period of time. I received a letter notification on March 2 of a public hearing scheduled for March 26, 2015 to discuss the LAX project. I have two major issues about this notification:

1. The hearing is scheduled to be held in downtown Los Angeles City Hall. Common sense would dictate that the meeting be held in Westchester so that the residents who have an interest and are impacted by the LAX project could participate in the meeting and have their voices heard. It appears to me that by having the meeting at City Hall during morning hours is a deliberate strategy to minimize attendance by making it as inconvenient as possible for the residents plus they're having to pay an exorbitant parking fee to attend the meeting. There are many facilities that could be made available to hold the meeting in Westchester that would enable the residents to participate. Also, the timing of the meeting should be changed to accommodate working home owners to attend the meeting during evening hours. The Department Of City Planning needs to reach-out to the public and inconvenience themselves rather than inconveniencing the home owners. And, by the way, holding the meeting in Westchester would not require residents to have to pay for parking in order to attend the meeting. I'm sure that one of the several schools or churches in the area would be willing to accommodate the meeting. The Emerson Manor Adult School auditorium located on W. 88th Street would be an appropriate location for such a meeting.

2. The area map included with the letter was completely useless. Even with a magnifying glass I couldn't determine the boundaries of the project. Was this also intentional? The map needs to be marked-up, clearly highlighted and then resent to the property owners so that we can clearly recognize the impacted area. Also, some detail should be noted on the map as to what is planned as far as buildings, etc. and their planned locations. Again, that would be helpful to the home owners who are going to be impacted by the LAX project.

I received a letter from LAX Environmental & Land Use Planning. As part of their letter they state that "Implementation of the proposed Project is expected to result in significant and unavoidable impacts related to air quality, traffic, and construction noise." However, they give no indication as to what actions are going to be implemented to minimize the effects on the home owners in the area. Is there such a plan? For example, with the work currently going-on at LAX our area is quite dirty with the dust flying in the air so that you can't maintain a clean car or open a window because of the daily pollutants in the air. We do not have a street cleaner on the South side of Manchester Blvd. so the streets are never cleaned. It will only get worse with LAX future construction. Is anything going to be done about it? (Note: The streets on the North side of Manchester Blvd. have always had a scheduled street cleaning schedule even though they are further away from LAX.)

Hopefully, you will take action to implement the above recommendations to make it easier for the home owners to attend the meeting rather than business as usual by City officials. Your response will be appreciated.

Thanks,  
Earl Smitley



Karen Hoo <karen.hoo@lacity.org>  
To: Earl <earlsmitley7@msn.com>  
Cc: "VALDIVIA, MARIANA" <MVALDIVIA@lawa.org>

Thu, Mar 5, 2015 at 4:15 PM

Thank you for your e-mail. I understand that having a public hearing in City Hall may be inconvenient for some residents, however, as a matter of practice, holding hearings at City Hall saves City resources by way of securing off-site rentals, ensuring ADA accessibility, providing adequate security and public safety, and providing a central location for Citywide residents interested in the project to attend.

If you are not able to attend, you are welcome to provide written correspondence as all e-mails and letters are included in the record, and given equal weight to public comment, for consideration by the decision-maker.

LAWA has set up a website explaining their proposal and all documents related to the project can be viewed there. It can be accessed at: <http://www.lawa.org/GDZ/projectDocuments.aspx>

The Design Guidelines and Standards proposed for the project area can be accessed at: [http://www.lawa.org/GDZ/pdf/LAX%20Design%20Guidelines%20and%20Standards%203\\_2\\_15.pdf](http://www.lawa.org/GDZ/pdf/LAX%20Design%20Guidelines%20and%20Standards%203_2_15.pdf) There you can find more detailed maps and details on what they are proposing.

Copies of the Draft EIR and Final EIR that LAWA prepared analyzing the proposed project can also be found on their website at <http://www.lawa.org/GDZ/projectDocuments.aspx> The Draft and Final EIR explains why significant and unavoidable impacts are expected and what mitigation measures are proposed.

[Quoted text hidden]

—  
Karen Hoo  
Los Angeles City Planning Department  
EIR Unit, Mail Stop 395  
200 North Spring Street, Suite 750  
Los Angeles, CA 90012  
(213) 978-1331

Earl <earlsmitley7@msn.com>  
To: Karen Hoo <karen.hoo@lacity.org>

Thu, Mar 12, 2015 at 11:09 PM

Thank you for responding and identifying websites for additional information. However, I was disappointed, but not surprised, in the response addressing where/why the meeting is to be held at City Hall.

The people of Westchester and Playa Del Rey, living adjacent to LAX, are the ones being impacted by the airport development activities and they should be given primary consideration as to convenience for the meeting. Your argument "... that having a public hearing in City Hall may be inconvenient for some residents, however, as a matter of practice, holding hearings at City Hall saves City resources by way of securing off-site rentals, ensuring ADA accessibility, providing adequate security and public safety, and providing a central location for Citywide residents interested in the project to attend" is artificial and is simply a "business as usual" attitude.

There have been meetings in past years in the local area regarding LAX with city officials and the rationale stated in your response didn't seem to be an issue then and shouldn't be one now. If the city really wants to have participation by the local residents to hear their issues or questions, then the meeting needs to be held locally and not for the convenience of the members of the Department of City Planning or a few citywide residents who are not impacted by the development but simply want to know about the project. Again, priority should be given to the local residents who are going to be impacted and the meeting should be held locally. The Emerson Adult Center located on 88th Street and Emerson Avenue is a logical location for such a meeting. Other government meetings with Westchester/Playa Del Rey residents have been held in the in the past in the



school's auditorium (which is owned by L.A. City and therefore would be at no cost to the City) and affords ADA accessibility. And, parking is available at no cost to the attendees.

As I had stated in my original note to you, I am of the opinion that it is the strategy of the Department Of City Planning or other City officials to minimize attendance by the affected residences by scheduling the meeting at City Hall in the morning hours rather than in the Westchester area and in the evening to accommodate local residents so that they could participate in the discussions. And, this view is shared by many Westchester residents.

Thanks,  
Earl Smitley  
[Quoted text hidden]

---

**Karen Hoo** <karen.hoo@lacity.org>  
To: earlsmitley7@msn.com

Thu, Mar 12, 2015 at 11:10 PM

Thank you for your email. I am out of the office and will return on Wednesday, March 18. If the matter is urgent, may I suggest contacting Darlene Navarrete at (213) 978-1332 and they can route your request to the appropriate staff member.

[Quoted text hidden]





William Barry  
Post Office Box 91015  
Los Angeles, CA 90009  
310-621-9361

March 3, 2015

RECEIVED  
CITY OF LOS ANGELES

MAR 12 2015

ENVIRONMENTAL  
UNIT

**RETURN RECEIPT REQUESTED**

L.A. City Planning Department  
Major Projects Section  
200 North Spring Street, Rm. 750  
Los Angeles, CA 90012  
(Attn: Karen Hoo)

Re: LAX Northside Plan  
Case No.: CPC-2014-437-SP, VTT-72148

Dear Hearing Officer:

This letter is submitted in connection with the hearing scheduled for March 26, 2014. I ask that it be made a part of the record. I will be travelling out of state on March 26th, and cannot attend the hearing in person.

If the Northside Plan is approved, I request that it include the following:

1. On-site parking sufficient for 110% of all employees, workers or visitors to the businesses and other developments that are part of the Plan.
2. That all employees, workers, vendors, contractors or visitors of the Plan area be prohibited from parking in, or passing through the residential area by vehicle or on foot.
3. That LAX and other all users of the Plan area be required to notify all employees in writing once a year that the employees are banned from parking in, or passing through the residential area by vehicle or on foot.
4. That there be no new vehicular or pedestrian access to the residential area from the North side of the Plan area. Hastings Avenue, Stanmoor Drive and Rayford Drive should remain blocked to vehicles and pedestrians.
5. That the MTA provide direct bus service on Westchester Parkway for the Plan area.
6. That an 8' barrier wall be built along the entire northern edge of the Plan area.

The reason for these requests is simple. Additional cars in the residential area are a major concern. The site abuts a mature residential area, whose streets are already crowded with parked cars on both sides of the street, especially 91st and 90th Streets. Cars bringing students to St. Bernard's High School and Westchester High School are a particular problem the entire length of

L.A. City Planning Department  
Major Projects Section  
200 North Spring Street, Rm. 750  
Los Angeles, CA 90012  
(Attn: Karen Hoo)  
March 3, 2015  
Page 2

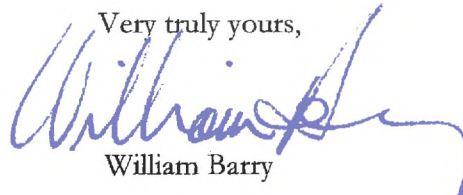
91st Street. The streets are hilly, which tends to limit visibility. The entrance to 91st Street at Loyola Blvd. is congested and dangerous enough as it is.

In addition, pedestrians from the Plan area are a concern because relatively few of the homes are occupied during the day. Crime would be a concern if pedestrian traffic were to increase. The proximity of major East-West and North-South highways tends to make us more vulnerable to opportunistic crimes.

The restricted access along the northern edge of the Plan area should not make emergency services more difficult. The LA Fire Station on Westchester Parkway has immediate access to the Plan area from Westchester Parkway. Its emergency vehicles do not need to go through the residential area. Emergency vehicles from other fire stations can access the Plan area via Pershing Avenue, Falmouth Avenue or Westchester Parkway, as they can at present. Of course, the emergency vehicles would still access the residential area as they have been doing since the North-South access from the Plan area was restricted 30 years ago.

Thank you for your consideration.

Very truly yours,

A handwritten signature in blue ink, appearing to read 'William Barry', with a stylized flourish extending to the right.

William Barry



Adam and Katie Hettinger  
8180 Manitoba St. APT 234  
Playa Del Rey, CA 90293

RECEIVED  
CITY OF LOS ANGELES

March 14, 2015

APR 03 2015

Los Angeles City Planning Department  
Major Projects Section  
200 N. Spring Street, Room 750  
Los Angeles, CA 90012  
Attention: Karen Hoo

ENVIRONMENTAL  
UNIT

**Project Case:** LAX Northside Plan  
**Plan Area:** Los Angeles International Airport  
**Case No:** CPC-2014-437-SP, VTT-72148

Dear Ms. Hoo:

We are current residents residing at 8180 Manitoba St. in Playa Del Rey who will be impacted by the proposed LAX Northside Plan. Our residence directly borders Area 1 as outlined in the Northside Design Guidelines and Standards. Our primary concerns are related to the potential impact to property values and the preservation of the open space to the west as a quiet and peaceful space for residents to enjoy. Our specific concerns are referenced below:

- Preserve existing lines of sight to the ocean by ensuring that proposed foliage and structures do not obstruct existing views, not only upon project completion, but with consideration for future growth of foliage and development in Area 1 (Figure 1).
- Ensure that foliage and structures along the proposed buffer area to not impede existing lines of sight to the ocean (Figure 2).
- Design of the northwest portion of Area 1 as primarily an open space (open space, rest area, picnic area) that is conducive to a quiet and low foot traffic area. The development of game fields, dog parks, playground equipment, etc. would detract from the existing peace that residents in this area enjoy and should be considered only in the southeast portion of Area 1 bordering Westchester Parkway (Figure 3).

We are generally pleased with the proposed plans for the LAX Northside Plan as a compromise to preserve and improve the existing space where possible. We appreciate your consideration of our specific concerns when addressing the landscape design of the northwest portion of Area 1.

Sincerely,

Adam and Katie Hettinger



Figure 1



Figure 2



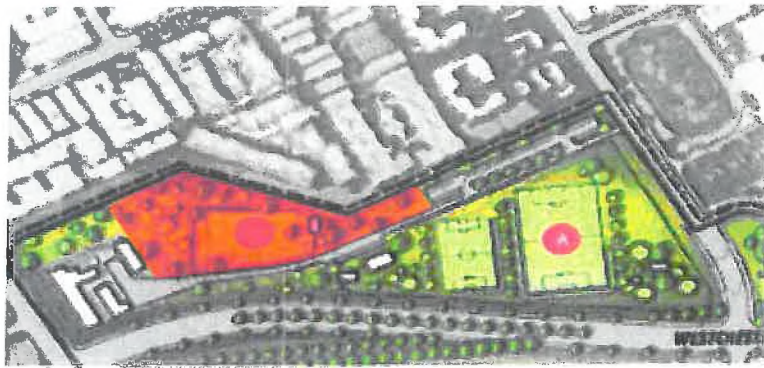


Figure 3





Karen Earl  
Cross Creek Village Condominium  
7765 W. 91<sup>ST</sup> ST Unit A3117  
Playa Del Rey, CA 90293

RECEIVED  
CITY OF LOS ANGELES

MAR 25 2015

ENVIRONMENTAL  
UNIT

VIA FIRST CLASS MAIL

March 20, 2015

Attn: Kathryn Pantoja  
Environmental Affairs Officer  
Los Angeles World Airports  
1 World Way  
Los Angeles, California 90045-5803

✓ Attn: Karen Hoo  
Los Angeles City Planning Department  
Major Projects Section  
200 N. Spring Street, Room 750  
Los Angeles, California 90012

Re: LAX Northside Plan Update

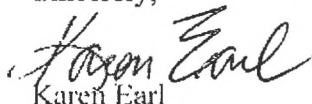
Dear Kathryn and Karen,

I live in the Cross Creek Village condo complex (address above), and complained about the level of noise per the attached letter. Please note – since there was no way for me to submit a complaint without picking a time, I had picked a random time. **The aircraft noise is actually loud ALL the time.** It was not this level of loudness when I moved to this location in 2010. **The noise has recently increased to unacceptable levels at ALL times.** I, along with all local residents, are very much against any expansion leading to even higher noise levels that affect our quality of life.

I am unable to attend the public hearing on Thursday, March 26<sup>th</sup> because I have to work that day. It's my understanding that LAX will expand regardless to local residents - so the larger airplanes will take off even closer to where we sleep at night. The noise is already coming loud and clear through our double-pane windows. Unfortunately, I am unable to move from this location.

What I want to know is – **will LAX be installing TRIPLE-PANE windows** at Cross Creek Village? Please let me know, and also kindly submit this letter for the public hearing. Thank you.

Sincerely,

  
Karen Earl

February 13, 2015

Mrs. Karen Earl  
7765 W 91st St  
Los Angeles, CA 90293

Dear Mrs. Earl:

Thank you for contacting Los Angeles International Airport (LAX) recently regarding aircraft noise. As part of our program of examining aircraft operations that result in community noise complaints, we have investigated the noise disturbance you reported.

The details of your complaint were as follows:

Incident date and time:	12/29/2014 1:15 AM
Disturbance type:	Loud noise
Complaint description:	The airplane engines sounds like a thunder-storm and keeps me from falling asleep. And this noise is coming through the double-pane windows we have installed in our Cross-Creek Village condo on 91st Street. Will LAX be installing triple-pane windows in our complex due to the expansion and noise-increase?

Our investigation shows that on the reported night, an Air New Zealand B777 aircraft was scheduled to depart at 10 p.m., but had to be delayed due to a fuel pump issue that was not fixed and ready for testing until 11 p.m. Consequently, an extended high power engine run-up had to be conducted for mechanics to determine if it was safe for flight. This engine run-up at the Qantas blast fence on the north side of the airfield was highly unusual in its duration.

Investigations are currently limited to five complaints (one event of disturbance per complaint) per individual per month. The Los Angeles World Airports' web site (<http://www.lawa.org>) has the Internet Flight Tracking System that can display almost live radar flight tracks (21-minute delay) or replay flight tracks from anytime during the previous 3-month period. Therefore, if you would like to see the graphical depiction of the aircraft activity you reported, please feel free to access this System and replay the data for that specific day and time. LAWA website also provides the monthly noise complaint summary report, under the LAX Noise Management section. If you do not have access to this website and would like to receive a copy of the report by mail, please contact Environmental and Land Use Planning Division, Noise Management at (424) 646-6500.

Los Angeles World Airports (LAWA) staff is committed to operating the airport as quietly as possible consistent with safety, and we place great emphasis on reducing noise from aircraft using the airport. To that end we have implemented various noise abatement procedures such as described below.

LAX

LA/Ontario

Van Nuys

City of Los Angeles

Eric Garcetti  
Mayor

Board of Airport  
Commissioners

Deputy Mayor  
for the City of Los Angeles

Deputy Mayor  
for the City of Los Angeles

Deputy Mayor  
for the City of Los Angeles

Deputy Mayor  
for the City of Los Angeles

Deputy Mayor  
for the City of Los Angeles

Deputy Mayor  
for the City of Los Angeles

The LAX Over-Ocean Operations (OOO) procedure, which is in effect from midnight to 6:30 AM, direct all aircraft traffic to fly over the ocean when arriving into and departing out of LAX, weather and safety conditions permitting. The run-up of aircraft engines for maintenance is prohibited between the hours from 11:00 PM until 6:00 AM. The preferential runway use procedures maximize the use of the inner runways for jet departures. In the evening/early morning hours between 10:00 PM until 7:00 AM, the preferential runway use procedures maximize the use of the inner runways for both arrivals and departures.

Other noise abatement programs seek and receive voluntary compliance. LAWA monitors various in-flight procedures such as the "early turn" and the "loop departure" to improve the operators' awareness and compliance with noise abatement procedures. Helicopter operators are advised to minimize their impact on nearby residential areas by using established routes and altitudes to approach and depart LAX.

LAWA monitors all departures and arrivals through the use of FAA radar flight tracks. When an aircraft deviates from a noise abatement procedure, an appropriate notification is triggered. However, airports do not have the authority to regulate aircraft in flight. FAA Air Traffic Controllers, along with the pilots in command of their respective aircraft, have ultimate control of aircraft operations, including the direction, speed, and altitude of aircraft for both arrivals and departures.

Thank you again for voicing your concerns.

Sincerely,

A handwritten signature in cursive script that reads "Kathryn Pantoja".

Kathryn Pantoja  
Environmental Affairs Officer

KP:LRS:md





Jordan Steinberg  
8174 Manitoba Street  
Unit # 5  
Playa Del Rey, CA 90293

RECEIVED  
CITY OF LOS ANGELES

MAR 27 2015

ENVIRONMENTAL  
UNIT

March 24, 2015

Attn: Karen Hoo  
Los Angeles City Planning Department  
Major Projects Section  
200 N. Spring Street  
Room 750  
Los Angeles, CA 90012

Re: Case No.:CPC-2014-437-SP, VTT-72148

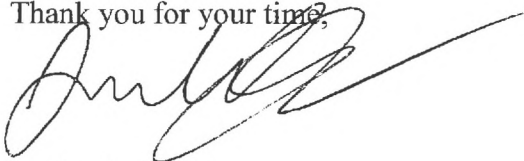
Ms. Hoo:

I have lived, with my family in Playa Del Rey for almost a year now. We thoroughly enjoy living in this small town in a big city. It is generally quiet, except for the planes from the airport. The crime rate is also generally low along with the traffic and very small amounts of congestion.

My family and I are against the proposed development under review of the north side of LAX (reference the case number listed above). We think that this development would increase the amount of traffic, noise, and congestion to Playa Del Rey and the surrounding areas. With an increase of traffic generally comes more noise, higher crime rates, and more pollution. In my opinion the amount of pollution that comes from LAX is quite high. Developing this area will only increase the amount of pollution in a location where (I'm guessing) air pollution is generally at an elevated rate already. I have seen the landscape that LAWA has planted in the proposed area and think that more greenspace, not development, should exist in that area. Finally, our little town of Playa Del Rey would no longer be the sleepy town it has become. It would most likely become a large, potentially crowded and heavily polluted city.

I am also concerned about the pollution because we have a two year-old son. This is why I am voicing my opinion in protest of the LAX Northside Plan.

Thank you for your time,



Jordan Steinberg







**Karen Dial**  
H.B. Drollinger Co.  
President

**John Ruhlen**  
Ruhlen & Associates  
Secretary

**Miki Payne**  
H.B. Drollinger Co.  
Treasurer

**William F. Allen**  
HFH Ltd.

**Jack Davis**  
Coldwell Banker Realtors

**Sander de Wildt**  
CBRE

**Heather Lemmon**  
Westbluff Realty  
and Property Management

**Donald R. Duckworth**  
Executive Director

March 25, 2015

City of Los Angeles  
Planning Commission  
200 N. Spring Street, Room 1050  
Los Angeles, Ca 90012

Attn: Karen Hoo

Re: LAX Northside Landuse Plan Update CPC-2014-437-SP, VTT-72148

As Executive Director of the Westchester Town Center Business Improvement District, I am writing to encourage the Planning Commission to approve and move forward with the LAX Northside project. The WTC BID Board of Directors has unanimously and enthusiastically endorsed this plan on a number of occasions.

Speaking for the hundreds of property and business owners in Westchester Town Center, which is immediately adjacent to the subject property, any new retail and office development would create a profound economic boon for our community. Moreover, this is a plan that was crafted the "right way," with extensive community involvement and participation.

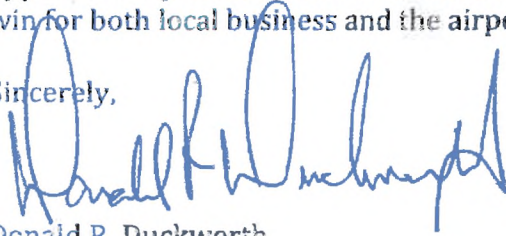
Los Angeles World Airport's plan for retail, restaurant, hotel and creative office uses has been designed to fit well within the fabric of what already exists. In particular, the mixed-use, commercial district at the eastern edge of the project will have a very positive impact on the existing commercial area along Sepulveda Boulevard. We see this as a symbiotic relationship that will allow existing businesses to thrive.

We are also excited about the proposal for the "paseo" component that will provide a convenient pedestrian and bicycle link between Westchester Town Center and the proposed research and development office uses. Creating a link that will allow those employees to shop and dine without having to get in their cars will not only have a positive impact on local traffic but will entice employees to shop locally.

In addition, by reducing the overall square footage and the number of vehicle trips that will be created by the project, LAWA has developed a plan that will not significantly impact our roadways.

We were pleased that the LAWA included us in the process, asked for the opinions of business people as it put together the plan before you, and provided on-going communications about it. We hope you will approve this plan and allow it to move forward because it represents a win for both local business and the airport.

Sincerely,

A handwritten signature in blue ink, appearing to read "Donald R. Duckworth". The signature is fluid and cursive, with a large initial "D" and "R".

Donald R. Duckworth,  
Executive Director

C: Westchester Town Center BID Board of Directors



## Streetscape Improvement Association

Beautify and improve local streetscapes, resulting in a better environment  
for both the commercial and residential areas of Westchester

March 25, 2015

Planning Commission  
City of Los Angeles  
200 N. Spring Street, Rm 1050  
Los Angeles, CA 90012

RE: Northside Landuse Plan Update Support

This letter is written to transmit the Westchester Streetscape Improvement Association's formal endorsement of the proposed Northside Landuse Plan Update as presented for Planning Commission approval.

The Board is a non-profit community beautification and streetscape improvement organization active in the Westchester area. WSIA has received numerous presentations from LAWA staff and others as this Plan has evolved and many of its members have participated directly in its multi-year preparation. The extensive out-reach and stakeholder involvement conducted in conjunction with the Plan's preparation has been remarkable and inspired the faith of the Board members in the Plan and LAWA's staff that prepared it.

The Board of Director's of the WSIA strongly supports the proposed Northside Landuse Plan Update and encourages its implementation without delay so that the local community can realize its benefits.

As we may assist further in any way, please contact me.

Sincerely,

John Ruhlen  
President

Westchester Streetscape Improvement Association

8726 S. Sepulveda Blvd. Suite D, #1621 - Westchester, CA 90045 - Phone: (310) 225-7630 - Fax: (310) 645-9820  
info@WestchesterStreetscape.org - www.WestchesterStreetscape.org - EIN 90-0080493





March 26, 2015

RECEIVED  
CITY OF LOS ANGELES

APR 14 2015

Karen Hoo  
L.A. City Planning Department  
Major Projects Section  
200 North Spring Street, Rm. 750  
Los Angeles, CA 90012

ENVIRONMENTAL  
UNIT

Re: LAX Northside Plan  
Case No.: CPC-2014-437-SP, VTT-72148

Dear Hearing Officer:

As a resident and home owner at 7348 W 89th in Westchester 90045, I'm writing to express my concerns about the implementation of LAX North Side Development Plan. I would have appeared in person for the hearing on March 26th if it weren't for a work conflict.

In July of 2014 we purchased our home at 7348 W. 89th. The proximity of the airport was not a concern for us as we found the neighborhood to be quiet, safe and free of both car and foot traffic. This neighborhood allowed our 2 and 4 year old children to have a safe place to play free of worry of a heavy amount of foot or auto traffic.

The North Side Development Plan gives me concern for a number of reasons and I want to go on record with these concerns and hope that the implementation will take these into account for me and all the residents of Westchester West. here are the key concerns.

1. The plan should include on-site parking sufficient for 110% of all employees, workers or visitors to the businesses. The neighborhood cannot and should not have to bear the weight of any on-street overflow parking.
2. All employees, workers, vendors, contractors or visitors of the Plan area be prohibited from parking in, or passing through the residential area by vehicle or on foot.
3. That LAX and other all users of the Plan area be required to notify all employees in writing once a year that the employees are banned from parking in, or passing through the residential area by vehicle or on foot. This must be worked into company policies and be revisited routinely.
4. That there be no new vehicular or pedestrian access to the residential area from the North side of the Plan area. Hastings Avenue, Stanmoor Drive and Rayford Drive should remain blocked to vehicles and pedestrians.

5. That the MTA provide direct bus service on Westchester Parkway for the Plan area.
6. That an 8' barrier wall be built along the entire northern edge of the Plan area.

The North Side development should not impact the quality of life for any of the residents of Westchester West, many of whom have been living there for 20, 30 and 40 years. Adhering to these guidelines will ensure that the Westchester West community and the North Side Development can co-exist without detrimental impact. We must also always keep in mind that our children and the schools of the neighborhood should be the utmost priority in assessing any changes to the neighborhood and surroundings. We must always ask... "Is this good for the children"? No amount of increase in pass through traffic on foot or in car can be allowed to happen near our schools and the the very small neighborhood where they live and play. We cannot allow the quality of life of our children to suffer for the benefit of land developers. They are the future and foundation and they need to be protected.

Sincerely,



Ezra Burke  
7348 W. 89th St.  
Los Angeles, CA 90045  
310-962-4079