

# Notice of Public Hearing

Aviso de Audiencia Pública • 공청회통지  
Abiso ng Pagdinig sa Publiko • 公開聽證會通知  
Հանրային խումբերի մասին ծանուցագիր

311

Traducción  
번역 • 翻译  
Pagsasalin  
Թարգմանություն



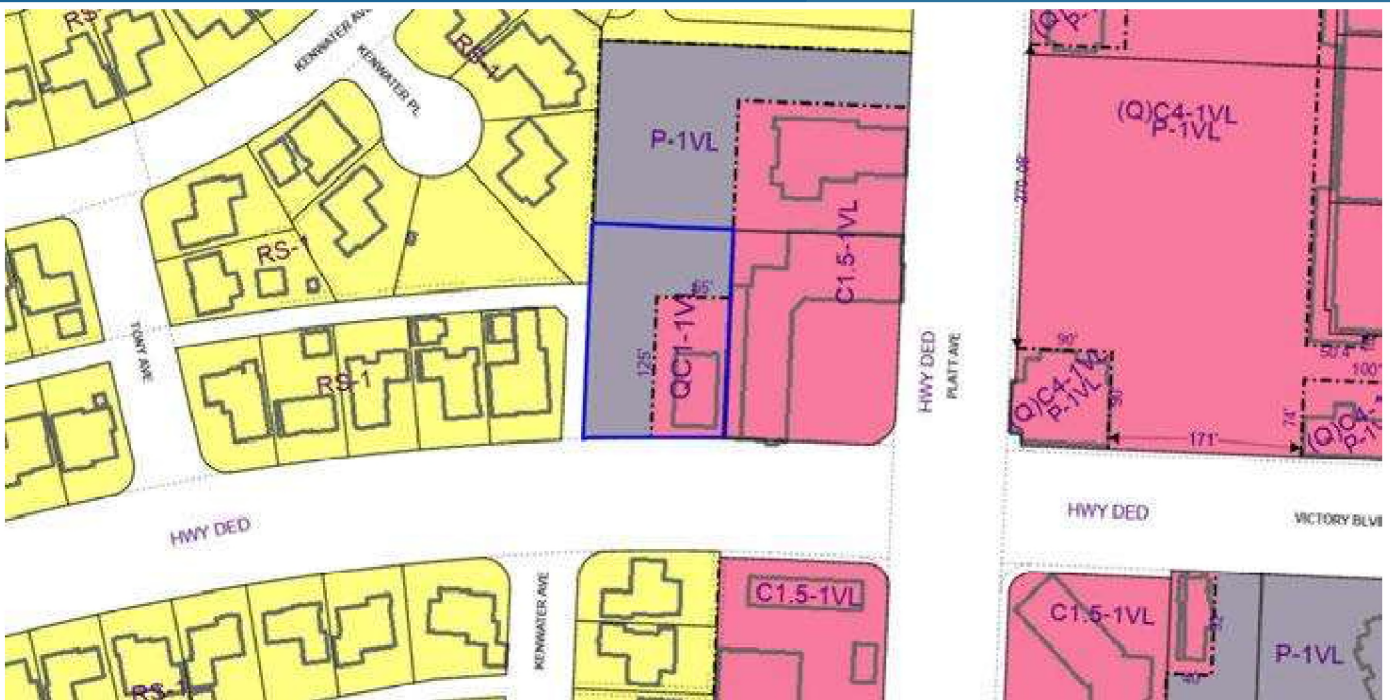
**February 12, 2026 after 4:30 p.m.**

South Valley Area Planning Commission

Marvin Braude San Fernando Valley  
6262 Van Nuys Boulevard, Room 18  
Van Nuys, CA 91401

This meeting may be available virtually, in a hybrid format. Please check the meeting agenda approximately 72 hours before the meeting for additional information.

Please see [planning4la.org/hearings](https://planning4la.org/hearings) for the meeting agenda.



## Project Address

Sitio de Proyecto  
프로젝트 주소 • 項目地址  
Address ng Proyecto  
ծրագրի Հասցե

23717 West Victory Boulevard,  
Los Angeles, CA 91307

## Proposed Project

Proyecto Propuesto  
프로젝트 제안 • 擬議項目  
Iminungkahing Proyecto  
Առաջարկվող ծրագիր

The proposed project involves a Conditional Use Permit and a Zone Boundary Adjustment to allow the construction, use, and maintenance of a new 1,479 square-foot Starbucks Drive-Through Coffee Shop with a trash enclosure and walk up/pick up window. The project proposes an adjustment to the C1 zone boundary so that the entire building, signage, patio, trash enclosure, and drive-through lanes are all within the boundaries of the existing commercial zone.

## Actions Requested

Acciones solicitadas • 요청 된 작업 • 所要求的事项 • Humiling ng Mga Pagkilos • Հայցվող գործողությունները

Appeal:

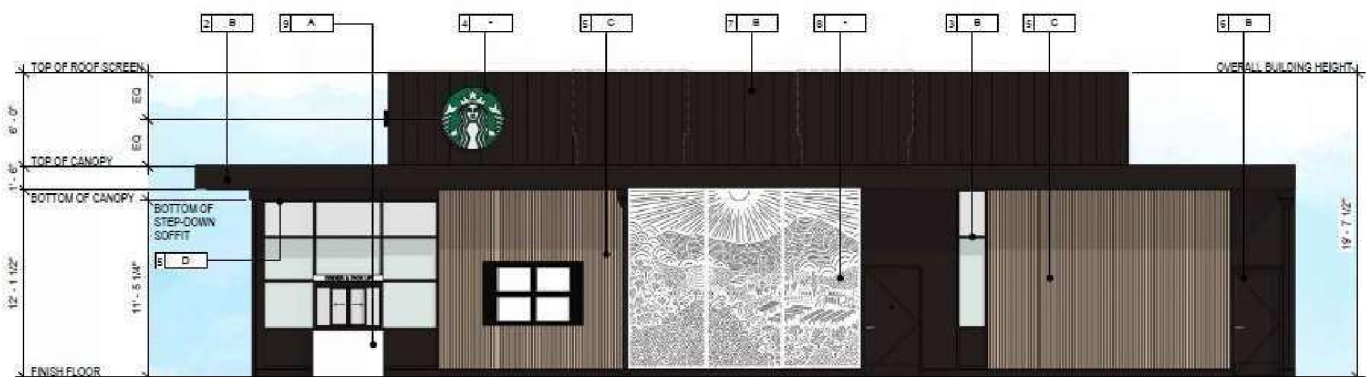
1. An Exemption from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Sections 15301 (Class 1) and 15305 (Class 5) and that there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies; and
2. Pursuant to Los Angeles Municipal Code (LAMC) Sections 12.24-W,17 and 13B.2.2, a Conditional Use to permit a drive-through coffee shop to be located adjacent to a residential zone.
3. Pursuant to LAMC Sections 12.24-W,27 and 13B.2.2, a Conditional Use to allow the following deviations from LAMC Section 12.22-A,23 in conjunction with a Commercial Corner Development in the QC1-1VL and P-1VL Zones:
  - a. Less than 50 percent transparent windows on exterior walls/doors fronting adjacent streets;
  - b. Less than five-feet width of landscaping planter area on the street frontage and the perimeter of the parking area;
  - c. The continued use of an existing on-site pole sign; and
  - d. Hours of operation exceeding the maximum permitted hours of 7:00 a.m. to 11:00 p.m., daily; and
4. Pursuant to LAMC Section 12.30-K, a Zone Boundary Adjustment to allow maintenance of necessary elements of the proposed coffee shop including the building, drive-through lane, trash enclosure, signage and path of travel to be contained within the existing C1 Zone.

### WINDOW TRANSPARENCY ANALYSIS

OVERALL ELEVATION SF: +/- 1,214 SF

TRANSPARENT WINDOW SF: +/- 65 SF

65 SF / 1,214 SF = 5% WINDOW TRANSPARENCY



1 SOUTH ELEVATION  
SCALE: 3/16" = 1'-0"

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## Case Information

Información del caso • 케이스 정보 • 案例資訊 • Impormasyon sa Kaso • Տեղեկություններ գործի վերաբերյալ

### Case Number(s):

ZA-2024-5222-CU2-ZBA-1A

### Environmental Case Number(s):

ENV-2024-5223-CE

### Related Case Number(s):

ZA-2024-5222-CU2-ZBA

### Overlay(s):

N/A

### Zone:

**P-1VL and QC1-1VL**

### Community Plan Area:

Canoga Park - Winnetka - Woodland Hills - West Hills

### Land Use Designation:

**Community Commercial**

### Assigned Staff Contact Information:

Stephanie Escobar, City Planning Associate  
Stephanie.Escobar@lacity.org  
213-978-1492  
200 North Spring Street  
Los Angeles, CA 90012 Room 763

### Council District:

12 - John Lee

### Applicant:

Bennett Lefebvre  
Starbucks Coffee Company  
2401 Utah Avenue South  
Seattle, WA 98134

### Appellant:

West Valley Alliance for Optimal Living  
7507 Winnetka Avenue  
Canoga Park, CA 91306

### Applicant Representative:

Ryan Abraham  
Kaidence Group  
5070 Noth 40th Street, Unit 210  
Phoenix, AZ 85018

### Appellant Representative:

Jamie T. Hall  
Channel Law Group, LLP  
8383 Wilshire Boulevard, Suite 750

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## Who's Receiving This Notice

Quién recibe este aviso • 본통지를받은사람들 • 誰會收到此通知

Sino ang Tumatanggap ng Paunawang Ito • Սույն ծանուցագիրը ստացող կողմը

You are receiving this notice either because you live on or own property that is on a site within 300 feet of where a project application has been filed with the Department of City Planning, are the Certified Neighborhood Council for the subject property, or because you requested to be added to the interested parties list, or you are otherwise legally required to receive this notice. You are invited to attend this hearing to learn more about the proposed project and offer feedback. If unable to attend, you may contact the planner to provide written comment, obtain additional information, and/or review the project file.

**General Information** - Visit our website at [planning4la.org/hearings](http://planning4la.org/hearings) for general information about public hearings and the exhaustion of administrative remedies.

**File Review** - The complete file will be available for public inspection by appointment only. Please email the staff identified on the front page, at least three (3) days in advance, to arrange for an appointment. Files are not available for review the day of or day before the hearing.

**Agendas And Reports** - Commission Agendas are accessible online at [planning4la.org/hearings](http://planning4la.org/hearings). Appeal Recommendation Reports are available on-line seven (7) days prior to the Commission meeting and are hyperlinked to the case numbers on the agenda. **Please note that Appeal Recommendation Reports are not prepared for appeals related to Zoning Administrator decisions.**

Be advised that the Commission may RECONSIDER and alter its action taken on items listed on the meeting agenda at any time during this meeting or during the next regular meeting, in accordance with the Commission Policies and Procedures and provided that the Commission retains jurisdiction over the case. **If a Commission meeting is cancelled or adjourned due to lack of quorum, all remaining agenda items shall be continued to the next regular meeting or beyond, as long as the continuance is within the legal time limits of the case or cases.**

**Testimony And Correspondence** - Your attendance is optional; oral testimony can only be given at the Commission meeting and may be limited due to time constraints. Written testimony or evidentiary documentation may be submitted prior to, or at the meeting in accordance to the Commission's submittal requirements. Commissions function in a quasi-judicial capacity and therefore, cannot be contacted directly. Any materials submitted to the Commission become City property and will not be returned. This includes any correspondence or exhibits used as part of your testimony.

**Requirements For Submission Of Materials** - Written materials may be submitted prior to or at the meeting in accordance with the submittal requirements below. The case number must be written on all communications, plans and exhibits.

- **Regular Submissions** – Written materials not limited as to volume must be received by the Commission Executive Assistant no later than by end of business day Monday of the week prior to the week of the Commission meeting. Materials must be delivered electronically to the staff and commission email identified on the front of this page.
- **Secondary Submissions** - All written materials in response to an Appeal Recommendation Report and/or additional comments must be submitted no later than **48 hours before to the Commission meeting (for Central, South LA and Harbor APCs, materials must be received no later than by 3:00 p.m., Thursday of the week prior to the Commission Meeting)**. Submissions, including exhibits, shall not exceed ten (10) pages and must be submitted electronically to the Commission identified on the front of this notice.
- **Day of Hearing Submissions** - Submissions less than 48 hours prior to, and including the day of the Commission meeting, must not exceed two (2) written pages, including exhibits. Photographs do not count toward the page limitation. These must be submitted electronically to the Commission email identified on the front of this page.
- **Non-Complying Submissions** - Submissions that do not comply with these rules will be stamped "File Copy. Non-complying Submission". Non-complying submissions will be placed into the official case file, but they will not be delivered to, or considered by the Commission. The Commission Rules and Operating Procedures are available online at [planning4la.org/hearings](http://planning4la.org/hearings) and selecting the specific Commission.

**Exhaustion Of Administrative Remedies And Judicial Review** - If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agenzized here, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. If you seek judicial review of any decision of the City pursuant to California Code of Civil Procedure Section 1094.5, the petition for writ of mandate pursuant to that section must be filed no later than the 90th day following the date on which the City's decision became final pursuant to California Code of Civil Procedure Section 1094.6. There may be other time limits which also affect your ability to seek judicial review.

**Accommodations** - As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability. To request a reasonable accommodation, such as translation or interpretation, please contact the Commission Executive Assistant at (213)978-1299, the Commission Office Main Line at (213) 978-1300 or by email at [apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org) a minimum of 3 days (72 hours) prior to the public hearing. Be sure to identify the language you need English to be translated into and indicate if the request is for oral interpretation or written translation services. If translation of a written document is requested, please include the document to be translated as an attachment to your email.

# **MASTER APPEAL FORM**

**WITH ATTACHMENTS**

# APPLICATIONS



## APPEAL APPLICATION Instructions and Checklist

### PURPOSE

This application is for the appeal of Los Angeles Department of City Planning determinations, as authorized by the LAMC. For California Environmental Quality Act Appeals, use form [CP13-7840](#). For Building and Safety Appeals and Housing Department Appeals, use form [CP13-7854](#).

### RELATED CODE SECTION

Refer to the Letter of Determination (LOD) for the subject case to identify the applicable Los Angeles Municipal Code (LAMC) Section for the entitlement and the appeal procedures.

### APPELLATE BODY

**Check only one. If unsure of the Appellate Body, check with City Planning staff before submission.**

- Area Planning Commission (APC)     City Planning Commission (CPC)     City Council  
 Zoning Administrator (ZA)

### CASE INFORMATION

Case Number: ZA-2024-5222-CU2-ZBA

APN: 2033-010-043

Project Address: 23717 West Victory Boulevard

Final Date to Appeal: November 5, 2025

### APPELLANT

**Check all that apply.**

- Person, other than the Applicant, Owner or Operator claiming to be aggrieved  
 Representative     Property Owner     Applicant     Operator of the Use/Site

## APPELLANT INFORMATION

Appellant Name: West Valley Alliance for Optimal Living

Company/Organization: \_\_\_\_\_

Mailing Address: 7507 Winnetka Avenue

City: Canoga Park State: CA Zip Code: 91306

Telephone: 818-348-0229 E-mail: powcp@aol.com

Is the appeal being filed on your behalf or on behalf of another party, organization, or company?

Self  Other: \_\_\_\_\_

Is the appeal being filed to support the original applicant's position?  YES  NO

## REPRESENTATIVE / AGENT INFORMATION

Name: Jamie T. Hall

Company/Organization: Channel Law Group, LLP

Mailing Address: 8383 Wilshire Blvd., Suite 750

City: Beverly Hills State: CA Zip Code: 90211

Telephone: (310) 347-0050 E-mail: jamie.hall@channellawgroup.com

## JUSTIFICATION / REASON FOR APPEAL

Is the decision being appealed in its entirety or in part?  Entire  Part

Are specific Conditions of Approval being appealed?  YES  NO

If Yes, list the Condition Number(s) here: \_\_\_\_\_

On a separate sheet provide the following:

Reason(s) for the appeal

Specific points at issue

How you are aggrieved by the decision

## APPLICANT'S AFFIDAVIT

I certify that the statements contained in this application are complete and true.

**Appellant Signature:**  **Date:** November 5, 2025

## GENERAL NOTES

*A Certified Neighborhood Council (CNC) or a person identified as a member of a CNC or as representing the CNC may not file an appeal on behalf of the Neighborhood Council; persons affiliated with a CNC may only file as an individual on behalf of self.*

*The appellate body must act on the appeal within a time period specified in the LAMC Section(s) pertaining to the type of appeal being filed. Los Angeles City Planning will make its best efforts to have appeals scheduled prior to the appellate body's last day to act in order to provide due process to the appellant. If the appellate body is unable to come to a consensus or is unable to hear and consider the appeal prior to the last day to act, the appeal is automatically deemed denied, and the original decision will stand. The last day to act as defined in the LAMC may only be extended if formally agreed upon by the applicant.*

### THIS SECTION FOR CITY PLANNING STAFF USE ONLY

**Base Fee:** \$178

**Reviewed & Accepted by (DSC Planner):** Jason Chan

**Receipt No.:** 200341070390 **Date:** 11/5/25

Determination authority notified

Receipt Number

## GENERAL APPEAL FILING REQUIREMENTS

If dropping off an appeal at a Development Services Center (DSC), the following items are required. See also additional instructions for specific case types. To file online, visit our [Online Application System \(OAS\)](#).

## APPEAL DOCUMENTS

### 1. Hard Copy

Provide three sets (one original, two duplicates) of the listed documents for each appeal filed.

Appeal Application

Justification/Reason for Appeal

- Copy of Letter of Determination (LOD) for the decision being appealed

## 2. Electronic Copy

- Provide an electronic copy of the appeal documents on a USB flash drive. The following items must be saved as individual PDFs and labeled accordingly (e.g., “Appeal Form”, “Justification/Reason Statement”, or “Original Determination Letter”). No file should exceed 70 MB in size.

## 3. Appeal Fee

- Original Applicant.* The fee charged shall be in accordance with [LAMC Section 19.01 B.1\(a\) of Chapter 1](#) or [LAMC Section 15.1.1.F.1.a. \(Appeal Fees\) of Chapter 1A](#) as applicable, or a fee equal to 85% of the original base application fee. Provide a copy of the original application receipt(s) to calculate the fee.
- Aggrieved Party.* The fee charged shall be in accordance with [LAMC Section 19.01 B.1\(b\) of Chapter 1](#) or [LAMC Section 15.1.1.F.1.b. \(Appeal Fees\) of Chapter 1A](#) as applicable

## 4. Noticing Requirements (Applicant Appeals Only)

- Copy of Mailing Labels.* All appeals require noticing of the appeal hearing per the applicable LAMC Section(s). Original Applicants must provide noticing per the LAMC for all Applicant appeals. See the Mailing Procedures Instructions ([CP13-2074](#)) for applicable requirements.

## SPECIFIC CASE TYPES

### ADDITIONAL APPEAL FILING REQUIREMENTS AND / OR LIMITATIONS

## DENSITY BONUS (DB) / TRANSIT ORIENTED COMMUNITES (TOC)

Appeal procedures for DB/TOC cases are pursuant to [LAMC Section 13B.2.5. \(Director Determination\) of Chapter 1A](#) or [LAMC Section 13B.2.3. \(Class 3 Conditional Use\) of Chapter 1A](#) as applicable.

- Off-Menu Incentives or Waiver of Development Standards are not appealable.
- Appeals of On-Menu Density Bonus or Additional Incentives for TOC cases can only be filed by adjacent owners or tenants and is appealable to the City Planning Commission.

- Provide documentation confirming adjacent owner or tenant status is required (e.g., a lease agreement, rent receipt, utility bill, property tax bill, ZIMAS, driver's license, bill statement).

## WAIVER OF DEDICATION AND / OR IMPROVEMENT

Procedures for appeals of Waiver of Dedication and/or Improvements (WDIs) are pursuant to [LAMC Section 12.37 I of Chapter 1](#) or [LAMC Section 10.1.10. \(Waiver and Appeals\) of Chapter 1A](#) as applicable.

- WDIs for by-right projects can only be appealed by the Property Owner.
- If the WDI is part of a larger discretionary project, the applicant may appeal pursuant to the procedures which govern the main entitlement.

## [VESTING] TENTATIVE TRACT MAP

Procedures for appeals of [Vesting] Tentative Tract Maps are pursuant [LAMC Section 13B.7.3.G. of Chapter 1A](#).

- Appeals must be filed within 10 days of the date of the written determination of the decision-maker.

## NUISANCE ABATEMENT / REVOCATIONS

Appeal procedures for Nuisance Abatement/Revocations are pursuant to [LAMC Section 13B.6.2.G. of Chapter 1A](#). Nuisance Abatement/Revocations cases are only appealable to the City Council.

### Appeal Fee

- Applicant (Owner/Operator)*. The fee charged shall be in accordance with the [LAMC Section 19.01 B.1\(a\) of Chapter 1](#) or [LAMC Section 15.1.1.F.1.a. \(Appeal Fees\) of Chapter 1A](#) as applicable.

For appeals filed by the property owner and/or business owner/operator, or any individuals/agents/representatives/associates affiliated with the property and business, who files the appeal on behalf of the property owner and/or business owner/operator, appeal application fees listed under [LAMC Section 19.01 B.1\(a\) of Chapter 1](#) shall be paid, at the time the appeal application is submitted, or the appeal application will not be accepted.

- Aggrieved Party*. The fee charged shall be in accordance with the [LAMC Section 19.01 B.1\(b\) of Chapter 1](#) or [LAMC Section 15.1.1.F.1.b. \(Appeal Fees\) of Chapter 1A](#) as applicable.

# Channel Law Group, LLP

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8383 Wilshire Blvd.  
Suite 750  
Beverly Hills, CA 90211

Phone: (310) 347-0050  
Fax: (323) 723-3960  
www.channellawgroup.com

JULIAN K. QUATTLEBAUM, III  
JAMIE T. HALL \*  
CHARLES J. McLURKIN  
GREGORY T. WITTMANN

Writer's Direct Line: (310) 982-1760  
jamie.hall@channellawgroup.com

\*ALSO Admitted in Texas

November 5, 2025

## **VIA ELECTRONIC UPLOAD**

**Re: Justifications for Appeal of Case No. ZA-2024-5222-CU2-ZBA, Conditional Use Permit and Zone Boundary Adjustment, 23717 Victory Boulevard (Proposed Starbucks Drive-Through)**

Dear South Valley Area Planning Commission:

This firm represents West Valley Alliance for an Optimal Living (“Association” or “Appellant”), a not-for project unincorporated association. Appellant and its members have a direct and substantial beneficial interest in ensuring that the City complies with laws relating to environmental protection and orderly growth of the City.

This letter outlines the justifications for appeal of the Zoning Administrator's (“ZA”) approval of Case No. ZA-2024-5222-CU2-ZBA, which authorizes the construction, use, and maintenance of a new 1,479 square-foot Starbucks Drive-Through Coffee Shop at 23717 Victory Boulevard.

As explained below, the ZA erred in making the requisite findings for the entitlements, as the administrative record, including the concerns raised during the public hearing on September 6, 2025, contains substantial evidence demonstrating that the Project's significant features, size, and operation will be incompatible with, and detrimental to, the adjacent Low Residential (RS-1) properties and the surrounding neighborhood.

## **GROUND'S FOR APPEAL: THE ZA ERRED IN MAKING REQUIRED FINDINGS**

The Zoning Administrator failed to apply the correct legal standard and ignored substantial evidence from the public testimony, which directly undermines the required Conditional Use Permit (CUP) and Drive-Through Establishment findings (Letter of Determination (“LOD”), Pages 15-24).

**I. CONDITIONAL USE FINDING NO. 2 (Compatibility and Adverse Effects)**

The ZA found that the project's location, size, height, operations, and other significant features "will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare and safety." (LOD, Page 17).

This finding is not supported by substantial evidence and is directly contradicted by the following testimony presented at the public hearing conducted on September 6, 2025.

- **Traffic and Circulation Degradation:** Public testimony repeatedly highlighted the project's impact on an already burdened intersection. Mr. Jeff Bornstein of West Valley Alliance and other residents explicitly stated concerns regarding traffic being added by the project and the intersection being busy (LOD, Pages 14-15). The ZA attempts to mitigate this with a "Right Turn Only" egress condition (LOD, Page 19, 25), yet ignores the inherent high-volume, high-turnover nature of the proposed use which will inevitably lead to traffic spillover onto Victory Boulevard (LOD, Page 16, 18), especially when the 13-car queue is exceeded (LOD, Page 19). The testimony suggests this use is an inadequate fit for a site so constrained, contradicting the finding of no adverse effect on public safety and vehicular circulation.
- **Noise and Late Night Operation:** The proposed hours (closing 10:00 PM to 11:00 PM) are an extension of the allowable Commercial Corner Development hours (7:00 AM to 11:00 PM). Mr. Bornstein specifically noted that the proposed hours were "too late" given the proximity to homes to the west (LOD, Page 14). The ZA's reliance on a new 6-foot wall and Automatic Volume Control (AVC) for the speaker box as adequate mitigation (LOD, Page 19) is speculative and inadequate. The primary source of noise is not the speaker box, but the vehicular movement, car doors, and customer conversations from 4:30 AM until 11:00 PM, all occurring immediately adjacent to the RS-1 properties across the 20-foot alleyway. This proximity demonstrates a clear potential for significant disturbances, directly undermining the finding of compatibility.

**II. DRIVE-THROUGH ESTABLISHMENT FINDING NO. 4 (Noise Protection)**

The ZA found that "residential uses in the vicinity of a proposed drive-through fast-food establishment will be adequately protected from any significant noise resulting from outdoor speakers, autos, or other sources of noise associated with the lot." (LOD, Page 21).

The ZA's finding fails to address the unique constraints of this site and the nature of the use, contradicting the public record.

- **Buffer Inadequacy:** The ZA claims the residential uses are "buffered by a 20-foot alley and trees on walls" (LOD, Page 19). The presence of a 20-foot alley is not a *buffer* provided by the project; it is a pre-existing condition, and is, in fact, the closest point of separation between the commercial and residential zones. The new 6-foot wall and

existing parking asphalt (LOD, Page 21) are insufficient to shield residents from high-volume, late-night traffic and operation noise. As the adjacent residential uses are single-family homes (RS-1), the noise from idling cars, honking, loud music, and customer interactions occurring during the drive-through's long operational hours (4:30 AM to 11:00 PM) will be projected directly toward and over a standard 6-foot masonry wall.

- **Queueing Proximity:** The design places the drive-through lane, capable of holding 13 vehicles, immediately along the western (residential-adjacent) side of the property, retaining "existing parking areas on the western property line" (LOD, Page 21) due to utility/easement constraints. This design choice forces all associated noise, idling fumes, and light scatter (despite conditions for shielding) into the most sensitive area of the property, directly impacting the RS-1 homes. The finding that the residents are "adequately protected" is factually unsupported given the operational proximity.

### **III. DRIVE-THROUGH ESTABLISHMENT FINDING NO. 6 (Minimizing Disturbance and Enhancing Privacy)**

The ZA found that "trash storage, trash pickup hours, driveways, parking locations, screening walls, trees and landscaping are provided for and located so as to minimize disturbance to the occupants of nearby residential uses, and to enhance the privacy of those uses." (LOD, Page 23)

The ZA's conclusion that the design minimizes disturbance and enhances privacy is flawed:

- **Parking Lot/Drive-Through as "Buffer":** The ZA relies on the parking lot and drive-through lane itself being retained on the western property line (closest to the RS-1 homes) as a necessary condition due to easements (LOD, Page 23-24). This choice places high-activity, noise, and fume-generating vehicle movement directly next to residences, which is inherently the opposite of minimizing disturbance. The required new 6-foot wall only offers minimal ground-level visual screening; it does not eliminate the operational noise, light, or privacy intrusion caused by patrons in cars, who are elevated above the fence line.
- **Trash Location and Scheduling:** While a trash enclosure is proposed to be locked and comply with standards, the ZA's finding fails to address the trash pickup hours. Given the extended hours of operation (ending at 11:00 PM), trash pickup is likely to occur in the early morning hours, creating significant noise disturbance (slamming containers, beeping trucks) for the RS-1 residents, who are separated only by a 20-foot alley. This is a common point of contention, and the finding that the location minimizes disturbance is questionable without specific conditions prohibiting early morning trash service.

#### IV. **MINI-SHOPPING CENTER/COMMERCIAL CORNER FINDING NO. 7 (Traffic Hazard/Congestion)**

The ZA found that ingress/egress "will not create a traffic hazard or cause significant traffic congestion or disruption of vehicular circulation on adjacent streets." (LOD, Page 24). As explained below, this finding was in error.

- **Identified Hazards Require Mitigation:** This finding is undermined by the ZA's own imposed conditions. The ZA explicitly required the applicant to "coordinate with the Department of Transportation (LADOT) West Valley District Office to... install traffic measures to prohibit left turns from the eastbound lanes of Victory Boulevard" and also conditioned the driveway to be "Right Turn Only" egress (LOD, Page 25, Condition 27). Furthermore, the ZA conditioned the presence of an employee during peak times and when the queue exceeds 13 cars "to facilitate traffic movement" (LOD, Page 25, Condition 28). The necessity of mandating signage, traffic measures, and employee facilitation demonstrates that, *without* these measures, the project *would* create a traffic hazard or significant disruption. This places the burden of preventing a known hazard onto the applicant's operations, rather than designing it out of the site, which suggests the project's size and design are inherently incompatible with the intersection.
- **Failure to Trigger Traffic Assessment:** The ZA stated the project "does not meet the threshold to trigger a traffic assessment" (LOD, Page 25). Given the substantial public testimony on traffic, the site's proximity to a major intersection, and the acknowledged need for mandated traffic controls (Right-Turn Only, No Left Turn In, employee management), the ZA erred by not requiring a focused traffic analysis. The anecdotal evidence from the public hearing is substantial enough to warrant formal CEQA analysis on traffic, as the ZA's reliance on LADOT's high-level review, which in turn required heavy operational conditions, is insufficient to support the finding that no significant congestion will occur.

#### V. **Failure to Comply with California Environmental Quality Act**

The Zoning Administrator's determination (LOD, Page 1) that the project is exempt from CEQA pursuant to CEQA Guidelines Sections 15301 (Class 1) and 15305 (Class 5) is in error. The project is simply not eligible for a categorical exemption from CEQA. Further, the project is subject to the Significant Effect/Unusual Circumstances Exception of CEQA Guidelines Section 15300.2(c).

- **Inapplicability of Class 1 (Existing Facilities) and Class 5 (Minor Alterations):**

The Project involves the demolition of the vacant 2,135 square-foot Pizza Hut building and new construction of a 1,479 square-foot drive-through coffee shop, a complete change in use and character requiring a Conditional Use Permit (CUP) and a Zone Boundary Adjustment. This is not a "minor alteration" or "negligible expansion of use" as required by Class 1. Furthermore, the project seeks a CUP for a drive-through adjacent to residential zoning and four deviations from Commercial Corner Development standards

(LOD, Page 2). Granting a highly discretionary CUP, extended operating hours (4:30 AM), and numerous deviations that facilitate a high-intensity vehicular use adjacent to a residential zone is not a "minor alteration in land use limitations" as required by Class 5.

- **Significant Effect Due to Unusual Circumstances (CEQA Guidelines §15300.2(c)):**
  - **Unusual Circumstances:** The combination of the Project's specific characteristics, as detailed in the four preceding sections, constitutes unusual circumstances with a reasonable possibility of having a significant environmental effect. The site requires a Zone Boundary Adjustment, sits immediately adjacent to RS-1 single-family homes (buffered only by a 20-foot alley), and is proposing an inherently high-volume vehicular use (drive-through) with operating hours extending until 11:00 PM and starting at 4:30 AM. Therefore, there are unusual circumstances which preclude the usage of a categorical exemption.
  - **Significant Effect:** Further, the administrative record contains substantial evidence (public testimony, LADOT conditions, etc.) of potential significant effects relating to:
    - **Noise:** Constant vehicular noise, car doors, idling, and trash collection noise between 4:30 AM and 11:00 PM, impacting the closest adjacent residential properties (LOD, Page 14).
    - **Traffic/Safety:** The necessity of mandatory conditions for a "Right Turn Only" egress, prohibition of left turns into the site, and required employee queuing management (Condition 27, 28) demonstrates that the project's design would otherwise result in a significant traffic hazard and disruption.
    - **Air Quality:** The inherent queuing and idling of up to 13 cars placed immediately adjacent to residential uses (LOD, Page 13) introduces localized air quality impacts (fumes/toxics) that were not analyzed by the ZA.

The existence of these unusual circumstances and the demonstrated potential for significant impacts remove the Project's eligibility for a categorical exemption from CEQA and require the preparation of an Initial Study to fully assess the environmental effects.

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**CONCLUSION**

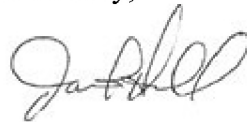
The Zoning Administrator's determination relies on insufficient and speculative operational conditions to mitigate inherent land use conflicts, noise, and traffic impacts on adjacent RS-1 residential properties. The public testimony provided substantial evidence that the proposed Starbucks drive-through, operating until 11:00 PM, is too intense and inappropriately located to be compatible with its immediate neighbors.

Appellant reserves the right to supplement these bases for appeal.

For the foregoing reasons, the appeal should be granted.

Thank you for your consideration of this matter.

Sincerely,

A handwritten signature in black ink, appearing to read "J. Hall", written in a cursive style.

Jamie T. Hall

**DETERMINATION  
LETTER**

OFFICE OF ZONING ADMINISTRATION  
200 N. SPRING STREET, ROOM 763  
LOS ANGELES, CA 90012-4801  
(213) 978-1318

ESTINEH MAILIAN  
CHIEF ZONING ADMINISTRATOR

ASSOCIATE ZONING ADMINISTRATORS

JACK CHIANG  
HENRY CHU  
TIM FARGO  
JONATHAN A. HERSHEY, AICP  
PHYLLIS NATHANSON  
CHARLES J. RAUSCH JR.  
CHRISTINE M. SAPONARA  
COURTNEY SHUM  
CHRISTINA TOY LEE  
JORDANN TURNER

CITY OF LOS ANGELES  
CALIFORNIA



KAREN BASS  
MAYOR

LOS ANGELES DEPARTMENT  
OF CITY PLANNING  
EXECUTIVE OFFICES

VINCENT P. BERTONI, AICP  
DIRECTOR

SHANA M.M. BONSTIN  
DEPUTY DIRECTOR

HAYDEE URITA-LOPEZ  
DEPUTY DIRECTOR

ARTHI L. VARMA, AICP  
DEPUTY DIRECTOR

LISA M. WEBBER, AICP  
DEPUTY DIRECTOR

[planning.lacity.org](http://planning.lacity.org)

Decision Date: October 21, 2025

Last Day to Appeal: November 5, 2025

Appeal Period Ends:

Bennett Lefebvre (A)  
Starbucks Coffee Company  
2401 Utah Avenue South  
Seattle, WA 98134

Megdal Pizzis, LLC (O)  
A California limited liability company  
c/o Elliot Megdal  
252 South Beverly Drive, Ste C  
Beverly Hills, CA 90212-3901

Ryan Abraham (R)  
Kaidence Group  
5070 North 40<sup>th</sup> Street, Unit 210  
Pheonix, AZ 85018

CASE NO. ZA-2024-5222-CU2-ZBA  
CLASS 2 CONDITIONAL USE PERMIT,  
ZONE BOUNDARY ADJUSTMENT  
23717 West Victory Boulevard  
Canoga Park–Winnetka–Woodland Hills–  
West Hills  
Zone: QC1-1VL and P-1VL  
C.D. : 12 – Lee  
D.M. : 180B093  
CEQA: ENV-2024-5223-CE  
Legal Description: Lot B, Tract PM 3275

Pursuant to California Environmental Quality Act, I hereby DETERMINE:

based on the whole of the administrative record, that the Project is exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines, Sections 15301, (Class 1) and 15305 (Class 5) and there is no substantial evidence demonstrating that any exceptions contained in Section 15300.2 of the State CEQA Guidelines regarding location, cumulative impacts, significant effects or unusual circumstances, scenic highways, or hazardous waste sites, or historical resources applies; and

Pursuant to Los Angeles Municipal Code (LAMC) Sections 12.24-W,17, I hereby APPROVE:

a Class 2 Conditional Use to permit a drive-through coffee shop located adjacent to a residential zone;

Pursuant to Los Angeles Municipal Code Sections 12.24-W,27 I hereby APPROVE:

a Class 2 Conditional Use Permit to allow the following deviations from LAMC Section 12.22-A,23 in conjunction with a Commercial Corner Development in the QC1-1VL and P-1VL Zones; and

Pursuant to Los Angeles Municipal Code Section 12.30 K, I hereby APPROVE:

a Zone Boundary Adjustment to allow maintenance of necessary elements of the proposed coffee shop including the building, drive-through lane, trash enclosure, signage and path of travel to be contained within the existing C1 Zone,

upon the following additional terms and conditions:

1. All other use, height and area regulations of the LAMC and all other applicable government/regulatory agencies shall be strictly complied with in the development and use of the property, except as such regulations are herein specifically varied or required.
2. The use and development of the property shall be in substantial conformance with the plot plan and floor plan submitted with the application and marked Exhibit "A", except as may be revised as a result of this action.
3. The authorized use shall be conducted at all times with due regard for the character of the surrounding district, and the right is reserved to the Zoning Administrator to impose additional corrective Conditions, if, in the Administrator's opinion, such Conditions are proven necessary for the protection of persons in the neighborhood or occupants of adjacent property.
4. All graffiti on the site shall be removed or painted over to match the color of the surface to which it is applied within 24 hours of its occurrence.
5. A copy of the first page of this grant and all Conditions and/or any subsequent appeal of this grant and its resultant Conditions and/or letters of clarification shall be printed on the building plans submitted to the Department of City Planning and the Department of Building and Safety for purposes of having a building permit issued at any time during the term of this grant.
6. Prior to the effectuation of this grant, a covenant acknowledging and agreeing to comply with all the terms and conditions established herein shall be recorded in

the County Recorder's Office. The agreement (standard master covenant and agreement form CP-6770) shall run with the land and shall be binding on any subsequent owners, heirs or assigns. The agreement with the conditions attached must be submitted to the Department of City Planning for approval before being recorded. After recordation, a certified copy bearing the Recorder's number and date shall be provided for inclusion in case file. Fees required per LAMC Section 19.01-E,3 for Monitoring of Conditional Use Permits and Inspection and Field Compliance Review of Operations shall be paid to the City prior to the final clearance of this condition.

7. Authorized herein is the construction, use, and maintenance of a new 1,479 square-foot drive-through coffee shop with one outdoor pick-up window with hours of operation from 4:30 a.m. to 10:00 p.m., Monday through Thursday, and 4:30 a.m. to 11:00 p.m., Friday through Sunday, with the following deviations from the mini-shopping center/commercial corner standards:
  - a. Less than 50 percent transparent windows on exterior walls/doors fronting Victory Boulevard;
  - b. Less than five-foot width of landscaping planter area on the Victory Boulevard street frontage and the perimeter of the parking area;
  - c. The continued use of an existing 26-foot tall on-site pole sign; and
  - d. Hours of operation exceeding the maximum permitted hours of 7:00 a.m. to 11:00 p.m., daily.
8. Authorized herein is a Zone Boundary Adjustment to allow the equal exchange of approximately 3,757 square feet between the C1-zoned and P-zoned portions of the parcel to permit the construction, use and maintenance of a new, approximately 1,479 square-foot Starbucks coffee-shop with drive-through depicted on Exhibit A.
9. After hours use shall be prohibited, except routine clean-up. This includes but is not limited to private or promotional events, special events, excluding any activities which are issued film permits by the City.
10. A new six (6)-foot tall masonry wall shall be installed along the west side of the property as shown on Exhibit A.
11. Parking shall be provided in compliance with the Municipal Code and to the satisfaction of the Department of Building and Safety. No variance from the parking requirements has been requested or granted herein.
12. **Complaint Log.** A telephone number and email address shall be provided for complaints or concerns from the community regarding the operation. The phone number and email address shall be posted at the following locations:
  - a. Entry, visible to pedestrians
  - b. Customer service desk, front desk or near the reception area.

Complaints shall be responded to within 24 hours. The applicant shall maintain a log of all calls and emails, detailing: (1) date complaint received; (2) nature of complaint, and (3) the manner in which the complaint was resolved.

13. The applicant shall ensure patrons do not loiter around the premises. Loitering is prohibited on or around these premises and the area under the control of the applicant. "No Loitering" signs shall be posted outside of the subject facility including the parking areas of the site.
14. The applicant shall be responsible for maintaining the premises over which they have control, including the adjoining sidewalk and any public or temporarily closed alleys abutting the site, free of debris and litter.
15. Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties.
16. **Electric Vehicle Parking.** All electric vehicle charging spaces (EV Spaces) and any electric vehicle charging stations (EVCS) shall comply with the regulations outlined in Sections 99.04.106 and 99.05.106 of Article 9, Chapter IX of the LAMC.
17. There shall be no outdoor speakers, except for the drive-through speaker box used for taking and receiving food and beverage orders, anywhere on the premises.
18. **Drive-Through Speaker Box.** Noise from the speaker box shall be inaudible beyond the property line. The applicant shall ensure that sound emitted from the speaker box is in compliance with the City's Noise Ordinance. All speaker boxes shall be equipped with the Automatic Volume Control (AVC) feature to reduce noise levels during quieter hours of the day. The applicant shall also consider incorporating captions on the menu boards where orders are taken to facilitate the ordering process and minimize noise.
19. Any music, sound or noise which is under control of the applicant shall not constitute a violation of Sections 112.06 or 116.01 of the LAMC (Citywide Noise Ordinance). At any time, a City representative may visit the site during operating hours to measure the noise levels. If, upon inspection, it is found that the noise level exceeds those allowed by the citywide noise regulation, the owner/operator will be notified and will be required to modify or eliminate the source of the noise or retain an acoustical engineer to recommend, design and implement noise control measures within property such as, noise barriers, sound absorbers or buffer zones.
20. The Applicant(s) shall comply with 6404.5(b) of the Labor Code, which prohibits smoking within any place of employment. The applicant shall not possess ashtrays or other receptacles used for the purpose of collecting trash or cigarettes/cigar butts within the interior of the subject establishment.

21. **Trash Enclosure.** Trash and Recycling bins shall be located within a gated, covered enclosure, in substantial conformance with Exhibit A. Trash/recycling containers shall be locked when not in use. Trash enclosure shall not be placed in or block access to required parking or the adjoining alley. Trash enclosure door shall not swing open onto any required parking space.
22. **On-site Commercial Deliveries.** All on-site deliveries shall be coordinated with the applicant's vendors to: 1) utilize driveways that would be optimal in minimizing delivery trucks from driving in reverse and triggering beeping noises, and 2) avoid utilizing the abutting alley.
23. **Driveway Plan.** Prior to the issuance of a building permit, a parking area and driveway plan shall be submitted to the Department of Transportation for review and approval.
24. All HVAC equipment shall be fully shielded or enclosed from line of sight of any adjacent property.
25. A camera surveillance system shall be installed and operating at all times to monitor the interior, entrance, exits and exterior areas, including the trash enclosure, in front of and around the premises. Recordings shall be maintained for a minimum period of 30 days and are intended for use by the Los Angeles Police Department.
26. **Pole Sign.** The applicant shall file for a Revocable Permit for the existing 26-foot tall pole sign located within the proposed five-foot dedicated area along the south portion of the site. The pole sign shall be double-faced and subject to the standards of Los Angeles Municipal Code Section 14.4.12. In the event a Revocable Permit is not issued for this pole sign, the applicant shall be subject to the provisions of the Code for any new sign constructed/installed.
27. The applicant shall coordinate with the Department of Transportation (LADOT) West Valley District Office to 1) install a "Right Turn Only" sign at the Victory Boulevard (egress) driveway, and 2) install traffic measures to prohibit left turns from the eastbound lanes of Victory Boulevard, to the satisfaction of the Department of Transportation.
28. An employee shall be stationed at the drive-through aisle during 1) peak ordering times, and 2) throughout the day when cars exceed the 13-car queue, to facilitate traffic movement, order and payment.

#### **ADMINISTRATIVE CONDITIONS**

29. **Expedited Processing Section.** Prior to the clearance of any conditions, the applicant shall show proof that all fees have been paid to the Department of City Planning, Expedited Processing Section.

30. Should there be a change in the ownership and/or the operator of the business, the property owner and the business owner or operator shall provide the prospective new property owner and the business owner/operator with a copy of the conditions of this action prior to the legal acquisition of the property and/or the business. Evidence that a copy of this determination including the conditions required herewith has been provided to the prospective owner/operator shall be submitted to the Department of City Planning in a letter from the new operator indicating the date that the new operator/management began and attesting to the receipt of this approval and its conditions. The new operator shall submit this letter to the Department of City Planning within 30-days of the beginning day of his/her new operation of the establishment along with any proposed modifications to the existing the floor plan, seating arrangement or number of seats of the new operation.
31. The Zoning Administrator reserves the right to require that the owner or operator file a Plan Approval application, if it is determined that the new operation is not in substantial conformance with the approved floor plan, or the operation has changed in mode or character from the original approval, or if documented evidence be submitted showing a continued violation(s) of any condition(s) of this grant resulting in a disruption or interference with the peaceful enjoyment of the adjoining and neighboring properties. The application, in association with the appropriate fees, and a 500-foot notification radius, shall be submitted to the Department of City Planning. The purpose of the plan approval will be to review the operation of the premise and establish conditions applicable to the use as conducted by the new owner or operator, consistent with the intent of the Conditions of this grant. Upon this review, the Zoning Administrator may modify, add or delete conditions, and if warranted, reserves the right to conduct this public hearing for nuisance abatement/revocation purposes.
32. At any time during the period of validity of this grant, should documented evidence be submitted showing continued violation of any condition of this grant of the location, resulting in an unreasonable level of disruption or interference with the peaceful enjoyment of the adjoining and neighboring properties, the Zoning Administrator (Upon his/her initiative, or upon written request by LAPD or the Council Office) reserves the right to call for a public hearing requiring the applicant to file for a plan approval application together with associated fees pursuant to LAMC Section 19-01-E, the purpose of which will be to review the applicant's compliance with and the effectiveness of these conditions. The applicant shall prepare a radius map and cause notification to be mailed to all owners and occupants of properties within a 500-foot radius of the property, the Council Office and the Los Angeles Police Department's corresponding division. The applicant shall also submit a summary and any supporting documentation of how compliance with each condition of this grant has been attained. Upon this review, the Zoning Administrator may modify, add or delete conditions, and reserves the right to conduct this public hearing for nuisance abatement/revocation purposes.

**33. INDEMNIFICATION AND REIMBURSEMENT OF LITIGATION COSTS**

Applicant shall do all of the following:

- a. Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- b. Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
- c. Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- d. Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- e. If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions include actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

### **OBSERVANCE OF CONDITIONS - TIME LIMIT - LAPSE OF PRIVILEGES**

All terms and conditions of the approval shall be fulfilled before the use may be established. The instant authorization is further conditional upon the privileges being utilized within three years after the effective date of approval and, if such privileges are not utilized or substantial physical construction work is not begun within said time and carried on diligently to completion, the authorization shall terminate and become void.

### **TRANSFERABILITY**

This authorization runs with the land. In the event the property is to be sold, leased, rented or occupied by any person or corporation other than yourself, it is incumbent upon you to advise them regarding the conditions of this grant.

### **VIOLATIONS OF THESE CONDITIONS, A MISDEMEANOR**

Section 12.29 of the Los Angeles Municipal Code provides:

"A variance, conditional use, adjustment, public benefit or other quasi-judicial approval, or any conditional approval granted by the Director, pursuant to the authority of this chapter shall become effective upon utilization of any portion of the privilege, and the owner and applicant shall immediately comply with its Conditions. The violation of any valid Condition imposed by the Director, Zoning

Administrator, Area Planning Commission, City Planning Commission or City Council in connection with the granting of any action taken pursuant to the authority of this chapter, shall constitute a violation of this chapter and shall be subject to the same penalties as any other violation of this Code.”

Every violation of this determination is punishable as a misdemeanor and shall be punishable by a fine of not more than \$2,500 or by imprisonment in the county jail for a period of not more than six months, or by both such fine and imprisonment.

### NOTICE

The applicant is further advised that subsequent contact regarding this determination must be with the staff assigned to this case. This would include clarification, verification of condition compliance and plans or building permit applications, etc., and shall be accomplished **BY APPOINTMENT ONLY**, in order to assure that you receive service with a minimum amount of waiting. You should advise any consultant representing you of this requirement as well.

### FINDINGS OF FACT

After thorough consideration of the statements contained in the application, the plans submitted therewith, and the statements made at the public hearing on March 26, 2025, all of which are by reference made a part hereof, as well as knowledge of the property and surrounding district, I find that the requirements for authorizing a conditional use approval under the provisions Sections 12.24-W,27, 12.30.K have been established by the following facts:

### BACKGROUND

The subject property is an irregular-shaped 23,478 square-foot lot with approximately 126 feet of frontage along Victory Boulevard. Approximately 8,125 square feet of that lot area is within the QC1-1VL Zone, while approximately 15,353 square feet of that lot area is within the P-1VL Zone. The property is currently improved with a vacant, approximately 2,135 square-foot formal Pizza Hut restaurant, one (1) non-conforming trash enclosure, two (2) wireless cell towers and the remaining site consists of existing parking lot.

The project site is located within the Canoga Park – Winnetka – Woodland Hills – West Hills Community Plan, which is one of 35 Community Plans which together form the land use element of the General Plan. The Community Plan designates the site for Community Commercial land uses corresponding to the CR, C2, C4, RAS3 and RAS4. The subject property is split-zoned QC1-1VL and P-1VL and is thus consistent with the land use designation on the site.

Furthermore, the present Q Condition (Ordinance No. 149,380) was passed by the City Council at its meeting of February 28, 1977. The Q condition is imposed on the C1 portion of the site requires the following limitations: no structures on the site shall

exceed one story in height, all open areas not used for buildings, driveways, parking areas or walks shall be attractively landscaped, landscaped areas shall be equipped with sprinklers and shall be maintained in a first-class condition at all times, a six (6)-foot masonry wall shall be installed along the west side of the property, access of the alley for commercial vehicle is prohibited.

The subject site is also located within an Urban and Built-up Land zone, Outside Flood zone, Urban Agriculture Incentive zone, and is located within 13.47 km from the Malibu Coast Fault.

The proposed project involves a Class 2 Conditional Use Permit and a Zone Boundary Adjustment to allow the construction, use, and maintenance of a new 1,479 square-foot Starbucks Drive-Through Coffee Shop with a trash enclosure and walk up/pick up window. The project proposes an adjustment to the C1 zone boundary so that the entire building, signage, patio, trash enclosure, and drive-through lanes are all within the boundaries of the existing commercial zone.

The building will sit street front with the drive through on back half of the property. The western property line will retain the existing parking asphalt with minor modifications, including adding a 20-foot property front landscaped area in the southwest corner to meet parking requirements (no car to be parked closer than 20-feet from the property line), the addition of an EV van accessible stall, and decreasing the size of the planter/landscaped island in the Northwest corner to accommodate one more parking space. The total number of parking spaces would be 16. Additionally, a trash enclosure would be built to LAMC code, and the transformer will be relocated with a staging area and access to LADWP requirements.

### **Surrounding Uses**

The property to the north, abutting the subject property is zoned P-1VL with a land use designation of Community Commercial and developed with a surface parking lot and a McDonald's fast-food restaurant with drive-through. Properties to the east, abutting the subject property are zoned C1.5-1VL with a land use designation of Community Commercial and developed with a one-story mini-shopping center with surface parking lot. Properties the south, across Victory Boulevard are zoned C1.5-1VL and RS-1 with land use designations Neighborhood Office Commercial and Low Residential and developed with a gasoline station located at southwest corner of Victory Boulevard and Platt Avenue and single-family residences along Victory Boulevard. Properties to the west, across an adjoining alleyway are zoned RS-1 with a land use designation Low Residential and developed with single-family residences.

### **Previous Cases, Affidavits, Permits, and Orders on the Subject Property:**

Case No. ZA-2009-8046-CU: On February 22, 2007, the Zoning Administrator approved a Conditional Use permit to allow the installation, use, and maintenance of an unmanned wireless telecommunications facility located at the subject property.

Case No. ZA-1998-247-ZV: On May 22, 1998, the Zoning Administrator approved a Zone Variance to allow the construction, use, and maintenance of a combination stealth wireless telecommunications facility and parking lot light 45 feet in height in the parking lot of the property improved with a Pizza Hut restaurant located at the subject property.

**Previous Cases, Affidavits, Permits, and Orders on the Surrounding Properties:**

There are no relevant cases surrounding the subject property within a 1,000-foot radius.

**Public Correspondence**

An email dated September 29, 2025 from West Hills Neighborhood Council, Zoning and Land Use Committee included the following: Concerns, observations, photos from walk-through of site, and recommendations were submitted. The Committee recommended a 10 p.m. closing time and no right-turn be placed on driveway egress facing Victory Boulevard.

An email dated September 9, 2025, from Jeff Bornstein, President of the West Valley Alliance for Optimal Living: Project adds traffic, noise and pollutants. There is no study of impacts under CEQA. Drive-through lane is awkward and cars will impact the streets. Too many deviations requested. McDonald's drive-through is further away from residential properties than this project site.

An email dated September 24, 2025 from Jen Westmoreland, stated opposition. She stated observations of accidents near Victory and Platt intersection. Some drivers speed. Inquired of project details, access for McDonald's customers, trash location, adequate parking, site cannot accommodate a drive-through and creating additional traffic nightmares,

An email dated September 10, 2025 from Fern White, stated opposition. The project adds traffic congestion to an already busy intersection. Unnecessary having another Starbucks and there is already another coffee shop next door.

An email dated September 11, 2025 from Fern White stated opposition. The intersection is busy. The project adds traffic congestion to an already busy intersection. Unnecessary having another Starbucks and there is already another coffee shop next door.

An email dated September 23, 2025 from Steven Brtis, stated the project is not a good fit. The project adds to traffic, parking concerns, cars exiting the site will they turn left or right and will drivers be able to adjust by going westbound?

An email dated September 15, 2025 from Anne Steinberger, stated the existing homeless people on the project site, and that the sidewalks are in terrible condition.

An email dated September 11, 2025 from Kathy Connell, stated the project will add to an increase in traffic dangers.

## Public Hearing

A public hearing was held on September 6, 2025. The hearing was attended by the applicant, Bennett Lefebvre, applicant's representatives, Ryan Abraham and Jennifer Quarry, and members of the community.

Ms. Quarry made the following statements:

- The request is to build a Starbucks coffee house just west of the northwest corner of Victory and Platt.
- There are four items requested. The first, CEQA exemption. The second is a Class 2 CUP to allow a drive through that's adjacent to residentially zoned property. Deviations from the commercial corner involve 1) the 50% transparency requirement, 2) from the five foot required landscape, 3) the continued use of the existing pole sign, and 4) we're operating hours to operate between 11:00 PM. and 7:00 AM.
- Also requested is a zone boundary adjustment between the P1 and C1 property boundaries.
- The surrounding neighborhood. There are many commercial uses in the area. Across the street there is across Platt Ave. is a major shopping center with an existing Starbucks Café in the grocery store. There's a Chase Bank, CVS, and many different uses there.
- Across the street and to the south, there's two gas stations with one at our corner. We are in a small shopping center. McDonald's is to the north of us and so is the substation and directly to the east of us there is a little small retail complex that has Goodwill, Domino's, and a Coffee Bean Tea and Leaf and a couple small users in there.
- On our property, there is a former Pizza Hut building that has been vacant for a number of years.
- The site has two different zoning categories and is the reason for the zone boundary adjustment. The majority of the site has P1VL zoning and then the building and the active back operation.
- The area has a C zoning from 1977, and is subject to five different conditions.
- The site's land use designation is community commercial.
- The existing Pizza Hut building has been vacant for a number of years, and is in rough shape.
- It is currently fenced off just because of vandalism and graffiti. There is a maintenance issue there. Everything on the site, including the sidewalk and driveways will be removed and replaced with new construction.
- This slide shows the inside of a typical Pizza Hut built circa 1975. It's in rough shape again and the structure and problematic areas are being removed.
- An entirely new site along the street will be built. The new building will be built up to the street to give it a street predominant look.
- Approximately 1,300 square-foot drive through coffee shop with a walk up area will be built. There is no interior cafe at this location because there's an existing Starbucks cafe across the street that will remain. This will not be removed and is a complimentary use.

- This allows us to keep the existing retail use, and offer the same amenities that most of the Starbucks and coffee houses in the area.
- The site has some landscaping in the front area. There's one or two and we will add a total 13 trees with a total square-footage of approximately 6900 square feet.
- There will be on-site queuing for 13 cars.
- There is currently parking along our western property line and that will remain since all the utilities run in this identified area.
- A new trash enclosure that meets City requirements will be added. Required EV parking stalls will be added.
- The menu board will be as far away from the residences as possible. It will be on the east side. With the layout, sound volume and transmission to the residential area will be further decreased.
- Starbucks utilizes an automatic volume reduction system so a speaker for the menu board is always kept just barely above the ambient level.
- There will be new site lighting. The applicant has conducted necessary photometric studies to demonstrate that lighting will not bleed into the surrounding.
- There's a small 3-ft tall screen wall adjacent to the alley. We will replace that entire screen wall with a new 6 foot tall wall fence
- This slide shows driving on Victory Blvd. We don't meet the required 50% transparency. We don't have a blank wall that faces out to the community. The right half of this building is backup operations. We propose to do some elevated design through a graphic art element.
- There are windows around the order taking, and additional windows.
- The aesthetics have been elevated to meet the intent of the ordinance.
- We don't have a letter of recommendation from surrounding neighbors and Community Association. We did meet and have continuing conversations with the West Hills Neighborhood Council several times over the last year, but unfortunately we have had scheduling conflicts, and we need to make progress on our application. Their next meeting will be a month and a half, but I want to highlight that we have listened continually to them.
- We have addressed issues and gave a presentation last October to the West Hills Neighborhood Planning and Zoning Committee meeting.
- We walked the site as well.
- Starbucks went out there in March and we did a site walk with them and showed them what's existing and walked them through the proposal. It went really well.
- We meet the posting requirements. The public notice was mailed in August to the neighbors within 300 feet and the West Hills Neighborhood Council was included in that mailing. Staff confirmed the affidavit for the mailing. We do meet the statutory and the ordinance requirements for public notification. We just don't have that letter in hand.
- We received feedback on initial hours of 24 hours of operating. We have reduced those hours so that we're closed between 11:00 PM and 4:30 AM. These hours mirrors what the adjacent McDonald's. The opening time mirrors the existing Starbucks across the street. The other issues were proximity to the residential homes to the West. We are building a brand new 6-ft tall wall adjacent to the

alleyway. It's currently a couple feet high. We are not immediately next to the abutting homeowner. There is an alley between us.

- Further back at the back of our property, there is a tall fence already in place there and that will remain.
- This portion of the property is furthest away from our existing building. The distance our building to the that residential will be at a greater distance.
- To respond to why is another Starbucks needed, there's another coffee house down the street and there is a Coffee Bean next door. This is not a replacement Starbucks. This only serves to complement the existing coffee house. Having a Coffee Bean right next door is no big deal. It is normal in shopping centers to see similar uses next to each other.
- What we also heard from the neighborhood council meeting is the community would like to see a recreation center in this neighborhood. This isn't city property, and it's not large enough nor would it be appropriate for that community rec center.
- However, Starbucks is a great community partner. We hire from within the community. We are a great employer, it is expected to employ 20 to 30 people here. We provide health insurance, benefits and educational aid for anyone who works 20 hours or more.
- To restate, we've met the intent of the 50% for transparency by providing the alternative materials and maximizing the windows where we can. We do have a hardship because it is an existing condition for that utility line on the west property line, so we can't add an additional landscape. But instead we put in the six foot wall, and added additional landscaping along the street side. Finally, we're proposing the continued use of that existing pole sign. This is a great example when you can reuse what's existing.
- Operating hours have been amended so that we're not 24 hours, We will open at 4:30 in the morning and closing at 11:00 PM.
- These boundary lines of the C and P zones will have adjustments as shown on the screen.

During the public comment portion of the public hearing, three people provided public testimony.

The first speaker, Michael Teck, stated he represents the abutting McDonald's restaurant. He noted that homeless exists on the site, spoke of the easement on the property between the site and the McDonald's site, and was in attendance to understand the project better.

The second speaker, Jeff Bornstein, stated he is with the West Valley Alliance for Optimal Living. He stated concerns of traffic being added by the project, the need to comply with CEQA, homes to the west of the site, and the hours proposed being too late. He noted the presence of a McDonald's drive-thru nearby, and the many deviations being requested.

The third speaker, Steve and Sue, stated they reside in Woodland Hills, stated concerns of traffic, asked if the exit to Victory Boulevard will be right turn only, the presence of other similar uses and potential conflict with the elementary school.

The fourth speaker, Char Rothstein, stated she is with the West Hills Neighborhood Council, and is requesting for postponement until the neighborhood council can schedule them for Oct 14. She stated Starbucks has been cooperative, and discussed how issues need to be addressed so as to not disturb neighboring residential uses.

At the conclusion of the public hearing, the Zoning Administrator took the case under advisement for two weeks to allow for the public to submit additional comments.

### **CONDITIONAL USE FINDINGS**

- The project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city or region.**

The subject property is an irregular-shaped 23,478 square-foot lot with approximately 126 feet of frontage along Victory Boulevard. Approximately 8,125 square feet of that lot area is within the QC1-1VL Zone, while approximately 15,353 square feet of that lot area is within the P-1VL Zone. The property is currently improved with a vacant, approximately 2,135 square-foot restaurant, one (1) non-conforming trash enclosure, two (2) wireless cell towers and the remaining site consists of existing parking lot.

The project site is located within the Canoga Park – Winnetka – Woodland Hills – West Hills Community Plan. The Community Plan designates the site for Community Commercial land uses corresponding to the CR, C2, C4, RAS3 and RAS4. The subject property is split-zoned QC1-1VL and P-1VL and is thus consistent with the land use designation on the site. The subject site is also located within an Urban and Built-up Land zone, Outside Flood zone, Urban Agriculture Incentive zone, and is located within 13.47 km from the Malibu Coast Fault.

According to public testimony and the applicant, the site has been closed off to prevent break-ins into the vacant building. Accounts from the public and the applicant also state that there is unwanted activities such as trash and debris, vagrancy, and graffiti. With inactivity of the site, there is a greater likelihood of more unwanted activities and nuisances on the premises.

The proposed project involves a Class 2 Conditional Use Permit and a Zone Boundary Adjustment to allow the construction, use, and maintenance of a new 1,479 square-foot Starbucks drive-through coffee shop with a trash enclosure and walk up/pick up window. The project proposes an adjustment to the C1 zone

boundary so that the entire building, signage, patio, trash enclosure, and drive-through lanes are all within the boundaries of the existing commercial zone.

The drive-through entrance will begin at the back half of the property while the front façade of the building will face Victory Boulevard. The drive through lane access point will be located in the interior of the lot, helping to ensure there is no spillover onto Victory Boulevard or impediment to traffic. The order point will be along the eastern property line, using a speaker box with Automated Volume Control, which will automatically adjust the volume according to ambient sound levels. This ensures that no excess noise is generated from the order point.

The western property line will retain the existing parking asphalt with minor modifications, including adding a 20-foot property front landscaped area in the southwest corner to meet parking requirements (no car to be parked closer than 20-feet from the property line). There will be the addition of an EV van accessible stall, and the size of the planter/landscaped island in the Northwest corner will be decreased to accommodate an additional parking space. The project will have a total of 16 parking spaces. Additionally, a trash enclosure would be built to LAMC Code requirements. The existing transformer will be relocated and a new staging area will be constructed to allow access and meet Los Angeles Department of Water and Power (LADWP) requirements.

The project will offer convenience and site enhancement to members of the community and to commuters at reasonable hours. While there are other options for coffee and other products, the subject project will bring an operation that will offer convenience for customers to pick up their orders through a walk-up window facing Victory Boulevard or to utilize the drive-through to order food and beverage and continue to their destination. With the use of mobile orders, and with no indoor and outdoor seats, the walk-up window and the drive-through can facilitate the duration of these transactions by allowing for drinks and other orders to be made in advance of pick-up. This business model will offer the community a convenient choice to pick up their beverages and food products in a quick and convenient manner. The project will add up to 13 trees to improve the aesthetics of the site and parking areas.

The project will offer a mural facing Victory Boulevard in lieu of meeting the provisions for 50 percent transparent windows on exterior walls/doors fronting Victory Boulevard. The mural will serve as an amenity of public art. Landscaping will be offered along and throughout the site as part of the project. There will be less than five-foot width of landscaping planter area along the west side of the site. A new 6-foot tall wall will be constructed along the western perimeter, and new landscaping and planting of trees will improve the aesthetics of the site. A new face sign will be proposed and utilize an existing pole sign near Victory Boulevard. The sign will be subject to the provisions of the sign ordinance and be subject to meet Code requirements. In addition, a new trash enclosure is proposed. It will be locked when not in use. Finally, the project will

re-grade the site allowing for the public sidewalk to be brought up to current Department of Transportation (DOT) requirements, as well as necessary highway dedication.

The proposed building is positioned towards the front of the property, to allow for a pedestrian friendly accessible walk-up window to place orders or pick up to-go orders, placed ahead of time. Lastly, ordering vehicles as well as site landscaping will help shielded to reduce noise. The direction of the drive through lane also helps ensure lights from vehicles do not shine directly at any residences.

As such, the proposed project will substantially upgrade the aesthetic and functional qualities of the site and will promote economic well-being and public convenience in the community and enhance the built environment by providing a use that will bring services that will be beneficial to the surrounding community. It will bring a use that will activate the site and remove a dilapidated building that can bring many unwanted activities to the community. The imposition of a number of conditions addressing the operational issues will ensure that the project will not be disruptive to but compatible with the surrounding community.

2. **The project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare and safety.**

The surrounding area is predominantly characterized with commercial and single-family zones. The property to the north, abutting the subject property is zoned P-1VL with a land use designation of Community Commercial and developed with a surface parking lot and a fast-food restaurant with drive-through. Properties to the east, abutting the subject property are zoned C1.5-1VL with a land use designation of Community Commercial and developed with a one-story mini-shopping center with surface parking lot. Properties the south, across Victory Boulevard are zoned C1.5-1VL and RS-1 with land use designations Neighborhood Office Commercial and Low Residential and developed with a gasoline station located at southwest corner of Victory Boulevard and Platt Avenue and single-family residences along Victory Boulevard. Properties to the west, across an adjoining 20-foot alleyway are zoned RS-1 with a land use designation Low Residential and developed with single-family residences. There is an existing cross-access easement with the parcel to the North (currently operating as a McDonalds Café with Drive Through), which runs from the McDonald's site on the south side through the proposed site to Victory Boulevard.

The proposed project involves a Conditional Use Permit and a Zone Boundary Adjustment to allow the construction, use, and maintenance of a new 1,479 square-foot Starbucks drive-through coffee shop with a trash enclosure and walk

up/pick up window. The project proposes an adjustment to the C1 zone boundary so that the entire building, signage, patio, trash enclosure, and drive-through lanes are all within the boundaries of the existing commercial zone.

As stated above, the drive-through entrance will begin at the back half of the property while the front façade of the building will face Victory Boulevard. The drive through lane access point will be located in the interior of the lot, helping to ensure there is no spillover onto Victory Boulevard or impediment to traffic. The order point will be along the eastern property line, using a speaker box with Automated Volume Control, which will automatically adjust the volume according to ambient sound levels. This ensures that no excess noise is generated from the order point.

The western property line will retain the existing parking asphalt with minor modifications, including adding a 20-foot property front landscaped area in the southwest corner to meet parking requirements (no car to be parked closer than 20-feet from the property line). There will be the addition of an EV van accessible stall, and the size of the planter/landscaped island in the Northwest corner will be decreased to accommodate an additional parking space. There will be a total of 16 parking spaces. Additionally, a trash enclosure would be built to LAMC Code requirements. The existing transformer will be relocated and a new staging area will be constructed to allow access and meet Los Angeles Department of Water and Power (LADWP) requirements.

The subject property is zoned and would be developed in a manner consistent with both the City's desired land use designation for the site and the surrounding area. The proposed Starbucks with drive-through will be compatible with the designated land uses. The project is further compatible with the function of the surrounding area as an area with an existing fast-food restaurant with drive-through services and will not be detrimental to surrounding uses.

Landscaping for the interior of the drive through/site would provide ground cover, deterring any kind of camping (other than where the required LADWP staging area is proposed for the new transformer location. Gates will be installed on the eastern edge of the building in both the southeast and northeast corners of the building between the building and the wall on the eastern property line, to further deter and prevent loitering or camping in the space between our building and the neighbor to the east.

The proposed establishment provides an amenity to the area, which will supplement and contribute to the function of the surrounding area as a mixed residential and commercial neighborhood and the project will be compatible with adjacent properties and the surrounding community. Concerns were raised by the public at the public hearing and submitted correspondence.

Traffic impacts, noise, lighting, late night operation, and potential disturbances were brought by the community.

With regard to traffic, as configured, vehicles will enter the site mainly through the driveway on Victory Boulevard. Customers who will utilize the drive-through feature will enter the beginning of the drive-through lane located near the back half of the site, and drive through the lane that can accommodate up to 13 vehicles that lead them back southerly toward Victory Boulevard. Vehicles would also have the option of utilizing the cross-access easement to the north to exit the site, and utilize the abutting driveway to the north and approach the Victory Boulevard and Platt Avenue intersection. The hours of operation were considered and conditioned to be from 4:30 a.m. to 10:00 p.m., Monday through Thursday, and 4:30 a.m. to 11:00 p.m., Friday through Sunday. These hours consider comments made by the West Hills Neighborhood Council, as well as operating characteristics of the subject business, and siting and distance from residential areas, and the imposed conditions, all discussed above.

In addition, conditions have been imposed to encourage responsible management, deter criminal activity, and ensure the drive-through operation will not be detrimental to the residential uses to the west or the any other of the surrounding uses. These conditions will ensure that the operations comply with all applicable regulations and that no aspects of the operations will adversely affect the surrounding area. Lighting will be shielded away from any abutting uses. No speakers to play outdoor background music is permitted on the premises. The speaker box utilized for drive-through orders will be located at the furthest possible point away from the abutting residential uses to the west. In addition, the residential uses are buffered by a 20-foot alley and trees on walls. The operator will utilize speakers that will adjust during quieter portions of the day. In addition, the Zoning Administrator has imposed the condition to have the applicant coordinate with vendors to ensure delivery trucks utilize the optimal driveways to minimize driving beeping noises from trucks when the drive in reverse. In addition, the hours have been conditioned from 4:30 a.m. to 10:00 p.m., Monday through Thursday, and 4:30 a.m. to 11:00 p.m., Friday through Sunday, to ensure no late night operations occur and no potential disturbances arise from the operation, and creating any late night disturbances. Also, during peak times, and when the drive-through line exceeds the 13-car queue, staff will be required to position an employee outside to take order to facilitate the drive-through. This along with the need for a right-turn only sign from the egress of the driveway leading to Victory Boulevard were identified by the Department of Transportation's West Valley Office, when reviewing the plans. The grant includes conditions to address these concerns.

In addition to the operating hours, the applicant will deviate from other commercial corner standards. Such include the 50 percent transparency requirement, where the applicant will provide an art mural to enhance the aesthetic near the corner of Victory Boulevard and Platt Avenue. The mural will

bring façade interest facing Victory Boulevard and will not include any lighting features that will distract drivers. The applicant will also offer a less than five-foot wide landscaping along the west property line to maintain parking and construct a new 6-foot tall wall, serving as a noise barrier to protect residential uses. The project has been conditioned to shield HVAC equipment to reduce noise and visual impacts.

As conditioned, the project will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare, and safety and the development of the community.

**3. The project substantially conforms with the purpose, intent and provisions of the General Plan, the applicable community plan, and any specific plan.**

The elements in the General Plan establish policies and provide the regulatory environment for managing the city and for addressing concerns and issues. The majority of the policies derived from the elements in the General Plan are in the form of Code Requirements, which collectively form the Los Angeles Municipal Code (LAMC). The requested entitlement is for a conditionally permitted use and does not propose to deviate from any of the requirements of the LAMC.

The project site is located within the Canoga Park – Winnetka – Woodland Hills – West Hills Community Plan, which is one of 35 Community Plans which together form the land use element of the General Plan. The Community Plan designates the site for Community Commercial land uses corresponding to the CR, C2, C4, RAS3 and RAS4. The subject property is split-zoned C1 and P-1 and is consistent with the land use designation on the site.

The site is subject to a Qualitative [Q] Condition under Ordinance No. 149,380 that was passed by the City Council at its meeting of February 28, 1977. The Q condition is imposed on the C1 portion of the site limits the following: no structures on the site shall exceed one story in height, all open areas not used for buildings, driveways, parking areas or walks shall be attractively landscaped, landscaped areas shall be equipped with sprinklers and shall be maintained in a first-class condition at all times, a six (6)-foot masonry wall shall be installed along the west side of the property, access of the alley for commercial vehicle is prohibited. The project will comply with the requirements of the Q Condition through its design and conditions of this grant.

By allowing the development of the proposed Starbucks with drive-through with the deviations from the provisions of the mini-shopping center/commercial corner standards, the project will enable further development of the surrounding area as an area with potential for increased commercial uses. The project enhances the aesthetics of the site and brings a use that has been conditioned to operate compatibly with surrounding uses. The proposed conditional use supports the continued success of neighborhood serving commercial establishments in the area and, thus, the continued vitality of the community commercial neighborhood.

The deviations from the commercial corner standards allows for the enhanced aesthetics and pedestrian-friendly frontage along Victory Boulevard, reuse of an existing pole sign, and adequate buffering and transition from the residential uses to the west. The early hours offers a convenient service for those who work at earlier times of the day. The limited hours ensure compatibility with the nearby residential uses. Conditions have been imposed to promote compatibility with surrounding uses. Lighting, noise, aesthetics, and design to address potential traffic impacts, have been incorporated as conditions of this grant. In addition, the project has been conditioned to shield HVAC equipment to reduce noise and visual impacts. Additionally, the project follows an established pattern of zoning and land use that is consistent and compatible with other properties and uses in the surrounding area, which include a variety of commercial and other compatible uses. Thus, the project substantially conforms with the purpose, intent, and provisions of the General Plan and the Community Plan.

#### **DRIVE-THROUGH ESTABLISHMENT FINDINGS**

- 4. That residential uses in the vicinity of a proposed drive-through fast-food establishment will be adequately protected from any significant noise resulting from outdoor speakers, autos, or other sources of noise associated with the lot.**

There is a 20-foot alleyway located west of the subject site that separates the subject site from the residential uses. The project includes a design that not only enhances the aesthetics of the site but also takes into consideration of having residential uses near the site.

The site has an existing cross-access easement with the parcel to the North (currently operating as a McDonalds Café with Drive Through), which runs from the McDonald's site on the south side through the proposed site to Victory Boulevard.

Additionally, there is an existing sewer easement with McDonalds that runs along the western property line of the subject site, connecting to Victory Boulevard. Because of these easement constraints, the project maintains these easements and locates the new building with drive-through feature to operate at the eastern portion of the site. The project retains existing parking areas on the western property line. This allows access the continued access between Victory Boulevard and the parcel to the North.

The current project design allows the order point and pickup window to be located approximately 138 feet, which includes the 20-foot alley, from the nearest residential parcel. The drive-through pickup window will be oriented on the north façade of the building, which will be sited closer to Victory Boulevard, and enhance the pedestrian experience through landscaping, mural, and a pedestrian walk-up window facing Victory Boulevard. Additionally, both the drive

through entrance and the drive through exit are located away from the street frontage, preventing back up on Victory Boulevard.

Furthermore, there is a present Q Condition under Ordinance No. 149,380 that was passed by the City Council at its meeting of February 28, 1977. The Q condition is imposed on the C1 portion of the site requires the following limitations: no structures on the site shall exceed one story in height, all open areas not used for buildings, driveways, parking areas or walks shall be attractively landscaped, landscaped areas shall be equipped with sprinklers and shall be maintained in a first-class condition at all times, a six (6)-foot masonry wall shall be installed along the west side of the property, access of the alley for commercial vehicle is prohibited. The project will comply with the requirements of the Q Condition.

The project will remove the two existing cell towers, one of which was approved at 45 feet and 4 inches in 2007, which not only pushes the height limits of the 1-VL zoning but exceeds it by 4 feet. The proposed development is a single-story building at 19 feet and 7 ½ inches, including roof top screening of mechanical equipment. The Drive Through Order point is placed on the far eastern property line, as far away from abutting residential parcels, with landscaping including trees to help mitigate sound. The system used for order points by Starbucks also includes an Automatic Volume Control which will assess the ambient sound levels at the speaker box and automatically adjust the volume, so that the volume at the speaker box is only as loud as necessary to allow for clear ordering by patrons, preventing unnecessary sound levels on the site.

Lastly, the landscaping designed for the interior of the drive through/site would provide ground cover, deterring any kind of camping (other than where the required LADWP staging area is proposed for the new transformer location. Gates will be installed on the eastern edge of the building in both the southeast and northeast corners of the building between the building and the wall on the eastern property line, to further deter and prevent loitering or camping in the space between our building and the neighbor to the east. The project will also construct a new 6-foot tall wall that will contribute to offsetting any potential noise and disturbances from the drive-through operation. As such, residential uses in the vicinity of a proposed drive-through fast-food establishment will be adequately protected from any significant noise resulting from outdoor speakers, autos, or other sources of noise associated with the lot.

5. **That all stationary light generated on the lot is screened to avoid any significant adverse impact on nearby residential uses.**

All lighting on the lot will be oriented toward the site. The project has been conditioned for lighting to be shielded so it does not spill onto the adjacent residential property. In addition, the proposed face of the pole sign will be subject to the lighting standards of the Code. The project will utilize the existing

pole sign located near Victory Boulevard, and not introduce a new location for where a lit pole sign will be proposed. In addition, any additional lighting to illuminate the drive-through feature will be oriented for the drivers who utilize the drive-through feature. They are not proposed to reach a height that creates a line of sight for any of the abutting residential uses.

6. **That trash storage, trash pickup hours, driveways, parking locations, screening walls, trees and landscaping are provided for and located so as to minimize disturbance to the occupants of nearby residential uses, and to enhance the privacy of those uses.**

The proposed trash storage will be located within a concrete block wall structure. The trash enclosure will not be an obstruction to any of the existing parking spaces. It will be enclosed and has been conditioned to be locked when not in use. Trash pickup hours will be within the hours for which the City provides this service to pickup the trash.

Driveways will remain at their present location. A cross access easement exists between the subject site and the property to the north. This property is developed with a McDonald's restaurant that also offers a drive-through feature that offers a separate dedicated lane for McDonald's drive-through customers to utilize and additional width to accommodate non-drive-through customers to exit onto the public right-of-way.

The site will accommodate 16 parking spaces. There will be the addition of an EV van accessible stall. The project reduces the size of the planter/landscaped island in the northwest corner to accommodate an additional parking space.

The western property line will retain the existing parking asphalt with minor modifications, including adding a 20-foot property front landscaped area in the southwest corner to meet parking requirements (no car to be parked closer than 20-feet from the property line). There will be a total of 16 parking spaces. Additionally, a trash enclosure would be built to LAMC Code requirements. The existing transformer will be relocated and a new staging area will be constructed to allow access and meet Los Angeles Department of Water and Power (LADWP) requirements.

Trees and landscaping presently one tree on the site. The project will enhance the site and have 13 trees throughout the site. New landscaping will be at the frontage facing Victory Boulevard.

Parking areas will contain landscape areas with trees to enhance the separation between commercial and residential uses. Hours of trash pickup will conform to the limitations of Section 12.22 -A, 23(b)(4) of the zoning code. Therefore, trash storage, trash pickup hours, driveways, parking locations, screening walls, trees

and landscaping are provided for and located so as to minimize disturbance to the occupants of nearby residential uses and enhance the privacy of those uses.

**MINI-SHOPPING CENTER/COMMERICAL CORNER FINDINGS PURSUANT TO LAMC CHAPTER 1, SECTION 12.24 W.27**

7. **That based on data provided by the City Department of Transportation or by a licensed traffic engineer, that ingress to and egress from the project will not create a traffic hazard or cause significant traffic congestion or disruption of vehicular circulation on adjacent streets.**

The subject property is an irregular-shaped 23,478 square-foot lot with approximately 126 feet of frontage along Victory Boulevard. Approximately 8,125 square feet of that lot area is within the QC1-1VL Zone, while approximately 15,353 square feet of that lot area is within the P-1VL Zone. The property is currently improved with a vacant, approximately 2,135 square-foot previously utilized by Pizza Hut restaurant, one (1) non-conforming trash enclosure, two (2) wireless cell towers and the remaining site consists of existing parking lot.

The building will sit street front with the drive through on back half of the property. The western property line will retain the existing parking asphalt with minor modifications, including adding a 20-foot property front landscaped area in the southwest corner to meet parking requirements (no car to be parked closer than 20-feet from the property line), the addition of an EV van accessible stall, and decreasing the size of the planter/landscaped island in the Northwest corner to accommodate one more parking space. The total number of parking spaces would be 16. Additionally, a trash enclosure would be built to LAMC code, and the transformer will be relocated with a staging area and access to LADWP requirements.

There is a cross-access easement with the parcel to the North (currently operating as a McDonalds Café with Drive Through), which runs from the McDonald's site south through our proposed site to Victory Blvd. Additionally, there is a sewer easement with McDonalds which runs along the western property line of our site, connecting to Victory Blvd. These constraints, and the inability to move the curb cut, forced the design of the site to place the building on the Eastern side of the property, and retaining the existing parking on the western property line, and direct access between Victory Blvd and the parcel to the North. We feel this works to the benefit of the project by pushing the building, order point and pickup window as far away from the residential parcels as possible. The building will sit at the front of the property, with the drive-through pickup window on the North side of the building. Additionally, both the drive through entrance and the drive through exit are moved away from the street frontage, preventing any kind of back up on Victory Boulevard.

The West Valley Office of the Department of Transportation reviewed the site plan and identified the potential of the drive-through exit being blocked off if there is a demand that brings more vehicles to interact past the drive-through exit. The Zoning Administrator imposed a condition requiring an employee be present to facilitate the drive-through during peak hours as well as any time when the drive-through line exceeds the 13-car queuing. In addition, the Department of Transportation commented on the need for a right turn only at the driveway egressing out to Victory Boulevard, and a right-turn only to ingress onto the property from Victory Boulevard. The Zoning Administrator included a condition to coordinate with the Department of Transportation to install appropriate signs and traffic measures to ensure ingress and egress from the Victory Boulevard drive-through does not become a hazard to the public right-of-way.

In addition, the project does not meet the threshold to trigger a traffic assessment. The project driveway and circulation plans will be subject to final review from the Department of Transportation.

8. **That project approval will not create or add to a detrimental concentration of Mini-Shopping Centers or Commercial Corner Developments in the vicinity of the proposed project.**

The project is located within an existing Commercial Corner Development, on property zoned for commercial use. The site is currently is developed with a one-story commercial development comprised of vacant building previously used as a neighborhood serving restaurant. Since the subject tenant space has been utilized as a restaurant and is sitting vacant, the project will not create or add to the concentration of Mini-Shopping Centers in the vicinity of the project. The hours of operation have been conditioned to be from 4:30 a.m. to 10:00 p.m., Monday through Thursday, and 4:30 a.m. to 11:00 p.m., Friday through Sunday. The increase in hours is not within any peak hours, which may create an impact leading to a detrimental concentration of mini-shopping centers.

#### **ZONE BOUNDARY ADJUSTMENT FINDINGS**

9. **The recommended zone boundary adjustment is in conformance with the public necessity, convenience, general welfare or good zoning practice in that:**

The existing split-zoning on the subject property limits development to the exact footprint of an existing aging building, thereby precluding the practical redevelopment of the site with a more efficient design.

The Zone Boundary Adjustment has been made contingent upon compliance with conditions imposed herein. Such limitations are necessary to protect the best interest of public necessity and convenience and assure a development and improvements more compatible with the surrounding properties. The applicant

will continue to meet all required dedications and improvements, determined necessary by the City Engineer, to facilitate circulation and access, and to ensure adequate infrastructure is provided relative to the zone boundary adjustment.

The Zone Boundary Adjustment proposes the equal exchange of approximately 3,757 square feet between the C1-zoned and P-zoned portions of the parcel to permit the construction of a new, approximately 1,479 square-foot Starbucks coffee-shop with drive-through, signage, patio, trash enclosure and drive-through lanes. The result will be a reconfiguration of the zones with no change in the existing square-footage devoted to each of the respective zones. The proposed Starbucks will be located wholly within the C1 Zone, with parking and walkways located within the P Zone. The Zone Boundary Adjustment will not result in an increase in the allocated areas of the C1 or P Zones.

The proposed project will retain the parking along the western property line due to an existing cross access easement between the parcel to the north (currently operated as a McDonalds drive through fast food restaurant), a utility easement along the western property line, and street utilities. Additionally, the site will be re-graded, with public sidewalk being brought up to current Department of Transportation (DOT) requirements, as well as necessary highway dedication.

The proposed building is positioned towards the front of the property, with a pedestrian friendly/accessible walk-up window to place orders or pick up to-go orders, placed ahead of time. The building will also feature an art mural on the south façade facing Victory Boulevard to help ensure the building elevates the aesthetic of that corner at Victory Boulevard and Platt Avenue.

The Zone Boundary Adjustment allows for the drive through lane access point to be located in the interior of the lot. This ensures there is no spill over onto Victory Boulevard and that there would be no impediment to the flow of traffic on Victory Boulevard. This also allows for the order point to be located along the eastern property line, where the speaker box will feature an Automated Volume Control that automatically adjusts the volume according to ambient sound levels. This ensures that no excess noise is generated from the order point, and with the hours of operation, ensures this will be compatible and not create disturbances to any of the surrounding uses. The direction of the drive through lane also helps ensure lights from vehicles do not shine directly onto any residences.

Furthermore, the development will remove both cell towers currently operating on the site and their corresponding equipment. In order to promote safety on the site, the project will construct two tall gates located on the eastern side of the property. A 5-foot clearance between the subject property and the shops to the East will be created, and also ensure that site lighting will not spillover to all neighboring sites.

As such, reconfiguring the zone boundary between the QC1-1VL and P-1VL Zones in the manner shown on the attached Exhibit A will establish a logical zoning pattern that is consistent with good zoning practice.

### **APPEAL PERIOD - EFFECTIVE DATE**

This grant is not a permit or license and any permits and/or licenses required by law must be obtained from the proper public agency. If any Condition of this grant is violated or not complied with, then the applicant or their successor in interest may be prosecuted for violating these Conditions the same as for any violation of the requirements contained in the Los Angeles Municipal Code (LAMC).

This determination will become effective after the end of appeal period date on the first page of this document, unless an appeal is filed with the Department of City Planning. An appeal application must be submitted and paid for before 4:30 PM (PST) on the final day to appeal the determination. Should the final day fall on a weekend or legal City holiday, the time for filing an appeal shall be extended to 4:30 PM (PST) on the next succeeding working day. Appeals should be filed early to ensure the Development Services Center (DSC) staff has adequate time to review and accept the documents, and to allow appellants time to submit payment.

An appeal may be filed utilizing the following options:

**Online Application System (OAS):** The OAS (<https://planning.lacity.org/oas>) allows entitlement appeals to be submitted entirely electronically by allowing an appellant to fill out and submit an appeal application online directly to City Planning's DSC, and submit fee payment by credit card or e-check.

**Drop off at DSC.** Appeals of this determination can be submitted in-person at the Metro or Van Nuys DSC locations, and payment can be made by credit card or check. City Planning has established drop-off areas at the DSCs with physical boxes where appellants can drop off appeal applications; alternatively, appeal applications can be filed with staff at DSC public counters. Appeal applications must be on the prescribed forms, and accompanied by the required fee and a copy of the determination letter. Appeal applications shall be received by the DSC public counter and paid for on or before the above date or the appeal will not be accepted.

Forms are available online at <http://planning.lacity.org/development-services/forms>.  
Public offices are located at:

Metro DSC  
(213) 482-7077  
201 N. Figueroa Street  
Los Angeles, CA 90012  
[planning.figcounter@lacity.org](mailto:planning.figcounter@lacity.org)

Van Nuys DSC  
(818) 374-5050  
6262 Van Nuys  
Boulevard  
Van Nuys, CA 91401  
[planning.mbc2@lacity.org](mailto:planning.mbc2@lacity.org)

West Los Angeles DSC  
(CURRENTLY CLOSED)  
(310) 231-2901  
1828 Sawtelle Boulevard  
West Los Angeles, CA  
90025  
[planning.westla@lacity.org](mailto:planning.westla@lacity.org)

City Planning staff may follow up with the appellant via email and/or phone if there are any questions or missing materials in the appeal submission, to ensure that the appeal package is complete and meets the applicable LAMC provisions.

If you seek judicial review of any decision of the City pursuant to California Code of Civil Procedure Section 1094.5, the petition for writ of mandate pursuant to that section must be filed no later than the 90th day following the date on which the City's decision became final pursuant to California Code of Civil Procedure Section 1094.6. There may be other time limits which also affect your ability to seek judicial review.

Verification of condition compliance with building plans and/or building permit applications are done at the City Planning Metro or Valley DSC locations. An in-person or virtual appointment for Condition Clearance can be made through the City's BuildLA portal ([appointments.lacity.org](http://appointments.lacity.org)). The applicant is further advised to notify any consultant representing you of this requirement as well.



QR Code to  
Online Appeal  
Filing



QR Code to  
Forms for In-  
Person Appeal  
Filing



QR Code to  
BuildLA  
Appointment  
Portal for  
Condition  
Clearance

Inquiries regarding this matter shall be directed to Stephanie Escobar, Planning Staff for the Department of City Planning at (213) 978-1492.



HENRY CHU  
Associate Zoning Administrator

cc: Councilmember John Lee  
Twelfth Council District  
Adjoining Property Owners



# PLANT LEGEND

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE
	CHLOPSIS LINEARIS BURGUNDY	DESERT WILLOW	24" BOX STD
	ABUTILON MARIANA	STRAWBERRY TREE	36" BOX MULTI
	ARKANSANSIA DESERT MUSSUMBI	FALO VERDE	36" BOX MULTI
	DIANELLA TASMANNICA VAREGATA	VAREGATED FLAX LILY	5 GAL
	BOUGAINVILLEA DARY VICTORIA	VAREGATED BOUGAINVILLEA	5 GAL
	MYRTUS COMMUNIS COMPACTA	COMPACT MYRTLE	5 GAL
	PITOSPORUM WHITELESI DWARF	DWARF MOCK ORANGE	5 GAL
	ELAGANIS PUNGENS	SILVERBERRY	5 GAL
	AGAVE ATTENUATA	AGAVE	5 GAL
	CALLISTEMON LITTLE JOHN	DWARF BOTTLEBRUSH	5 GAL
	DIANELLA REVOLUTA LITTLE REV	LITTLE REV FLAX LILY	5 GAL
	LOMANDORA LONGIFOLIA BREZZE	DWARF NAT RUSH	5 GAL
	HAGEDBERGIA VAPPY WANDERER	PURPLE LIAC VINE	5 GAL

**IRRIGATION NOTE:**  
 FUTURE IRRIGATION PLAN WILL MEET DESIGN STANDARD 1001  
 BY USING AN AUTOMATIC IRRIGATION SYSTEM  
 CAPABLE OF COMMUNICATING WITH THE WEATHER, SATELLITES,  
 THE IRRIGATION WILL BE A DRIP IRRIGATION SYSTEM AND IT WILL  
 COMPLY WITH ALL CITY AND STATE REQUIREMENTS, INCLUDING  
 BUT NOT LIMITED TO USING 1/2" E Z A 250A (10.4)

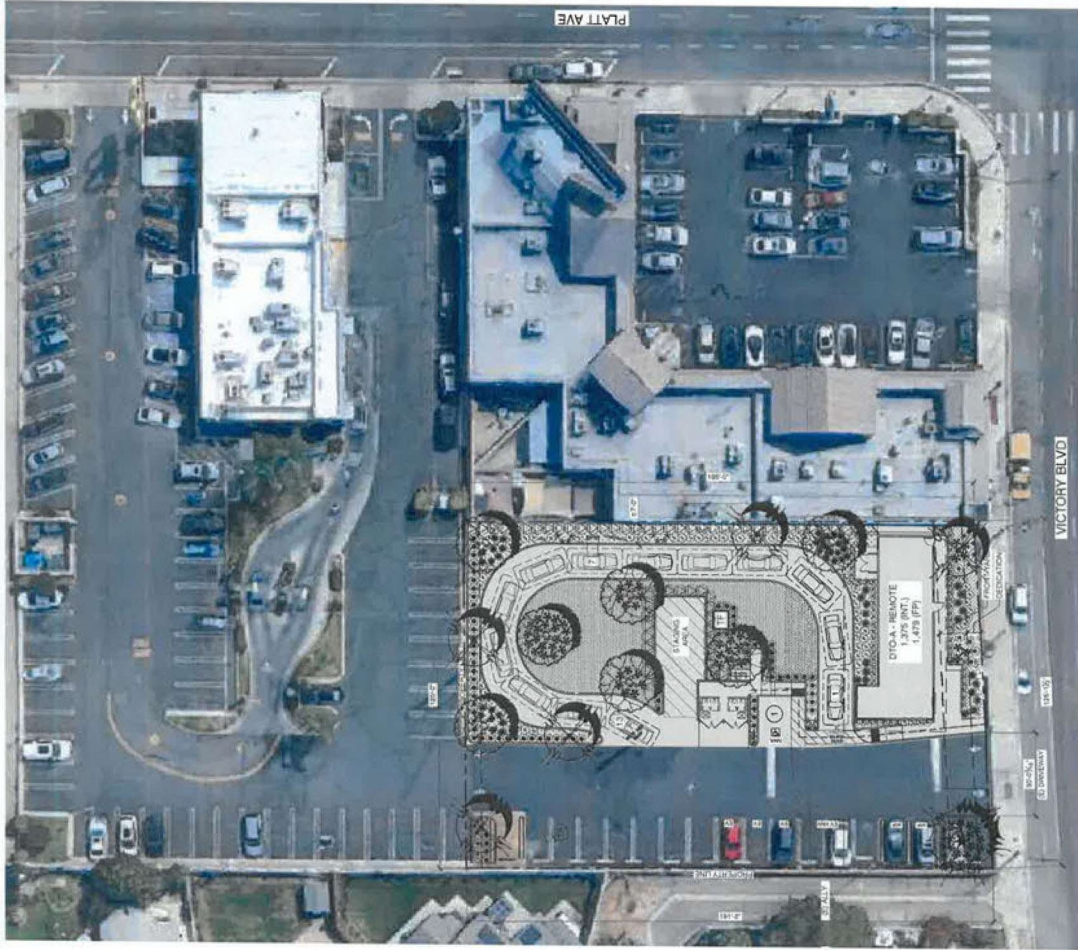


DATE: 10/16/2024  
 MCL JOB #: 2116246

DATE: \_\_\_\_\_

REVISIONS: \_\_\_\_\_

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## DRIVE-THRU COFFEE SHOP VICTORY & PLATT

23717 Victory Blvd.  
 Los Angeles, CA 91307



**PROJECT DIRECTORY**  
**APPLICANT:** STARBUCKS  
 2811 VAN WATVILE BOOTH  
 1400 S. GARDEN ST.  
 LOS ANGELES, CA 90007  
**ARCHITECT:** MCG ARCHITECTURE  
 18150 VAN WATVILE BOOTH  
 LOS ANGELES, CA 90007  
 TEL: (310) 552-5522  
 FAX: (310) 552-5522  
 WWW.MCGARCHITECTURE.COM

### SITE DATA

A.P.N. NO. 2038401004  
 OFFERS ZONING: P.U.M. (D)1-H/L  
 GENERAL PLAN: COMMUNITY COMMERCIAL  
 SITE AREA: 5.83 ACRES (253,283 S.F.)  
 PROPOSED USE: RESTAURANT  
 CONSTRUCTION TYPE: DRIVE-THRU WITH MAKEUP (DRIVE THROUGH)  
 OCCUPANCY: 1977A, B  
 BUILDING HEIGHT: 14 FT 6 IN  
 PARCELS: 14 PARCELS  
 REQUIRED (1000 S.F.) IN 1977 ARE: 5 SPACES  
 PROVIDED: 18 SPACES  
 ACCESSIBLE SPACES PROVIDED: 1 SPACE  
 BY CHAIRS SPACES REQUIRED: 4 SPACES  
 BY TABLES SPACES REQUIRED: 4 SPACES  
**PROJECT SCOPE OF WORK:**  
 THE PROJECT WILL CONSIDER AN EXISTING SINGLE-STORY 2,500 S.F. BUILDING FOR CONVERSION AND ADAPTATION TO A DRIVE-THRU COFFEE SHOP AND MAKEUP (DRIVE THROUGH) RESTAURANT BUILDING WITH A DRIVE-THRU LANE AND 18 DRIVE-THRU SPACES. THE PROJECT WILL ALSO CONSIDER THE REMOVAL OF EXISTING DRIVE-THRU SPACES AND THE RECONSTRUCTION OF THE DRIVE-THRU SPACES. NO MAJOR GRADING OR SITE ALTERATIONS ARE ANTICIPATED FOR THIS DEVELOPMENT.  
 BUILDING PERMITS BY OTHERS UNDER SEPARATE PERMIT

### LEGEND:

UNDESIGNED AREA

**EXHIBIT "A"**  
 Page No. 2 of 2  
 Case No. CA-2024-5022-MC-24  
 (Handwritten initials)



SHEET TITLE: LANDSCAPE PLAN  
 SHEET NUMBER: LP-1



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 PHOENIX  
 ORLANDO  
 IRVINE  
 GLENORA  
 DENVER  
 CLEVELAND

# TREES



CHLOPSIS L. 'BURGUNDY' DESERT WILLOW  
FORM OF ACTUAL SPECIMEN



ARBUTUS MARINA STRAWBERRY TREE



PARKINSONIA DESERT MUSEUM PALO VERDE



HAZENBERGIA 'HAPPY WANDERER' PURPLE ILAC VINE

# SHRUBS



AGAVE ATENUATA AGAVE



BOUGAINVILLEA 'BABY VICTORIA'



CALLISTEMON LITTLE JOHN DINNER BOTTLEBRUSH



DIANELLA REVOLUTA LITTLE REV



DIANELLA TASMANICA 'VAR. ...'



ELAEAGNUS FUNGENS SILVERBERRY



LOWMANOKA L. 'BREEZE' DWARF MAT RUSH



MERTENSIA COMMUNIS 'COMPACTA' COMPACT MYRTLE



OULE SHIRUB



FITTOSPORUM WHEELERS DWARF WHEELERS MOCK ORANGE

DATE	10/15/2024
MCQ JOB #	23.526109
DATE	
REVISIONS	

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## PLANT IMAGES



## DRIVE-THRU COFFEE SHOP VICTORY & PLATT

23717 Victory Blvd.  
Los Angeles, CA 91307



SHEET TITLE: PLANT IMAGES  
SHEET NUMBER: LP-2



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EXHIBIT "A"  
Page No. 3 of 4  
Case No. 2024-5222-012  
2024

**MATERIAL LEGEND:**

**MATERIALS**

1. METAL PANEL
2. METAL CANOPY
3. METAL ROOF
4. ALUMINUM WINDOW FRAME
5. ALUMINUM WINDOW GLASS
6. METAL SCREEN WIDE KNEE PANEL
7. ALUMINUM WIDE KNEE PANEL
8. ALUMINUM WINDOW FRAME
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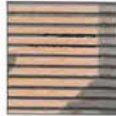
**COLORS**



A. SOLID SURFACE COBAM MANUFACTURER DESIGNER WHITE



B. METAL PANEL ACCENT LABEL LOGO/EMBLEM MANUFACTURER: PFD MANUFACTURING CO. COLOR NAME: DARK LEONARD VLS

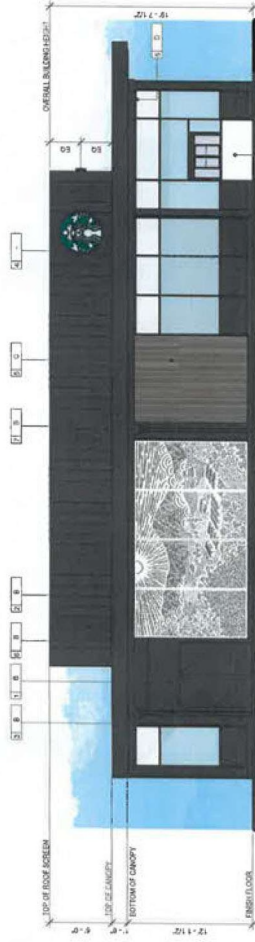


C. WALL PANEL WOOD GRAIN



D. WINDOW FRAME MATERIAL BY NOSHIA

**EXHIBIT "A"**  
 Page No. 4 of 14  
 Case No. 24-2024-5222-002-004  
 AP

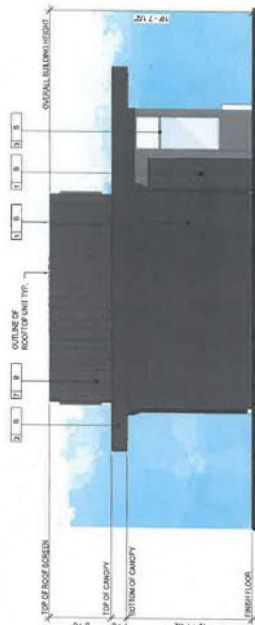


3 NORTH ELEVATION  
 SCALE: 3/16" = 1'-0"

**WINDOW TRANSMITTANCE ANALYSIS**  
 OVERALL ELEVATION SF: 44-174 SF  
 TRANSPARENT WINDOW SF: 44-45 SF  
 66 SF / 1274 SF = 5% WINDOW TRANSPARENCY



1 SOUTH ELEVATION  
 SCALE: 3/16" = 1'-0"



4 EAST ELEVATION  
 SCALE: 3/16" = 1'-0"



2 WEST ELEVATION  
 SCALE: 3/16" = 1'-0"

**DRIVE-THRU COFFEE SHOP AT  
 VICTORY & PRATT**

23717 VICTORY BLVD.  
 LOS ANGELES, CA 91307

SHEET TITLE:  
 EXTERIOR ELEVATIONS

SHEET NUMBER:

EL-1

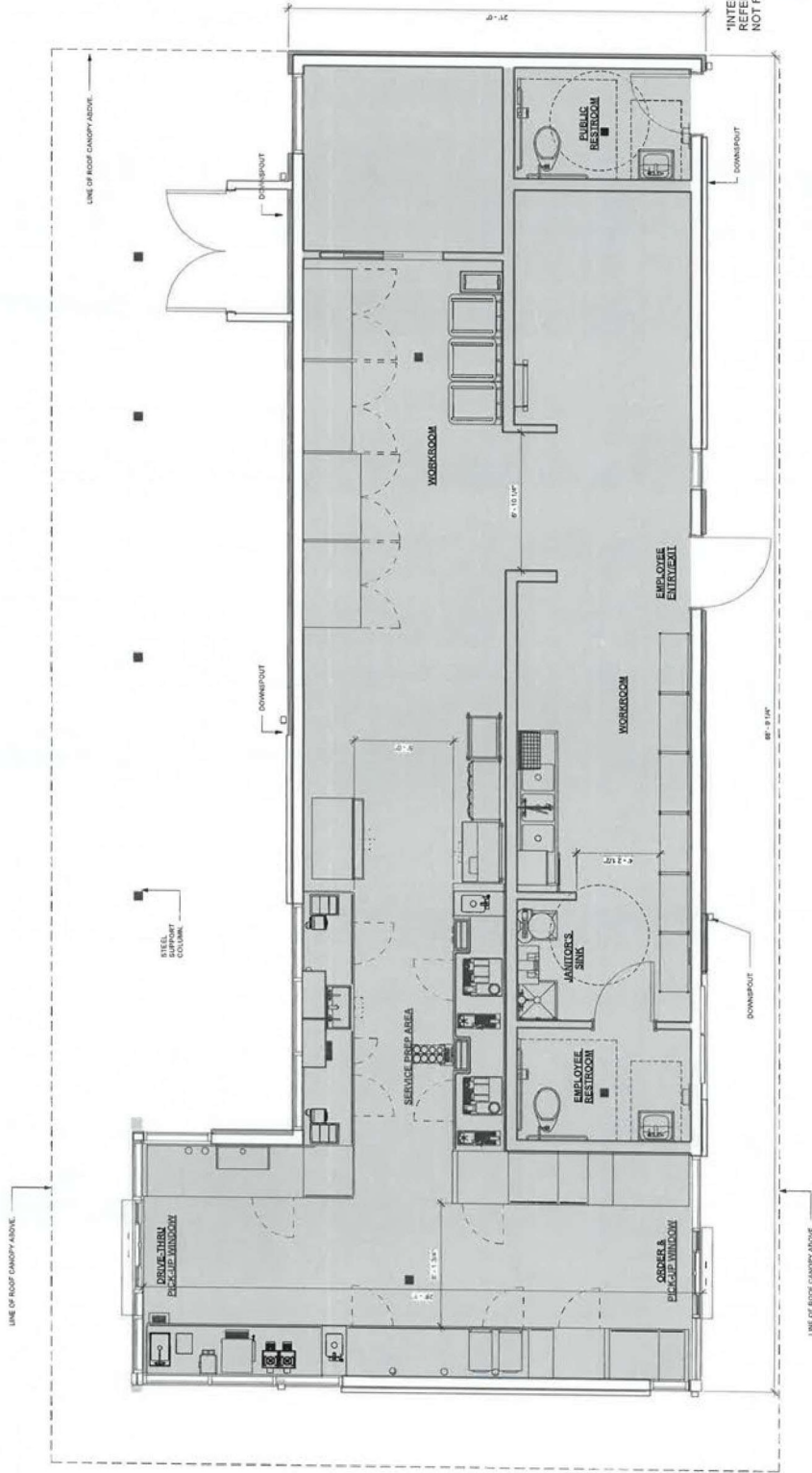


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 MOJ JOB #: 2326108

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 Page No. 5 of 14  
 Case No. 22-5022-00288  
 (A)

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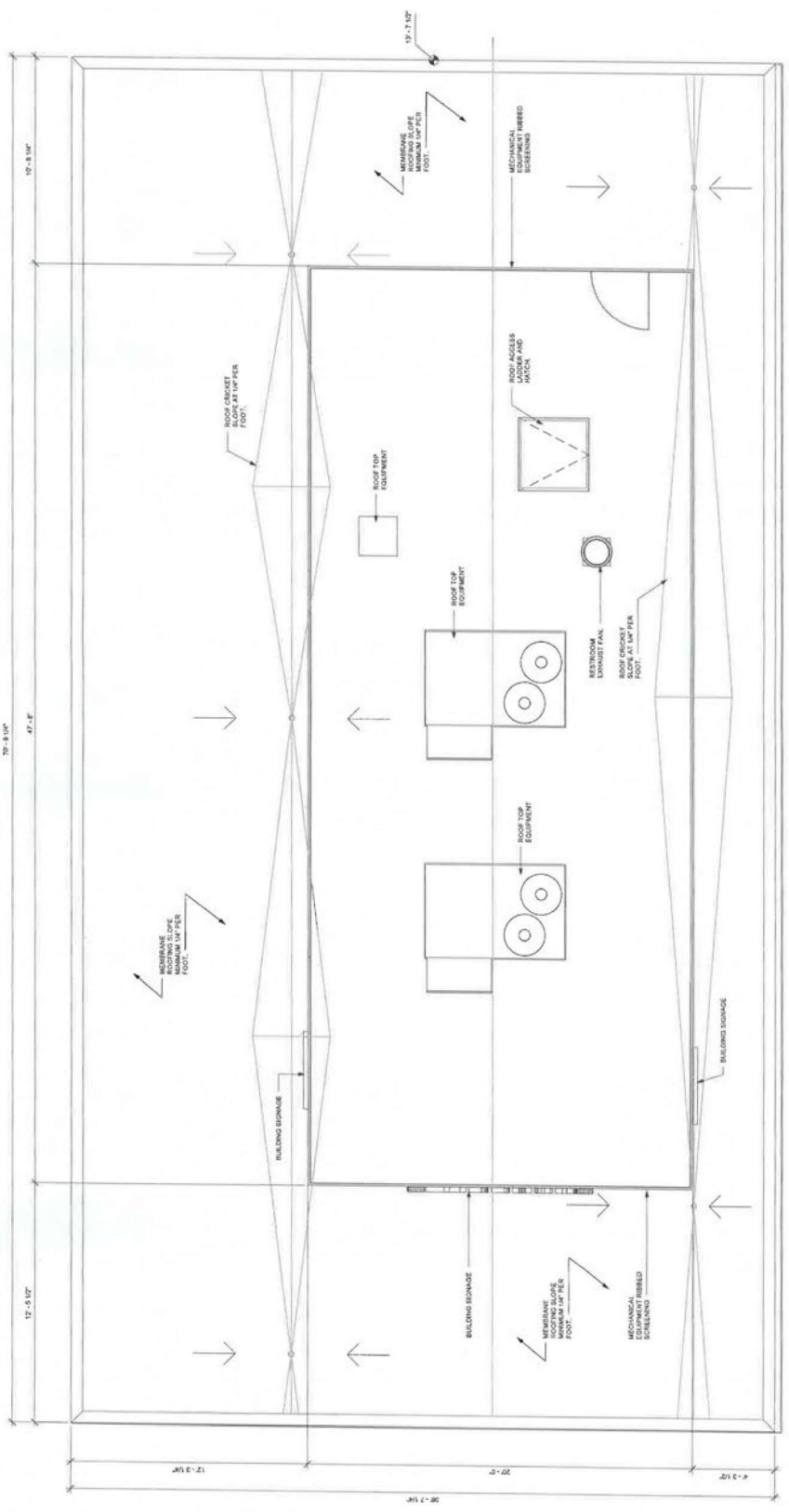
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SHEET TITLE:  
**BUILDING FLOOR PLAN**



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FP1



**EXHIBIT "A"**  
 Page No. 6 of 14  
 Case No. 2A-2004-5222-MS-20  
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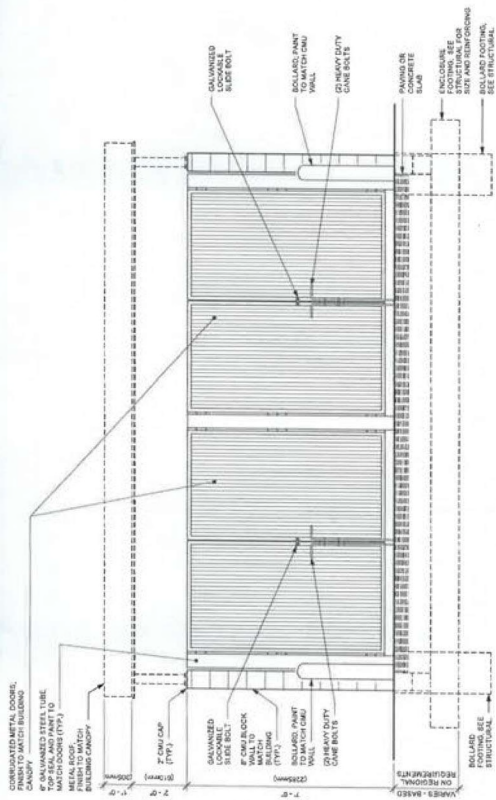
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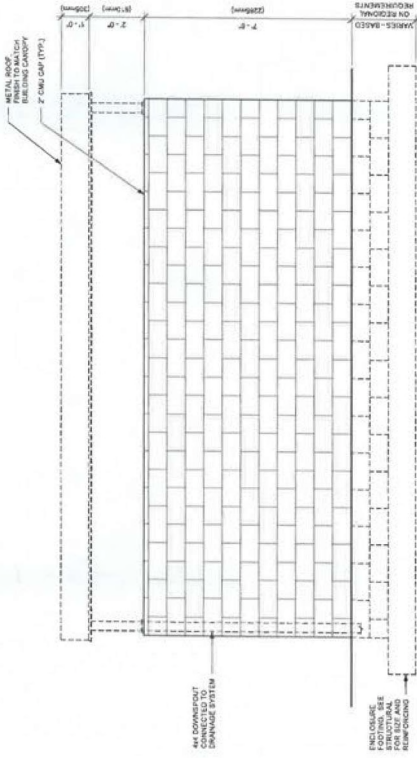
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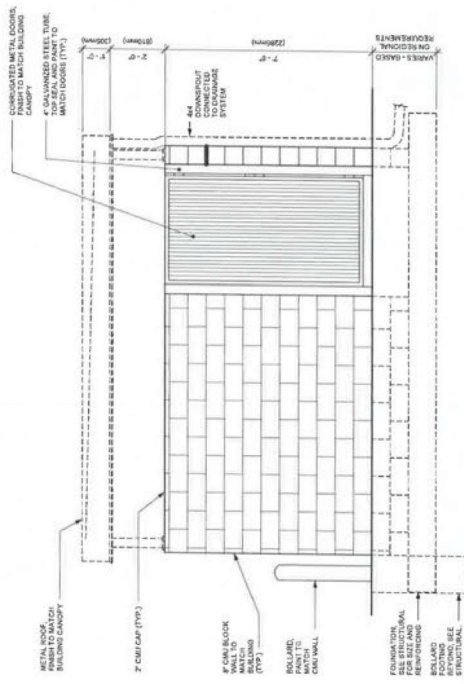
SHEET TITLE: **BUILDING ROOF PLAN**  
 SHEET NUMBER: **RP-1**



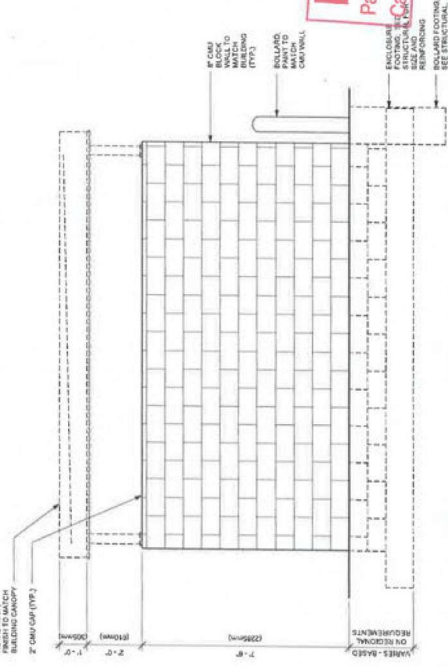
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3 BACK ELEVATION  
SCALE: 1/2" = 1'-0"



1 SIDE ELEVATION "A"  
SCALE: 1/2" = 1'-0"



2 SIDE ELEVATION "B"  
SCALE: 1/2" = 1'-0"

**EXHIBIT "A"**  
Page No. 1 of 14  
Case No. 24-241-San-Mig



**DRIVE-THRU COFFEE SHOP AT  
VICTORY & PRATT**

23717 VICTORY BLVD.  
LOS ANGELES, CA 91307

SHEET TITLE: TRASH ENCLOSURE  
ELEVATIONS  
SHEET NUMBER: TE-2

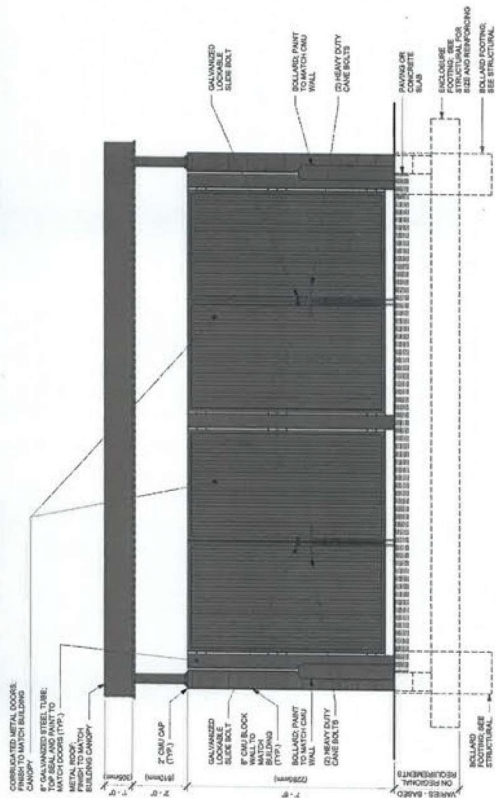


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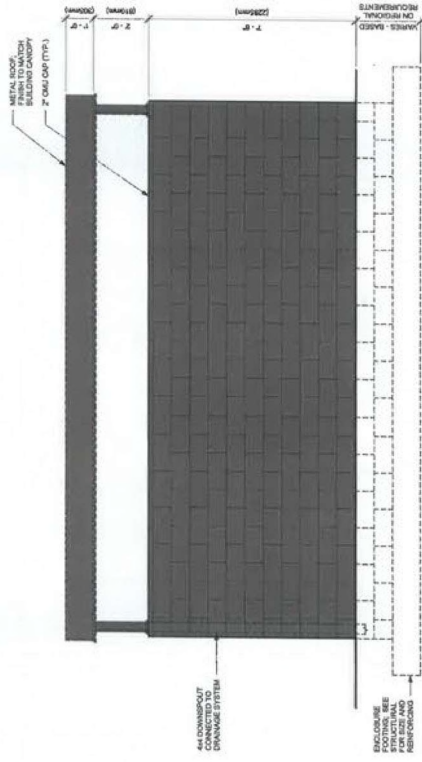
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DATE: \_\_\_\_\_ REVISIONS: \_\_\_\_\_

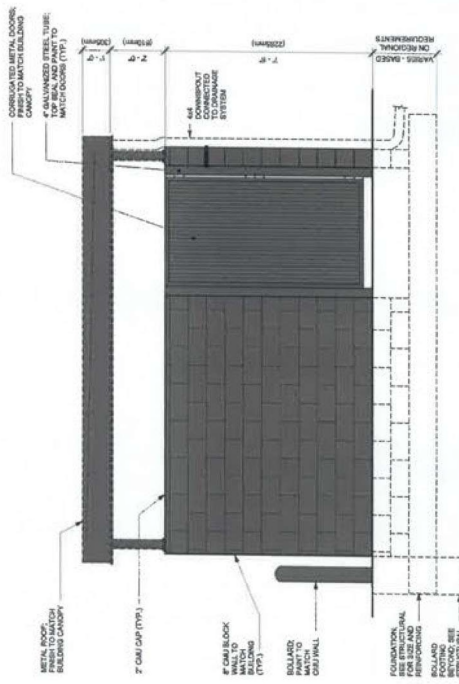
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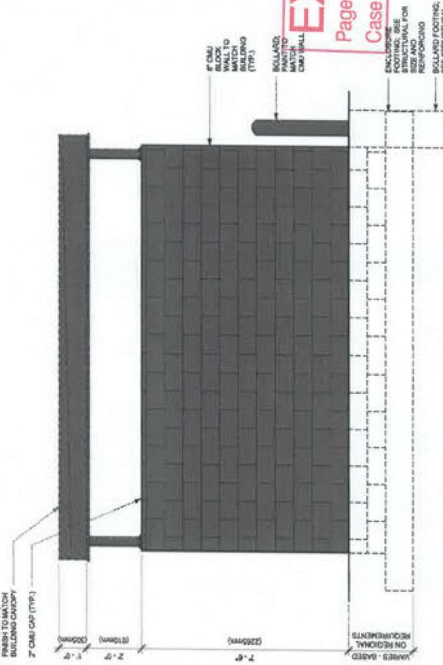
④ FRONT ELEVATION  
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③ BACK ELEVATION  
SCALE: 1/2" = 1'-0"



① SIDE ELEVATION "A"  
SCALE: 1/2" = 1'-0"



② SIDE ELEVATION "B"  
SCALE: 1/2" = 1'-0"

**EXHIBIT "A"**  
Page No. **8** of **14**  
Case No. **2A-2024-522-Cap 204**

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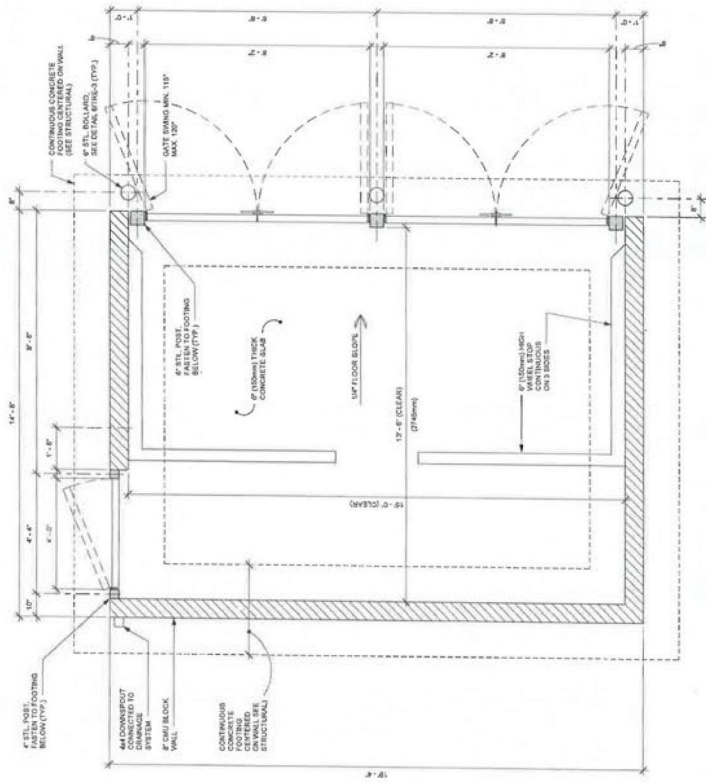
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SHEET TITLE:  
**TRASH ENCLOSURE  
ELEVATIONS**

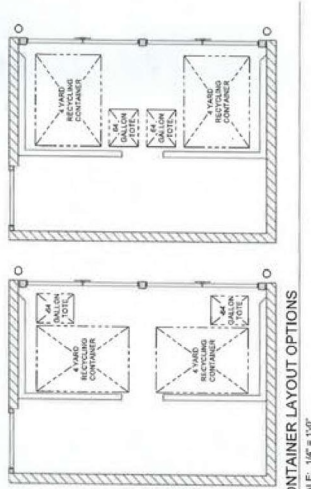
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DATE:	REVISIONS

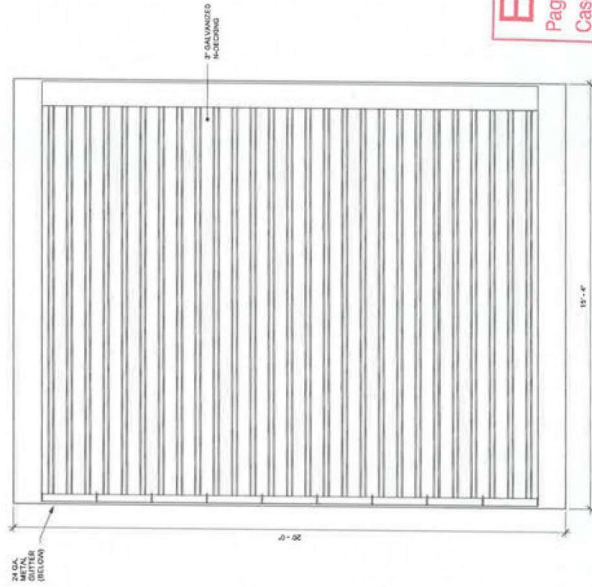
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1 FLOOR PLAN  
SCALE: 1/2" = 1'-0"



3 CONTAINER LAYOUT OPTIONS  
SCALE: 1/4" = 1'-0"



2 ROOF PLAN  
SCALE: 1/2" = 1'-0"

EXHIBIT "A"  
Page No. 9 of 14  
Case No. 20-24502-003-208

DRIVE-THRU COFFEE SHOP AT  
VICTORY & PRATT

23717 VICTORY BLVD.  
LOS ANGELES, CA 91307

DATE: 02/14/20  
MCO JOB #: 2308103

DATE: \_\_\_\_\_ REVISIONS: \_\_\_\_\_

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SHEET TITLE: TRASH ENCLOSURE PLAN  
SHEET NUMBER: TE-1

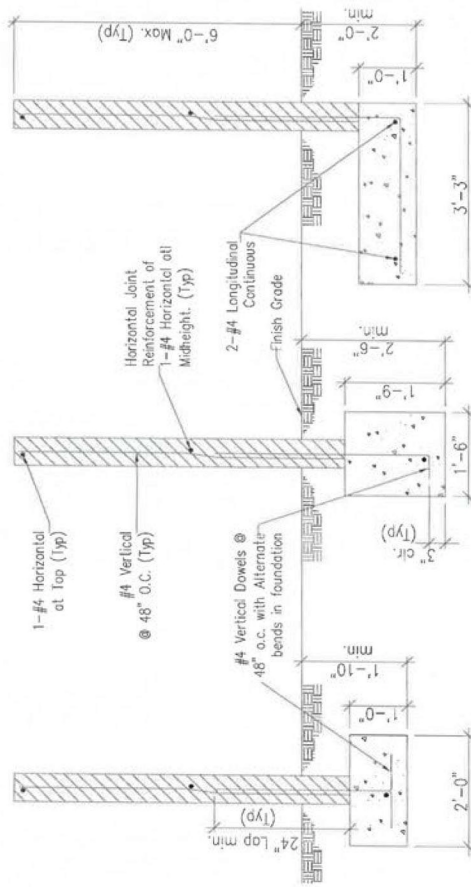








**6" CONCRETE BLOCK MASONRY WALL DETAIL  
 (6'-0" HEIGHT MAXIMUM)**



**GENERAL SPECIFICATIONS:**

1. Concrete mix for footing to be 1 part cement to 2.5 parts sand to 3.5 parts gravel with a maximum of 7.5 gallons of water per sack of cement/ Minimum  $f_c = 2,500$  psi.
2. Concrete block units shall conform to ASTM C90.
3. Reinforcing steel to be deformed and conformed to ASTM Standard A615 Grade 40 or Grade 60
4. Rebar shall be centered in the concrete block cell in which it is located.
5. Concrete block units shall be placed in a running bond with head joints in successive courses horizontally offset at least one-quarter the unit height.
6. Concrete block units to have vertical continuity of cells unobstructed. All cells containing reinforcing shall be solid grouted (vertical and horizontal reinforcement). Cells not containing reinforcing shall not be grouted.
7. Mortar mix for concrete to be 1 part cement to 3 parts sand to 3 parts damp loose sand.
8. Grout mix for concrete block wall to be 1 part cement to 3 parts sand to which may be added not more than 0.1 part lime. Sufficient water to be added to produce consistency for pouring without segregation of constituents. The grout may contain 2 parts pea gravel, maximum size 3/8".
9. Block fence may be placed in the center of the "T" type foundation and at either edge to "L" type foundation.
10. A first inspection is required when trenches are ready for concrete and all required steel is tied in place. A second inspection is required when first horizontal bar and all vertical bars are in place but not grouted.
11. Foundation must be poured against undisturbed soil with no appreciable slope of sidewalls on all types of foundation.
12. Masonry or concrete fences over 3'-6" in height measured from adjacent grade shall require a building permit.
13. Height of fence shall comply with all provisions of the zoning code.

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**EXHIBIT "A"**  
 Page No. 14 of 16  
 Case No. 21-21-5922-CM2-28A

*DR*

**DEPARTMENT OF  
CITY PLANNING  
APPLICATION**

# **ENVIRONMENTAL REPORT**

# **PLOT PLANS**



**PROJECT DIRECTORY**  
 APPLICANT: STARBUCKS  
 240 LOMA AVENUE SOUTH  
 SAN FRANCISCO, CA 94111  
 TEL: (415) 435-2818  
 ARCHITECT: MCG ARCHITECTURE  
 25 EAST 19TH AVENUE PARKWAY, SUITE 600  
 DENVER, CO 80202  
 TEL: (303) 733-0278  
 FAX: (303) 733-0279  
 WWW.MCGARCHITECTURE.COM

**SITE DATA**  
 A.P.N. NO.: 2030901000  
 EXISTING ZONING: P-UL, UCH-15  
 GENERAL PLAN: COMMUNITY COMMERCIAL  
 SITE AREA: 0.55 ACRES (0.6000 A.P.)  
 PROPOSED USE: RESTAURANT  
 CONSTRUCTION TYPE: VES  
 BUILDING HEIGHT: 19 FT 7"  
 OCCUPANCY: B  
 BUILDING AREA: 1,475 SF  
 PARKING: REQUIRED (MINIMUM) 11 SPACES, 15 SPACES PROVIDED, 15 SPACES PROVIDED  
 ACCESSIBLE SPACES PROVIDED: 1 SPACE  
 BY CHANGES AND/OR REVISIONS: 0 SPACES  
 ACCESSIBLE SPACES REQUIRED: 0 SPACES

**PROJECT SCOPE OF WORK:**  
 THE PROJECT WILL DEMOLISH AN EXISTING SINGLE-STORY 2,300 SQUARE FOOT RESTAURANT BUILDING WITH A SINGLE-STORY DRIVE-THRU RESTAURANT BUILDING WITH A DRIVE-THRU WINDOW AND SITE SERVICE. NO MAJOR DRAINING OR SITE UTILITIES ARE ANTICIPATED FOR THIS DEVELOPMENT.  
 BUILDING GRANTED BY OTHER UNDER SEPARATE PERMIT

- KEY NOTES**
1. ACTIVATED SURFACE
  2. ACCESSIBLE PARKING SPACE
  3. ACCESSIBLE PARKING SPACE
  4. ACCESSIBLE PATH OF TRAVEL
  5. DRIVE-THRU WINDOW
  6. DRIVE-THRU FUNCTIONAL ZONE
  7. DRIVE-THRU WINDOW
  8. DRIVE-THRU WINDOW
  9. DRIVE-THRU WINDOW
  10. DRIVE-THRU WINDOW
  11. DRIVE-THRU WINDOW
  12. DRIVE-THRU WINDOW
  13. DRIVE-THRU WINDOW
  14. DRIVE-THRU WINDOW
  15. DRIVE-THRU WINDOW
  16. DRIVE-THRU WINDOW
  17. DRIVE-THRU WINDOW
  18. DRIVE-THRU WINDOW
  19. DRIVE-THRU WINDOW
  20. DRIVE-THRU WINDOW

**LEGEND**

UNDEVELOPED AREA



VICINITY MAP

SHEET TITLE: SITE PLAN

SHEET NUMBER: SP-1



**RECEIVED**  
 CITY OF LOS ANGELES  
 MAR 04 2025  
 CITY PLANNING DEPT.  
 ZONING ADMINISTRATION

**DRIVE-THRU COFFEE SHOP**  
**VICTORY & PLATT**  
 23717 Victory Blvd,  
 Los Angeles, CA 91307

SCALE: 1" = 20'  
 0' 10' 20' 40'

DATE: 02/12/2025  
 MCG JOB #: 23.026.05

DATE	REVISIONS

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**EXHIBIT A**  
 Page No. 1 of 1  
 Case No. 23.026.05-2025-CUP-20A

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# TREES



CHILOPSIS L. 'BURGUNDY'  
FLOWER COLOR  
FORM OF ACTUAL SPECIMEN  
DESERT WILLOW



ARBUTUS MARINA  
STRAWBERRY TREE



PALO VERDE  
PARKINSONIA DESERT MUSEUM

# VINE



HARDENBERGIA 'HAPPY WANDERER'  
PURPLE ILIAC VINE

# SHRUBS



AGAVE ATTENUATA  
AGAVE



BOUGAINVILLEA 'BABY VICTORIA'



CALISTEMON LITTLE JOHN DWARF BOTTLEBRUSH



DIANELLA REVOLUTA LITTLE REV



DIANELLA TASMANICA



ELEAGNUS PUNGENS  
SILVERBERRY



LOMANDRA L. 'BREEZE' DWARF MAT RUSH



MYRTUS COMMUNIS COMPACTA  
COMPACT MYRTLE



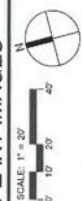
OLEA EUROPAEA LITTLE OLLIE



PITTOSPORUM WHEELERS DWARF  
WHEELERS' MCKCK ORANGE

EXHIBIT "A"  
Page No. 3 of 4  
Case No. 2024-5222-012  
JED

DATE	10/15/2024
MCQ JOB #	23-526109
DATE	
REVISIONS	



DRIVE-THRU COFFEE SHOP  
VICTORY & PLATT  
23717 Victory Blvd.  
Los Angeles, CA 91307



PLANT IMAGES  
SHEET NUMBER: LP-2  
SAN FRANCISCO  
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**MATERIAL LEGEND:**

**MATERIALS**

1. METAL PANEL
2. METAL CANOPY
3. METAL ROOF
4. ALUMINUM WINDOW FRAME
5. ALUMINUM WINDOW GLASS
6. METAL SCREEN WIDE KNEE PANEL
7. ALUMINUM WINDOW WIDE KNEE PANEL
8. ALUMINUM WINDOW WIDE KNEE PANEL
9. ALUMINUM WINDOW WIDE KNEE PANEL
10. ALUMINUM WINDOW WIDE KNEE PANEL

**COLORS**



- A. SOLID SURFACE  
CROWN  
MANUFACTURER: PPG  
DESIGNER: WHITE  
COLOR: [Swatch]



- B. SOLID SURFACE  
CROWN  
MANUFACTURER: PPG  
DESIGNER: WHITE  
COLOR: [Swatch]



- C. WALL PANEL  
WOOD GRAIN  
[Swatch]

- D. WINDOW FRAME  
BY: HOSHA  
[Swatch]

**EXHIBIT "A"**  
Page No. 4 of 14  
Case No. 24-2024-5222-002-204  
APD

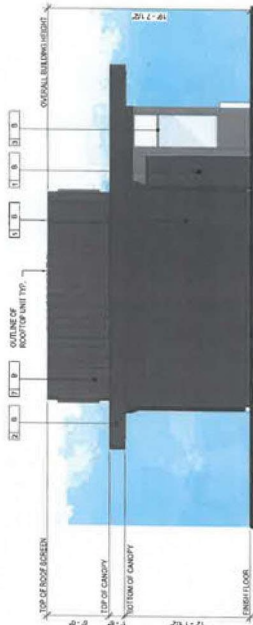


3 NORTH ELEVATION  
SCALE: 3/16" = 1'-0"

**WINDOW TRANSMITTANCE ANALYSIS**  
OVERALL ELEVATION SF: 44,174 SF  
TRANSPARENT WINDOW SF: 4645 SF  
66 SF 1/2" x 4" x 1/8" WINDOW TRANSPARENCY



1 SOUTH ELEVATION  
SCALE: 3/16" = 1'-0"



4 EAST ELEVATION  
SCALE: 3/16" = 1'-0"



2 WEST ELEVATION  
SCALE: 3/16" = 1'-0"

**DRIVE-THRU COFFEE SHOP AT  
VICTORY & PRATT**

23717 VICTORY BLVD.  
LOS ANGELES, CA 91307



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SHEET TITLE: EXTERIOR ELEVATIONS

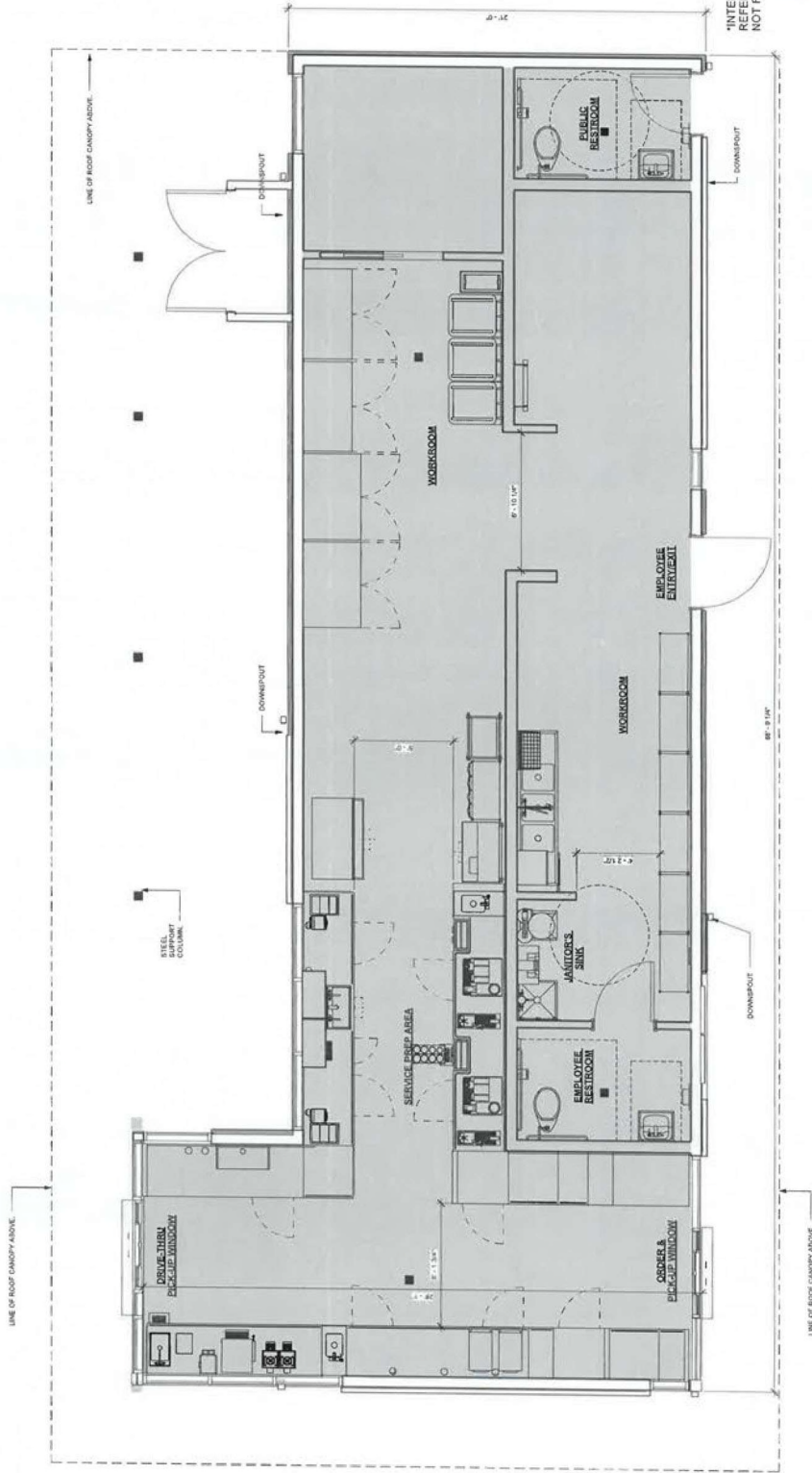
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EL-1

DATE: 02/16/25  
MOJ JOB #: 2326108

DATE: REVISIONS

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**EXHIBIT "A"**  
 Page No. 5 of 14  
 Case No. 22-5022-00288  
 (A)

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 LOS ANGELES, CA 91307

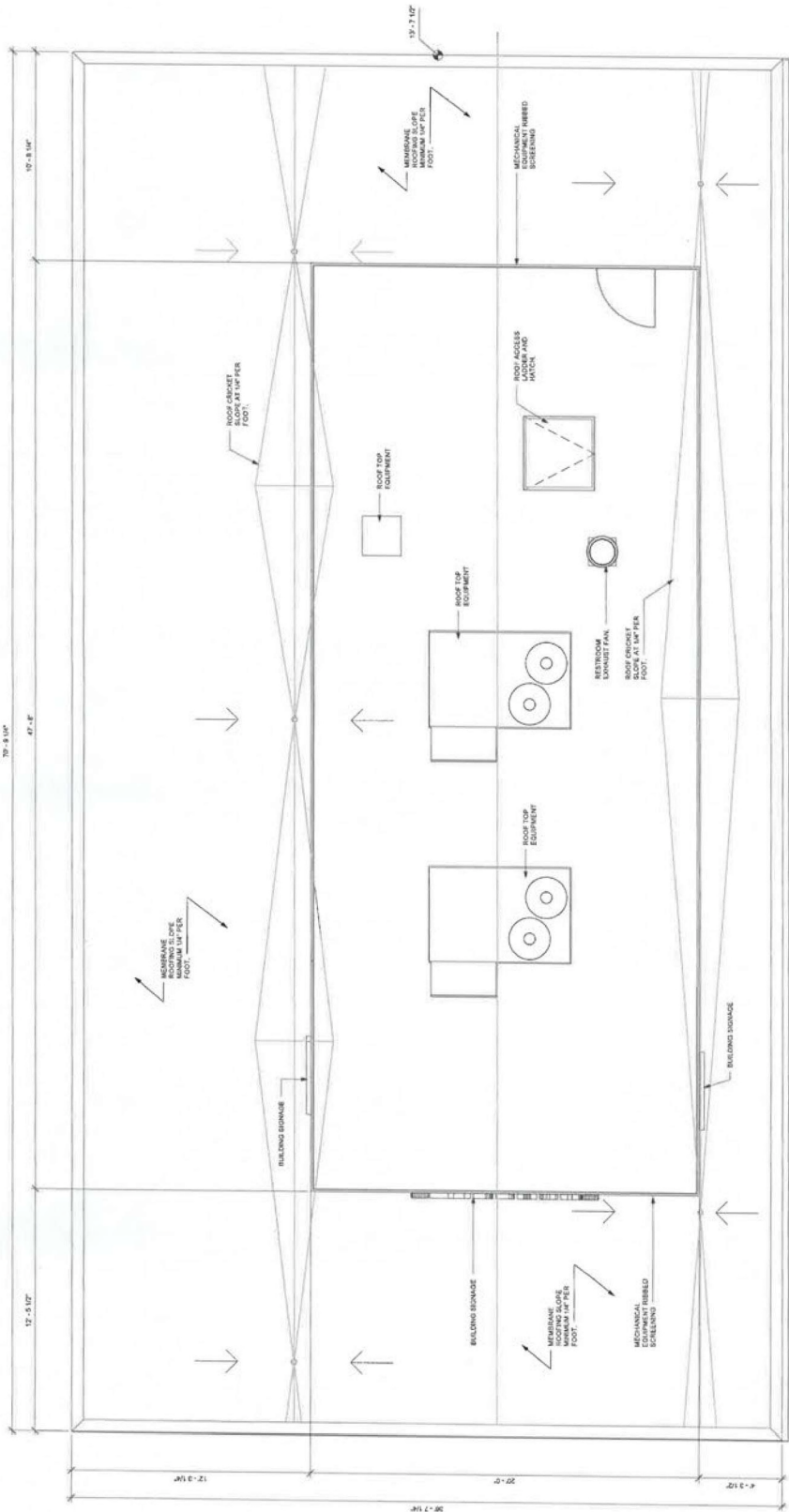


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MCO JOB #:	202020
DATE:	
REVISIONS:	

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SHEET TITLE: BUILDING FLOOR PLAN  
 SHEET NUMBER: FP1



**EXHIBIT "A"**  
 Page No. 6 of 14  
 Case No. 2A-2004-5222-408-204  
 AP

**DRIVE-THRU COFFEE SHOP AT  
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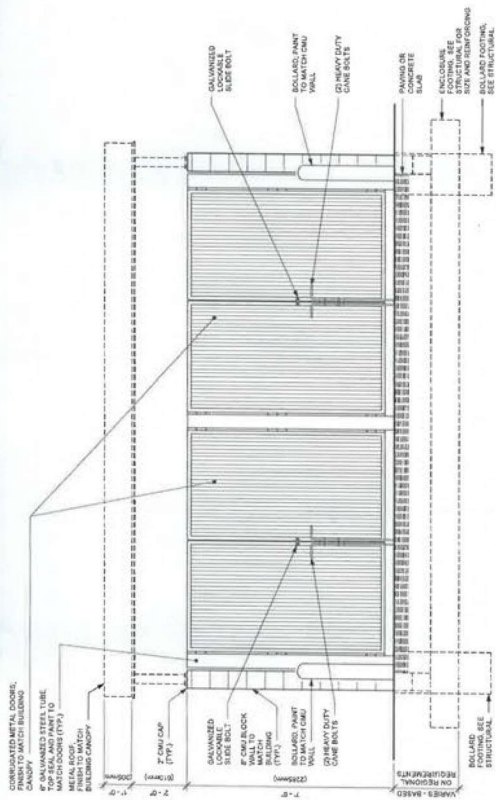
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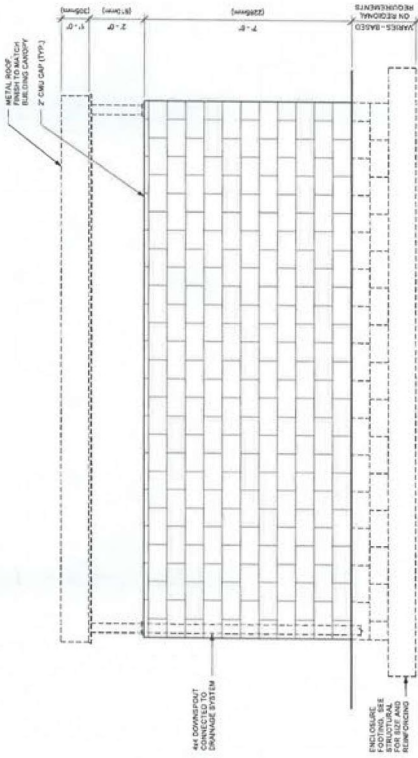


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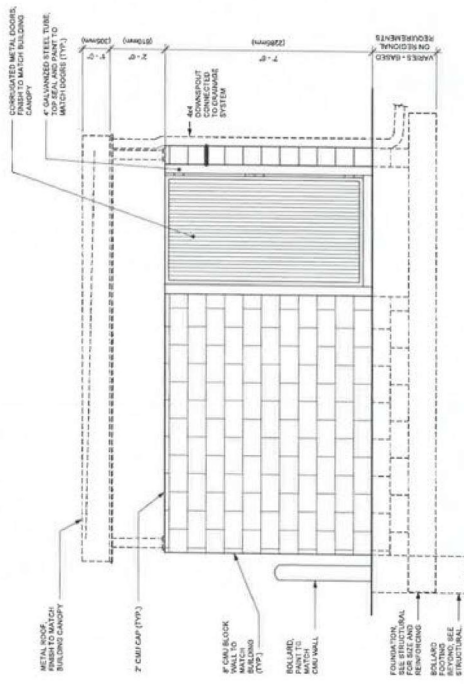
SHEET TITLE: **BUILDING ROOF PLAN**  
 SHEET NUMBER: **RP-1**



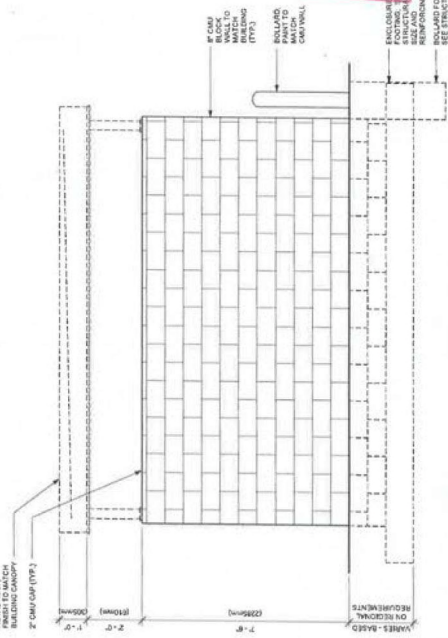
① FRONT ELEVATION  
SCALE: 1/2" = 1'-0"



③ BACK ELEVATION  
SCALE: 1/2" = 1'-0"



② SIDE ELEVATION "A"  
SCALE: 1/2" = 1'-0"



④ SIDE ELEVATION "B"  
SCALE: 1/2" = 1'-0"

**EXHIBIT "A"**  
Page No. 1 of 14  
Case No. 24-241-San-Mig



**DRIVE-THRU COFFEE SHOP AT  
VICTORY & PRATT**

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LOS ANGELES, CA 91307

SHEET TITLE: TRASH ENCLOSURE  
ELEVATIONS  
SHEET NUMBER: TE-2

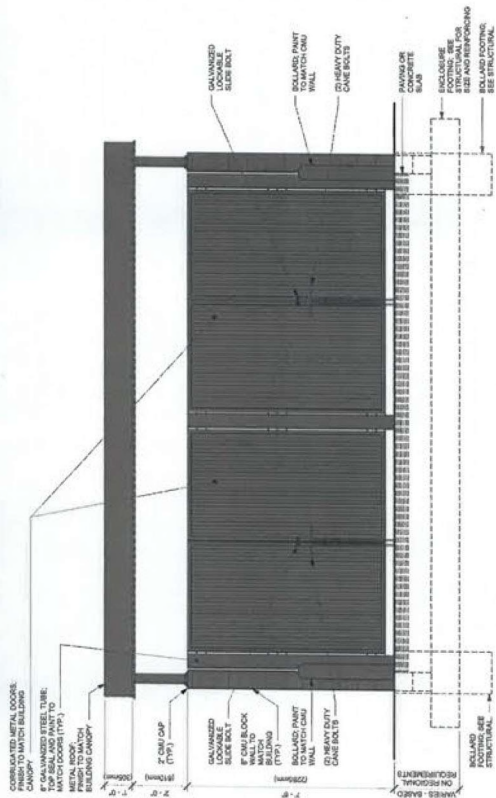


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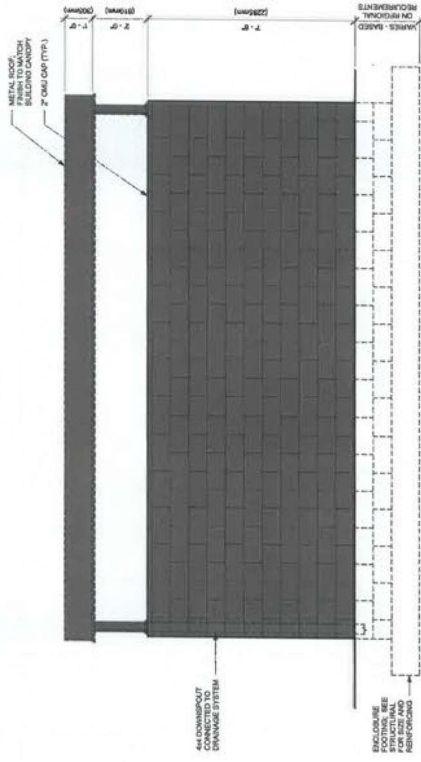
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DATE: REVISIONS

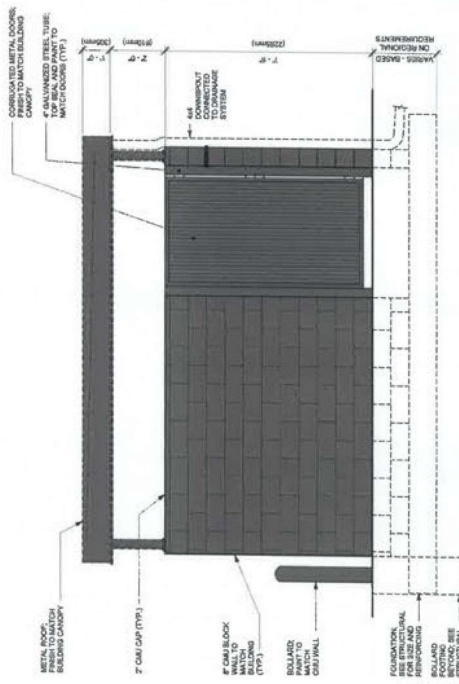
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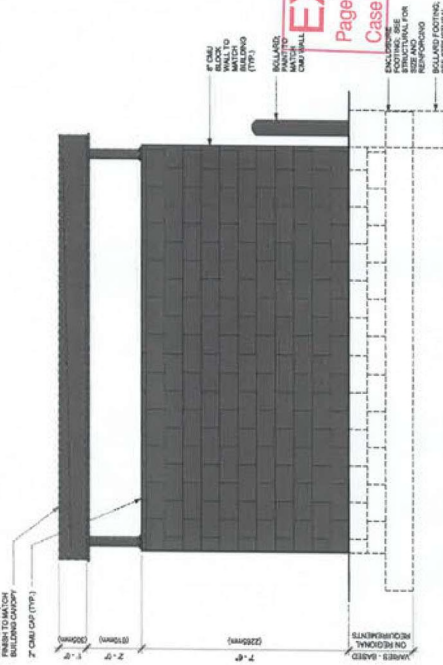
④ FRONT ELEVATION  
SCALE: 1/2" = 1'-0"



③ BACK ELEVATION  
SCALE: 1/2" = 1'-0"



① SIDE ELEVATION "A"  
SCALE: 1/2" = 1'-0"



② SIDE ELEVATION "B"  
SCALE: 1/2" = 1'-0"

EXHIBIT "A"  
Page No. 8 of 14  
Case No. 2A-2024-5222-Cap 204

RD

DRIVE-THRU COFFEE SHOP AT  
VICTORY & PRATT

23717 VICTORY BLVD.  
LOS ANGELES, CA 91307



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SHEET TITLE: TRASH ENCLOSURE ELEVATIONS  
SHEET NUMBER: TE-2

DATE:	CONTR:	REVISIONS:
MCO JOB #:	25026LOS	
DATE:		

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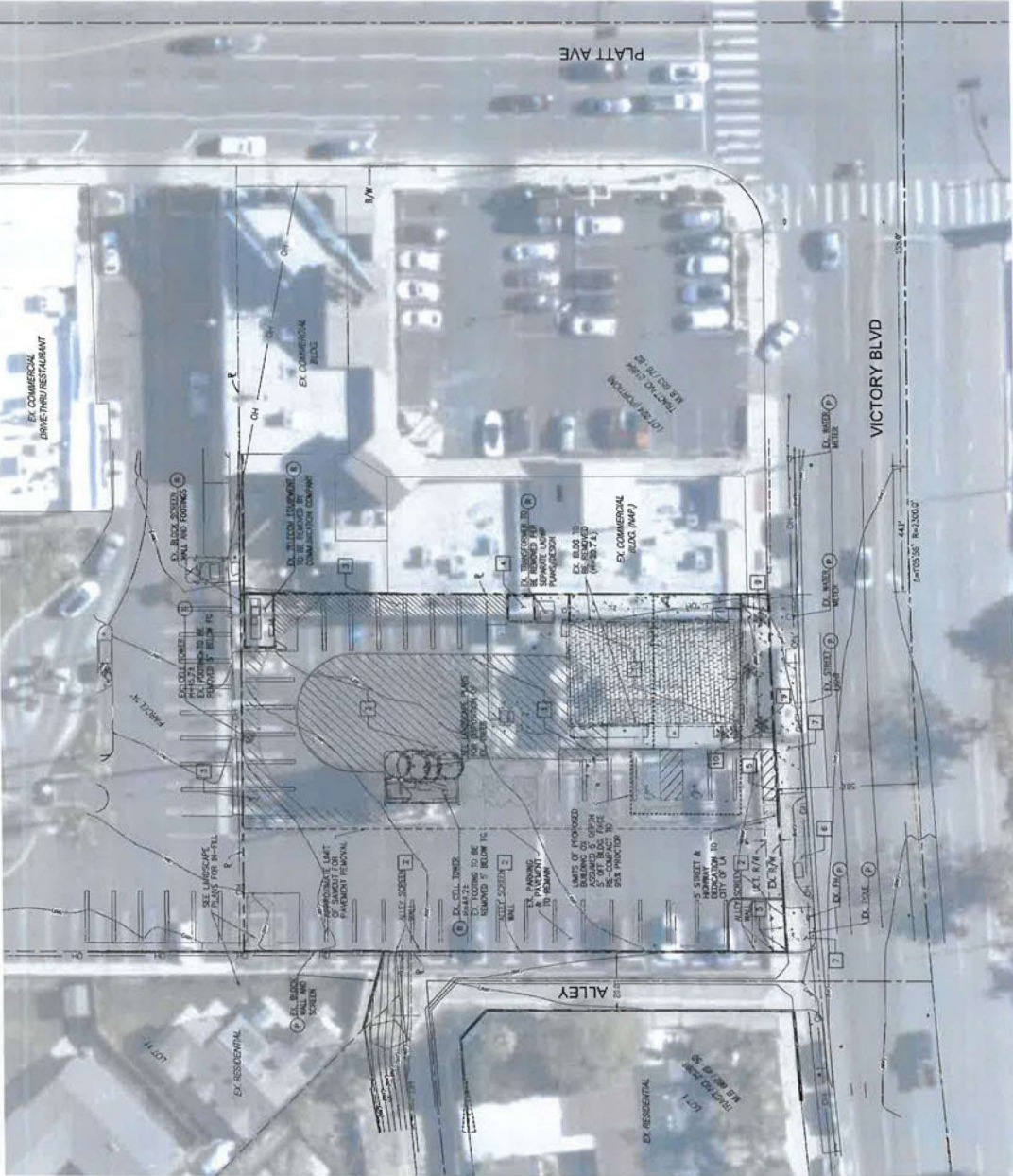
SITE DEMOLITION

PROJECT NAME:  
STARBUCKS AT  
VICTORY BLVD & PLATT AVENUE  
PROJECT ADDRESS:  
2377 VICTORY BLVD  
WEST HILLS, CA 91307

SHEET TITLE:  
**EXH-003**  
SHEET NUMBER:  
**1 OF 1**

**DEMOLITION ASSUMPTION NOTES:**

- 1. EXISTING WALL & FOOTING, TO BE RECONSTRUCTED FULL 6" HIGH (ALLEY GRADE TO TOP OF WALL) WITH 12" MIN. DEPTH.
- 2. REMOVE AND DEPOSE OF EX. AS PAVING, ASSUME 4" THICK, SCARIFY 12" MIN. DEPTH.
- 3. REMOVE AND DEPOSE OF EXISTING TROUGH ENCLOSURE.
- 4. REMOVE AND DEPOSE OF EX. BLOCK WALLS TO EDGE OF DEMOLITION.
- 5. REMOVE AND DEPOSE OF EX. PAVEMENT AND INSTALL NEW DSE DRIVEWAY.
- 6. REMOVE EXISTING LANDSCAPE, WALK, SOPS, AND BOLLARD. INSTALL NEW SIDEWALK PER STANDARD. 1.5% MINIMUM SLOPE.
- 7. REMOVE EX. TREE IN DEMOLITION AREA CONFORM DISPOSITION WITH LANDSCAPE PLANS, ASSUME 4" X 6" MIN. AREA TO BE INFILLED WITH SUITABLE FILL TO SUPPORT PUBLIC UTILITY. EX. POLE SON TO REMAIN (VIA REVOCABLE PERMIT) & STORAGE TO BE RE-DOING PER SEPARATE SON PERMIT.



**EXHIBIT "A"**  
Page No. 12 of 14  
Case No. A-24-522-00-28A

**ADDRESS**  
2377 VICTORY BLVD, LOS ANGELES, CA 91307

**LEGAL DESCRIPTION**  
THESE ARE PARTS OF THE CITY OF WEST HILLS, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:  
THAT PORTION OF LOT 224 OF TRACT NO. 27884, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 653 PAGES 76 TO 82, INCLUSIVE OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:  
BEGINNING AT THE MOST SOUTHWESTERLY CORNER OF SAID LOT 224;  
THENCE NORTH 21°43'00" EAST 152.00 FEET;  
THENCE SOUTH 7°14'30" WEST 152.43 FEET TO THE NORTHERLY LINE OF VICTORY BOULEVARD, A PARCEL TO SAID POINT BEARING NORTH 1°07'45" EAST 152.00 FEET TO THE POINT OF BEGINNING;  
CENTRAL ANGLE OF 303°03' AND ARC LENGTH OF 125.13 FEET TO THE POINT OF BEGINNING;  
A PARCEL TO SAID POINT BEARING NORTH 15°42'27" WEST;  
SAID LAND IS ALSO SHOWN AS PARCEL 8 OF PARCEL MAP 3275, RECORDED IN BOOK 77 PAGES 78 AND 79 OF PARCEL MAPS.  
EXCEPTING THEREFROM ALL OIL, GAS, MINERALS AND OTHER HYDROCARBON SUBSTANCES IN AND UNDER SAID LAND, AND ALL RIGHTS AND INTERESTS THEREIN, AND ALL RIGHTS AND INTERESTS IN SAID LAND, AS RESERVED IN A DEED BY GEORGE E. PLATT COMPANY, A CORPORATION, RECORDED APRIL 7, 1935 ON BOOK 47417 PAGE 214, OFFICIAL RECORDS.



2025/02/25

# SUMMARY OF INFORMATION

**ADDRESS**  
23717 VICTORY BOULEVARD  
WEST HILLS

**ZONING**  
APN: 2033-010-043  
ZONING: P-1VL (AUTOMOBILE PARKING ZONE)  
CC1-1VL (LIMITED COMMERCIAL ZONE)

**COMMUNITY PLAN AREA:**  
CANOGA PARK - WINNETKA - WOODLAND HILLS - WEST HILLS

**SITE AREA**  
GROSS SITE AREA ±23,468 SF OR 0.539 AC

**BUILDING DATA**  
BUILDING AREA ± 1,479 SF  
F.A.R. 0.063 (1.5:1 MAX.) (1L HT. DISTRICT)  
BUILDING HEIGHT 75' (MAX.)

**BUILDING SETBACK**  
FRONT 10'  
SIDE AND REAR NONE

**PARKING DATA**  
EXISTING PARKING:  
STANDARD STALLS 35 STALLS  
ACCESSIBLE STALLS 1 STALL  
TOTAL PROVIDED 37 STALLS

**PARKING REQUIRED:** 15 STALLS (⊙ 1/1000 SF)

**PARKING PROVIDED:** 11 STALLS (9' x 18' ⊙ 90°)  
ADA ACCESSIBLE 1 STALL  
EV CHARGING 4 STALLS  
TOTAL PROVIDED 16 STALLS

**PARKING RATIO:** 10.8 /1000 SF  
BIKE RACKS 4 BIKES  
BIKE LOCKERS 2 (2 BIKES PER LOCKER)

**SITE LAYOUT DATA**  
PARKING BAY WIDTH 61'-4" ⊙ 90° (9' WIDE STALL)  
(2-WAY, DOUBLE LOADED)

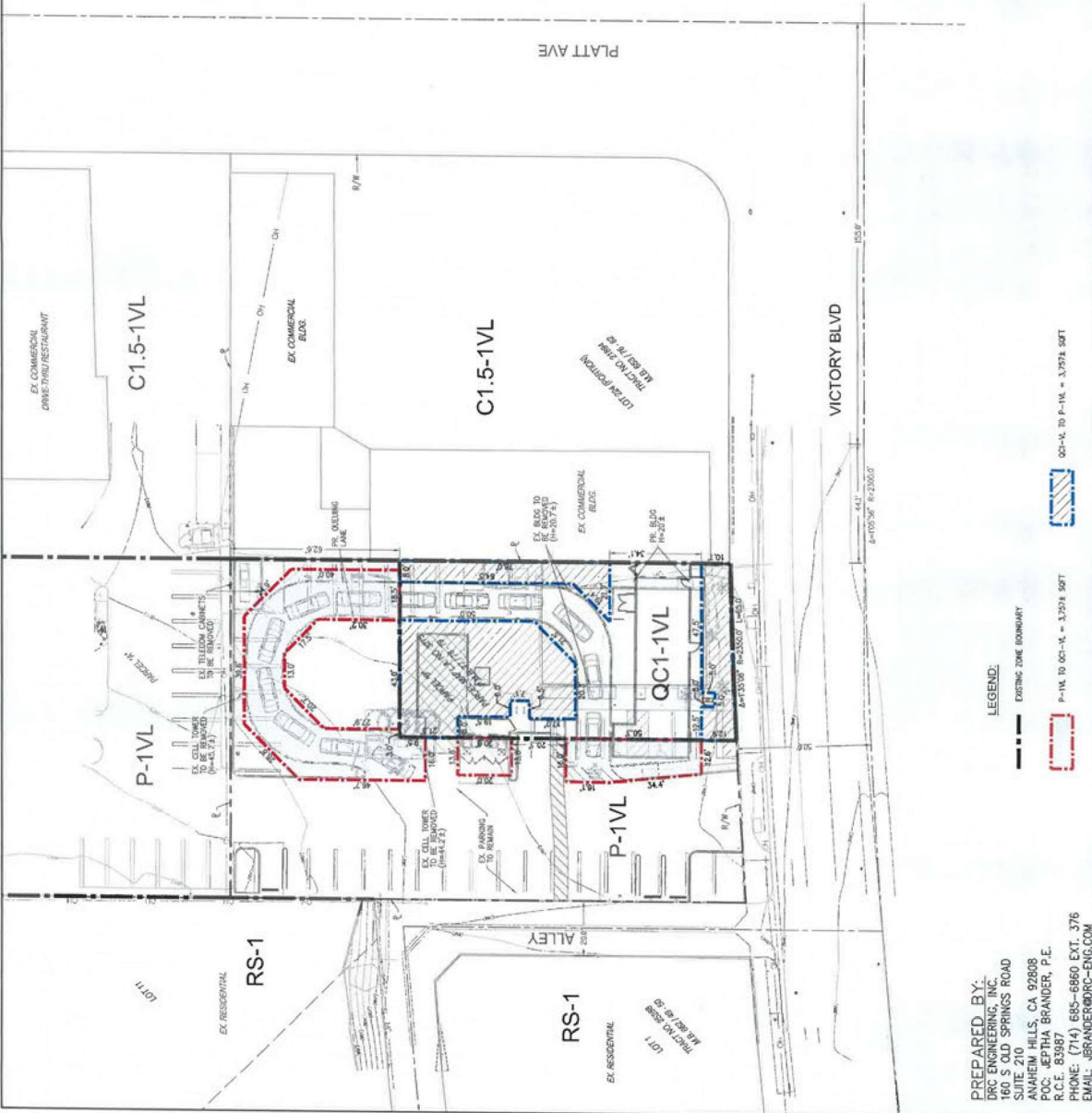
**FIRE LANE** 26' (MIN.)

**EXHIBIT "A"**  
Page No. 3 of 17  
Case No. 24-5226-042-288

**ADDRESS**  
23717 VICTORY BLVD, LOS ANGELES, CA 90074

**LEGAL DESCRIPTION**  
THE LAND DESCRIBED TO HEREIN BELOW IS SITUATED IN THE CITY OF WEST HILLS, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:  
LOT 224 OF PARCEL MAP NO. 20894, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND PART OF PARCELS 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000.

2011 VECTOR INCLUDES  
ZONE BOUNDARY ADJUSTMENT PLAN  
WEST HILLS, CA  
DATE: 08/14/13  
SCALE: 1" = 20' FT  
GRAPHIC SCALE: 1" = 20' FT



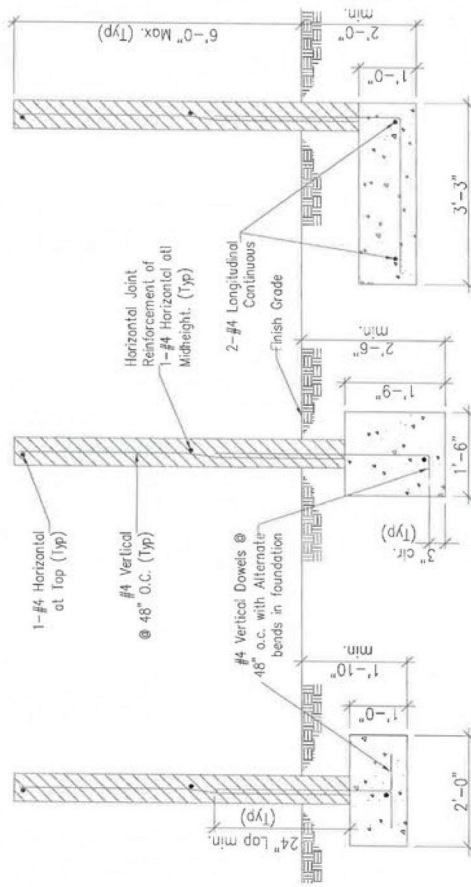
**PREPARED BY:**  
DRC ENGINEERING, INC.  
160 S OLD SPRINGS ROAD  
SUITE 210  
ANAHEIM HILLS, CA 92808  
P.O. BOX 65987  
PHONE: (714) 685-6660 EXT. 376  
EMAIL: JBRANDER@DRC-ENG.COM

**LEGEND:**  
--- EXISTING ZONE BOUNDARY  
--- P-1VL TO 105'-4" = 3,751.50 FT  
--- QC1-1VL TO 105'-4" = 3,751.50 FT

**ADDRESS**  
23717 VICTORY BLVD, LOS ANGELES, CA 90074

**LEGAL DESCRIPTION**  
THE LAND DESCRIBED TO HEREIN BELOW IS SITUATED IN THE CITY OF WEST HILLS, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:  
LOT 224 OF PARCEL MAP NO. 20894, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND PART OF PARCELS 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000.

**6" CONCRETE BLOCK MASONRY WALL DETAIL  
 (6'-0" HEIGHT MAXIMUM)**



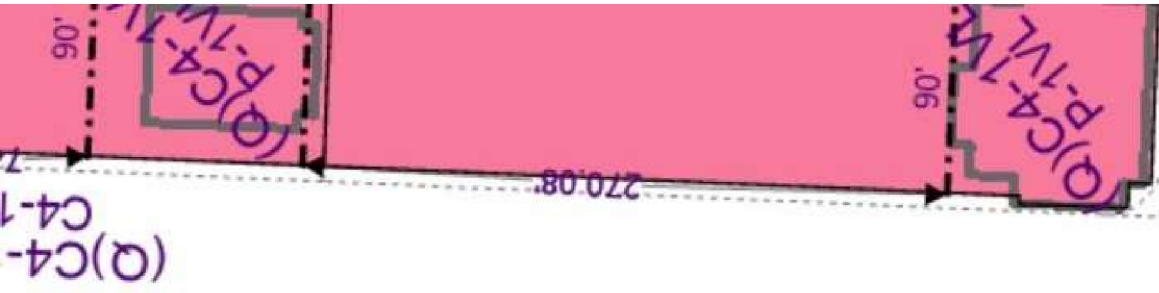
**GENERAL SPECIFICATIONS:**

1. Concrete mix for footing to be 1 part cement to 2.5 parts sand to 3.5 parts gravel with a maximum of 7.5 gallons of water per sack of cement/ Minimum  $f_c = 2,500$  psi.
2. Concrete block units shall conform to ASTM C90.
3. Reinforcing steel to be deformed and conformed to ASTM Standard A615 Grade 40 or Grade 60
4. Rebar shall be centered in the concrete block cell in which it is located.
5. Concrete block units shall be placed in a running bond with head joints in successive courses horizontally offset at least one-quarter the unit height.
6. Concrete block units to have vertical continuity of cells unobstructed. All cells containing reinforcing shall be solid grouted (vertical and horizontal reinforcement). Cells not containing reinforcing shall not be grouted.
7. Mortar mix for concrete to be 1 part cement to 3 parts sand to 3 parts damp loose sand.
8. Grout mix for concrete block wall to be 1 part cement to 3 parts sand to which may be added not more than 0.1 part lime. Sufficient water to be added to produce consistency for pouring without segregation of constituents. The grout may contain 2 parts pea gravel, maximum size 3/8".
9. Block fence may be placed in the center of the "T" type foundation and at either edge to "L" type foundation.
10. A first inspection is required when trenches are ready for concrete and all required steel is tied in place. A second inspection is required when first horizontal bar and all vertical bars are in place but not grouted.
11. Foundation must be poured against undisturbed soil with no appreciable slope of sidewalls on all types of foundation.
12. Masonry or concrete fences over 3'-6" in height measured from adjacent grade shall require a building permit.
13. Height of fence shall comply with all provisions of the zoning code.

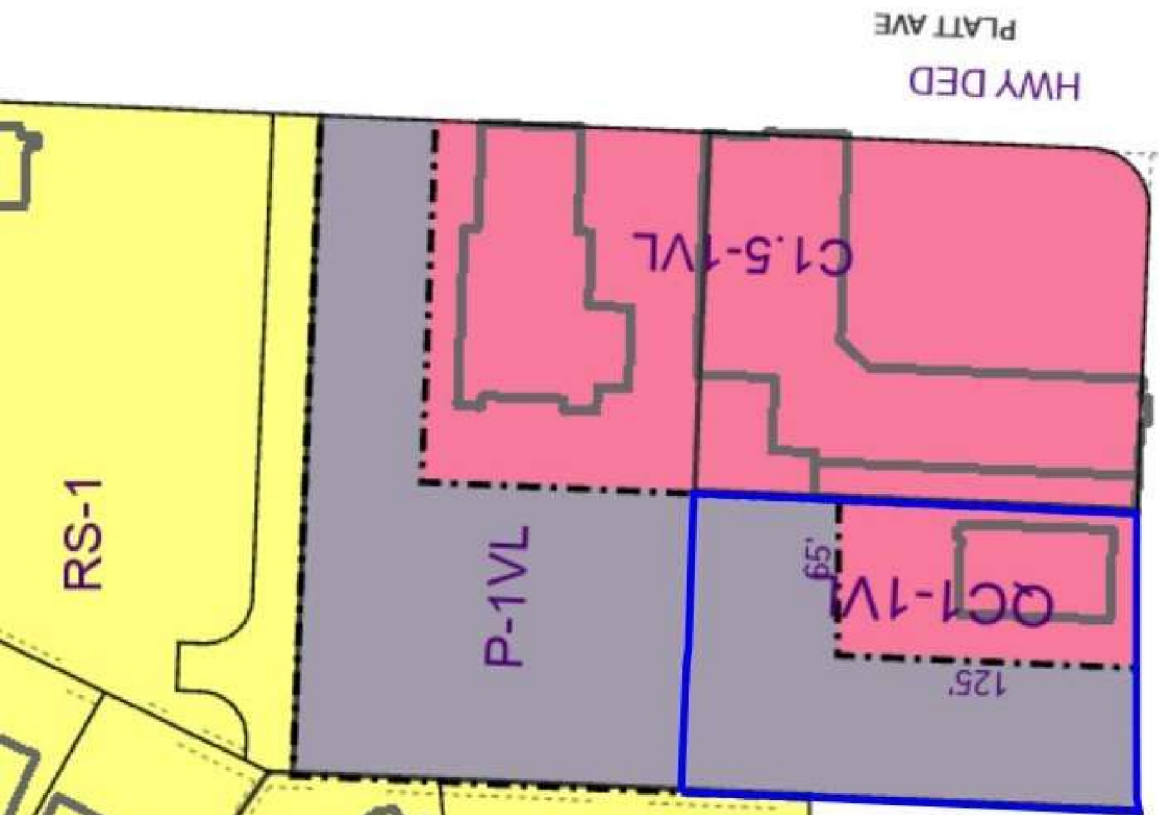
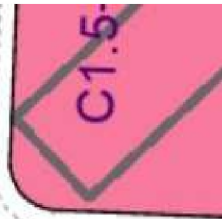
As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability and, upon request, will provide reasonable accommodation to ensure equal access to its programs, services and activities. For efficient handling of information internally and in the internet, conversion to this new format of code related and administrative information bulletins including MGD and RGA that were previously issued will allow flexibility and timely distribution of information to the public.

**EXHIBIT "A"**  
 Page No. 14 of 16  
 Case No. 21-21-5922-CAD-28A

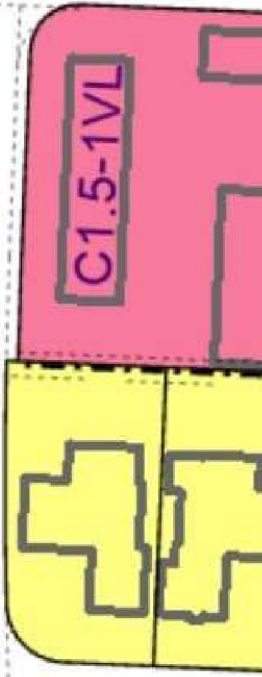
# **MISCELLANEOUS REPORTS**



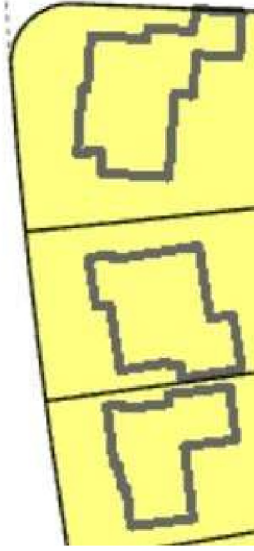
HWY



VICTORY BLVD



KENWATER AVE

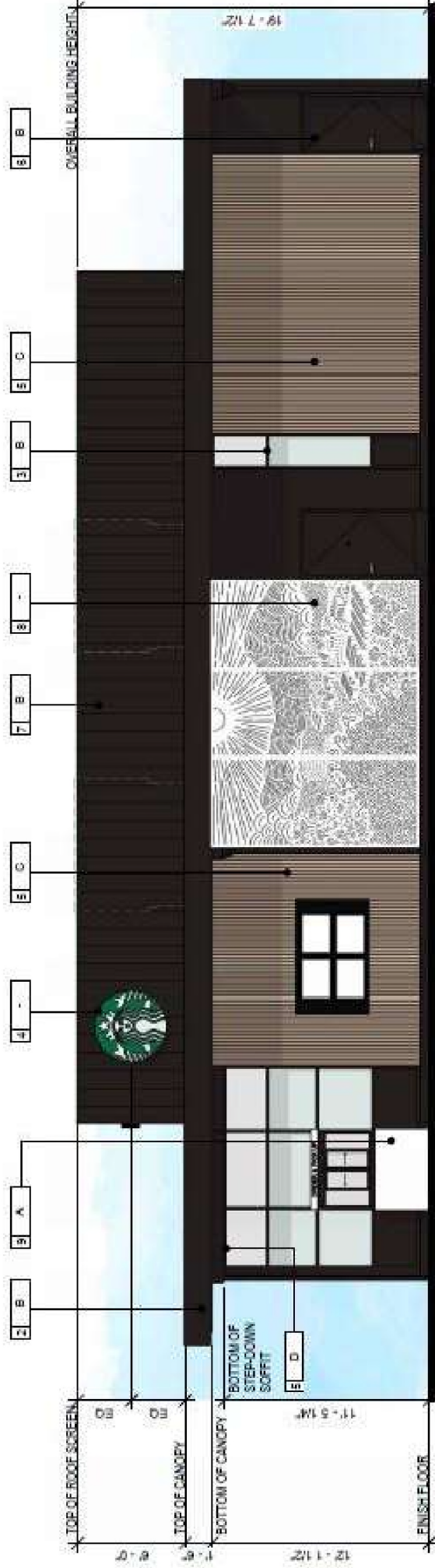


WINDOW TRANSPARENCY ANALYSIS

OVERALL ELEVATION SF: +/- 1,214 SF

TRANSPARENT WINDOWS SF: +/- 65 SF

65 SF / 1,214 SF = 5% WINDOW TRANSPARENCY



1 SOUTH ELEVATION

SCALE: 3/16" = 1'-0"

# **RADIUS MAP**

# **CORRESPONDENCE**


# INITIAL SUBMISSIONS

The following submissions by the public are in compliance with the Commission Rules and Operating Procedures (ROPs), Rule 4.3a. Please note that “compliance” means that the submission complies with deadline, delivery method (hard copy and/or electronic) AND the number of copies. The Commission’s ROPs can be accessed at <http://planning.lacity.org>, by selecting “Commissions & Hearings” and selecting the specific Commission.

The following submissions are not integrated or addressed in the Staff Report but have been distributed to the Commission.

Material which does not comply with the submission rules is not distributed to the Commission.

## ENABLE BOOKMARKS ONLINE:

\*\*If you are using Explorer, you will need to enable the Acrobat  toolbar to see the bookmarks on the left side of the screen.

If you are using Chrome, the bookmarks are on the upper right-side of the screen. If you do not want to use the bookmarks, simply scroll through the file.

If you have any questions, please contact the Commission Office at (213) 978-1300.

# Channel Law Group, LLP

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8383 Wilshire Blvd.  
Suite 750  
Beverly Hills, CA 90211

Phone: (310) 347-0050  
Fax: (323) 723-3960  
www.channellawgroup.com

JULIAN K. QUATTLEBAUM, III  
JAMIE T. HALL \*  
CHARLES J. McLURKIN  
GREGORY T. WITTMANN

Writer's Direct Line: (310) 982-1760  
jamie.hall@channellawgroup.com

\*ALSO Admitted in Texas

February 2, 2026

## VIA ELECTRONIC MAIL

South Valley Area Planning Commission  
200 North Spring Street, Room 763  
Los Angeles, CA 90012  
[apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org)  
[henry.chu@lacity.org](mailto:henry.chu@lacity.org)  
[stephanie.escobar@lacity.org](mailto:stephanie.escobar@lacity.org)

**Re: Supplemental Justification for Appeal of Case No. ZA-2024-5222-CU2-ZBA,  
Conditional Use Permit and Zone Boundary Adjustment, and ENV Case No.  
2024-5223-CE, 23717 Victory Boulevard (Proposed Starbucks Drive-Through)**

Dear South Valley Area Planning Commission:

This firm represents West Valley Alliance for Optimal Living ("Appellant"), a not-for profit unincorporated association. Appellant and its members have a direct and substantial beneficial interest in ensuring that the City of Los Angeles ("City") complies with laws relating to environmental protection and orderly growth of the City.

This supplemental letter<sup>1</sup> continues to outline the justifications for appeal of the Zoning Administrator's ("ZA") approval of Case No. ZA-2024-5222-CU2-ZBA, and ENV Case No. 2024-5223-CE ("Project Approval"), which authorizes the construction, use, and maintenance of a new 1,479 square-foot Starbucks Drive-Through Coffee Shop at 23717 Victory Boulevard ("Project"). The project site is zoned P-1VL and QC1-1VL ("Project Site"). The Project Site is located within the Canoga Park-Winnetka-Woodand Hills-West Hills Community Plan ("Community Plan"). The subject site is also within an Urban and Built-up Land zone, Outside Flood zone, Urban Agriculture

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<sup>1</sup> In addition to providing the precise bases for the City's failure to proceed in the manner required by the California Environmental Quality Act, this letter is also intended to provide pre-suit notification to the City to the extent such notification may be required. The City may remedy the errors identified herein by not deeming the project exempt from CEQA and instead preparing an environmental report for the Project. Appellant will provide the City with a reasonable opportunity to correct these errors by not commencing litigation for at least 14 days after the City's final approval of the Project (should that occur).

Incentive zone, and located within 8.37 miles (13.47 kilometers) from the Malibu Coast Fault. The Letter of Determination (“LOD”) acknowledges that to the south and to the west of the project site, there are low residential and single-family residences. LOD at p. 10. There are also residences to the north of the project site.

As explained below, the ZA erred in making the requisite findings for the entitlements, as the record, including the concerns raised during the public hearing on September 6, 2025, contains substantial evidence demonstrating that the Project's significant features, size, and operation will be incompatible with, and detrimental to, the adjacent Low Residential (RS-1) and single-family residences properties and the surrounding neighborhood. The Project harms the public’s health, safety, and welfare. Therefore, the Project’s approval should be set aside, and the City should conduct proper project review and environmental review as required by the California Environmental Quality Act (“CEQA”).

**I. GROUNDS FOR APPEAL: THE ZA ERRED IN MAKING REQUIRED FINDINGS**

The Zoning Administrator failed to apply the correct legal standard and ignored substantial evidence from the public testimony, which directly undermines the required Conditional Use Permit (CUP) and Drive-Through Establishment findings (LOD at p. 15-24), and CEQA findings and determination (LOD at p. 1-15).

**A. Conditional Use Finding No. 2 – Compatibility and Adverse Effects**

The ZA found that the Project's location, size, height, operations, and other significant features "will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare and safety." (LOD at p. 17). The Project’s location, size, height, operations and other significant features will not be compatible with and will adversely affect or further degrade adjacent properties, the surrounding neighborhood, and the public health, welfare, and safety. The February 1, 2026, noise report prepared by Steve Rogers Acoustics (“Noise Report”) supports the position the Project is against the public health, welfare, and safety, and Project Approval should be set aside. A true and correct copy of the Noise Report is attached hereto as **Exhibit 1**.

The ZA’s finding is not supported by substantial evidence and is directly contradicted by the Noise Report and the following testimony presented at the public hearing conducted on September 6, 2025, among other things.

The Noise Report points out the ZA’s deficiencies in stating, at page 2, “However, the ZA provides no analysis [or] other supporting evidence for the claim that a 6-foot high wall and a screen around the rooftop HVAC equipment will be sufficient to eliminate adverse noise impacts in the neighboring homes or to ensure that those residential properties are not degraded by the e noise of drive-through operations, which include nighttime operations.”

**1. Traffic and Circulation Degradation**

Public testimony repeatedly highlighted the project’s impact on an already burdened intersection. Mr. Jeff Bornstein of West Valley Alliance for Optimal Living and other residents

explicitly stated concerns regarding traffic being added by the Project and the intersection being busy (LOD at p. 14-15). The ZA attempts to mitigate this with a "Right Turn Only" egress condition (LOD at p. 19, 25), yet ignores the inherent high-volume, high-turnover nature of the proposed use which will inevitably lead to traffic spillover onto Victory Boulevard (LOD at p. 16, 18), especially when the 13-car queue is exceeded (LOD at p. 19). Starbucks is a very popular coffee shop. And its drive throughs often exceed capacity in urban Los Angeles. Undoubtedly, the 13-car queue will be exceeded. In an email dated September 9, 2025, from Mr. Bornstein, Mr. Bornstein stated that the drive-through lane is awkward and cars will impact the streets and that too many deviations are being requested. (LOD at p.11.) Moreover, in an email dated September 24, 2025, from Jen Westmoreland, Ms. Westmoreland stated observations of accidents near Victory and Platt intersection. (LOD at p..11.) She further stated that some drivers speed. Ms. Westmoreland further stated that the site cannot accommodate a drive-through and created additional traffic *nightmares*. In an email dated September 10, 2025, and another email dated September 11, 2025, Mr. Fern White states that the project adds traffic congestion to an already busy intersection and that there already exists a coffee shop next door. (LOD at p.11.) In an email dated September 23, 2025, Steven Brtis states that the Project adds to traffic and parking concerns. (LOD at p.11.) In an email dated September 11, 2025, Cathy Connell stated the project will add to an increase in traffic dangers. (LOD at p. 11.) The testimony provides substantial evidence that the project use is an inadequate fit for a site so constrained, contradicting the finding of no adverse effect on public safety and vehicular circulation.

## **2. Table A: 23717 Victory Blvd. Project Trip Generation (Fast-Food Pass-By)**

The project will generate 674 daily trips, whereas a coffee shop with drive through is usually 533.57 daily trips. See attached **Exhibit 2**, which is a true and correct copy of Table A from the City's files. Thus, the Project will generate over 140 daily trips more than a coffee shop with drive-through. Since there is currently no existing operating business at the Project Site, the Project will generate over 674 additional daily trips at the Project Site. It appears the prior business at the Project Site has been non-operational since at least 2022. The LOD at p. 12 admits that the former Pizza Hut building has been vacant for a number of years.

## **3. Noise and Late-Night Operation**

The proposed hours (4:30 AM to 10:00 PM Monday through Thursday and 4:30 AM to 11:00 PM Friday to Sunday) are an extension of the allowable Commercial Corner Development hours (7:00 AM to 11:00 PM). Mr. Bornstein specifically noted that the proposed hours were "too late" given the proximity to homes to the west (LOD at p. 14), in addition to the homes that are to the north and south of the project site. The ZA's reliance on a new 6-foot wall and Automatic Volume Control ("AVC") for the speaker box as adequate mitigation (LOD at p. 19) is speculative and inadequate. In addition to the speaker box noise, there is greater noise from the vehicular movement, car doors, customer conversations, and customers walking to and from the Starbucks from 4:30 AM until 11:00 PM, all occurring immediately adjacent to the RS-1 properties across the 20-foot alleyway, and the residences to the south, west, and north of the project site, which do not have the alley as a separation. This proximity demonstrates a clear potential for significant disturbances, directly undermining the finding of compatibility. Furthermore, the 16 parking spots

and EV charging station will provide additional noise to the neighboring residences. Also, the “routine” cleanup will be allowed after hours. (LOD at p. 3., Item 9.) The LOD does not define “routine” cleanup. In addition to these noises, there will be noises generated by the employees’ required activities, such as talking to each other and to customers, parking, taking out the trash, and cleaning.

The Noise Report, at page 2, specifically states that Conditions 17 and 18 of the LOD do not address the steady flow of vehicles through the drive-through, vehicle doors and music playing in vehicles, interactions at the ordering and pickup points (and with Starbucks staff taking orders outside in the case that the queue exceeds 13 vehicles), etc.

The Noise Report, at page 2, further states that: “Condition 22 does not eliminate truck reversing/beeping (which would be impossible in practice) nor does it forbid delivery trucks driving (forward or backwards) in the alleyway. It also places no restrictions on the time that deliveries may occur – presumably opening the possibility of nighttime deliveries as late as 11:00 PM and as early as 4:30 AM. So, as written, Condition 22 does not adequately mitigate adverse noise impacts or ensure that the noise of delivery trucks does not degrade the nearby residential properties.

#### **4. Zone Boundary Adjustment**

The Project requires a Conditional Use Permit (“CUP”) and zone boundary adjustment to allow the construction, use, and maintenance of a new 1,479 square-foot Starbucks drive-through coffee shop. The Project proposes an adjustment to the C1 zone boundary so that the entire building, signage, patio, trash enclosure, and drive-through lanes are all within the boundaries of the commercial zone. The Project does not fit within the confines of the existing zoning and land use parameters, so the project applicant requires a CUP, zone boundary adjustment, and a zone change. Thus, the project applicant seeks a reconfiguration of the existing project site to accommodate his project and peg it into the existing land.

- **Failure to Specify Duration of CUP:** The ZA findings fail to specify the duration of the CUP. The findings need to specify when the CUP will expire.
- **At Minimum 2 Other Coffee Shops Nearby:** There are at least two other coffee shops currently existing near the project site. For example, a Coffee Bean Tea and Leaf is next to the project site, approximately 15 feet away, and an existing Starbucks is across the street from the project site. This additional drive-through Starbucks is not needed in the community.
- **The Project Site Easements Are Not Delineated:** The City documents reference easements in connection with the Project Site, but the City fails to delineate the easements at or near the Project Site. Neither the legal description nor the physical layout of the easements have been provided to the public.

#### **B. Drive-Through Establishment Finding No. 4 – Noise Protection**

The ZA found that "residential uses in the vicinity of a proposed drive-through fast-food

establishment will be adequately protected from any significant noise resulting from outdoor speakers, autos, or other sources of noise associated with the lot." (LOD at p. 21).

The ZA's finding fails to address the unique constraints of this site and the nature of the use, contradicting the public record.

**1. Buffer Inadequacy:**

The ZA claims the residential uses are "buffered by a 20-foot alley and trees on walls" (LOD at p. 19). The presence of a 20-foot alley is not a *buffer* provided by the Project; it is a pre-existing condition, and is, in fact, the closest point of separation between the commercial and residential zones. The new 6-foot wall and existing parking asphalt (LOD at p. 21) are insufficient to shield residents from high-volume, late-night traffic, and operation noise. As the adjacent residential uses are single-family homes (RS-1), the noise from idling cars, honking, loud music, and customer interactions occurring during the drive-through's long operational hours (4:30 AM to 11:00 PM) will be projected directly toward and over a standard 6-foot masonry wall. The Noise Report, at pages 2 to 3, states that there is no analysis to show how much a 6-foot wall will reduce noise from cars, trucks, in-car music, ordering/pickup interactions, etc., or what the net levels of noise received beyond the western property line will be. In other words, there is no support for the ZA's claim that the neighboring homes will be "adequately protected" from noise. Also, the alleyway is only partially to the west, so it does not cover the entire western boundary. There is no alley at the northern part of the western boundary. The ZA ignores the residences to the north and south of the project site. Further, all of the easements on the Project Site need to be delineated and disclosed to the public, but they are not.

**2. Queuing Proximity**

The design places the drive-through lane, capable of holding 13 vehicles, immediately along the western (residential-adjacent) side of the property, retaining "existing parking areas on the western property line" (LOD at p. 21) due to utility/easement constraints. This design choice forces all associated noise, idling fumes, and light scatter (despite conditions for shielding) into the most sensitive area of the property, directly impacting the RS-1 homes, which are to the north, south, and west of the project site. The finding that the residents are "adequately protected" is factually unsupported given the operational proximity.

**C. Drive-Through Establishment Finding No. 6 – Minimizing Disturbance and Enhancing Privacy**

The ZA erroneously found that "trash storage, trash pickup hours, driveways, parking locations, screening walls, trees and landscaping are provided for and located so as to minimize disturbance to the occupants of nearby residential uses, and to enhance the privacy of those uses." (LOD at p. 23)

The ZA's conclusion that the design minimizes disturbance and enhances privacy is flawed.

### **1. Parking Lot/Drive-Through as “Buffer”**

The ZA relies on the parking lot and drive-through lane itself being retained on the western property line (close to the RS-1 homes) as a necessary condition due to easements (LOD at p. 23-24). This choice places high-activity, noise, and fume-generating vehicle movement directly next to residences, which is inherently the opposite of minimizing disturbance. The required new 6-foot wall only offers minimal ground-level visual screening; it does not eliminate the operational noise, light, privacy intrusion caused by patrons in cars, who are elevated above the fence line, or other noise generated at the Project Site.

### **2. Trash Location and Scheduling**

While a trash enclosure is proposed to be locked and comply with standards, the ZA's finding fails to address the trash pickup hours. Given the extended hours of operation (starting at 4:30 AM and ending at 10:00 PM Monday through Thursday and ending at 11:00 PM Friday through Sunday), trash pickup is likely to occur in the early morning hours, creating significant noise disturbance (slamming containers, beeping trucks) for the RS-1 residents, who are separated only by a 20-foot alley at most. This is a common point of contention, and the finding that the location minimizes disturbance is questionable without specific conditions prohibiting early morning trash service.

### **3. Noise Report Determination That ZA Findings Are Deficient and Inadequate**

**Drive-Through Location:** Moving, parked and parking vehicles will be generating noise all over the Project Site – including close to the western property line and the neighboring homes. So, the ordering point and speaker box location do not mitigate the noise issues. (Noise Report at p. 3.)

**Speaker Box, Volume Control:** This does nothing to mitigate noise impacts from vehicles/traffic. (Noise Report at p. 3.)

**Vehicles Not Noise Barriers:** Vehicles, whether queuing in the drive-through itself or elsewhere on the site, are a source of noise in their own right – much more significant than the ordering speaker box – and yet are not addressed in the findings. (Noise Report at p. 3.)

**New 6-Foot-High Wall:** No calculations are provided in the findings to show how much reduction the wall will provide and what the net noise levels will be at the nearby homes. (Noise Report at p. 4.)

**Alley Not a Mitigating Feature:** The alley is an existing part of the neighborhood, not a project design feature. The findings lack analysis to show how much noise attenuation is achieved by the extra distance. There is no alley at the northwest boundary. (Noise Report at p. 4.)

**Landscaping:** It is simply not true that the proposed landscaping will act as a barrier to noise. It would take a deep (50 to 100 feet) continuous band of densely-planted trees to provide any meaningful reduction in noise. (Noise Report at p. 4.)

**D. Mini-Shopping Center/Commercial Corner Finding No. 7 – Traffic Hazard/Congestion**

The ZA incorrectly found that ingress/egress "will not create a traffic hazard or cause significant traffic congestion or disruption of vehicular circulation on adjacent streets." (LOD at p. 24). As explained below, this finding was in error.

**1. Identified Hazards Require Mitigation**

This finding is undermined by the ZA's own imposed conditions. The ZA explicitly required the applicant to "coordinate with the Department of Transportation ("LADOT") West Valley District Office to... install traffic measures to prohibit left turns from the eastbound lanes of Victory Boulevard" and also conditioned the driveway to be "Right Turn Only" egress (LOD at p. 25, Condition 27). Furthermore, the ZA conditioned the presence of an employee during peak times and when the queue exceeds 13 cars "to facilitate traffic movement" (LOD at p. 25, Condition 28). The necessity of mandating signage, traffic measures, and employee facilitation demonstrates that, *without* these measures, the Project *would* create a traffic hazard or significant disruption. This places the burden of preventing a known hazard onto the applicant's operations, rather than designing it out of the site, which suggests the Project's size and design are inherently incompatible with the intersection. The ZA ignores that McDonald's has an existing drive-through lane which compounds the Project Site's significant and adverse hazards, traffic, and circulation issues. Further, the ZA fails to address where customers will park their vehicles when there are no available parking spaces at the project site and there are 13 cars in the drive-through lane.

**2. Failure to Trigger Traffic Assessment**

The ZA stated the project "does not meet the threshold to trigger a traffic assessment" (LOD at p. 25). Given the substantial public testimony on traffic, the site's proximity to a major intersection, and the acknowledged need for mandated traffic controls (Right-Turn Only, No Left Turn In, employee management), the ZA erred by not requiring a focused traffic analysis. The substantial evidence based on fact from the public hearing is substantial enough to warrant formal CEQA analysis on traffic, as the ZA's reliance on LADOT's high-level review, which in turn required heavy operational conditions, is insufficient to support the finding that no significant traffic or congestion will occur. The City cannot rely on a threshold because thresholds are not conclusive nor dispositive. (*Mejia v. City of Los Angeles* (2005) 130 Cal.App.4th 322.) Substantial evidence exists supporting significant adverse impacts to traffic and circulation, requiring a traffic assessment and study prior to any project approval.

**E. Conditional Use Finding No. 1 – Enhancement of Surrounding Neighborhood and Essential or Beneficial Service**

The ZA erroneously found that the project will enhance the built environment in the surrounding neighborhood or perform a function or service that is essential or beneficial to the city, community, or region. (LOD at p. 15.) The Project, consisting of a Class 2 CUP and a Zone Boundary Adjustment to allow the construction, use, and maintenance of a new Starbucks drive-through coffee shop is a zone change to accommodate the Project of approximately 1,479 square feet. This is a zone change and is inconsistent with the Canoga Park-Winnetka-Woodland Hills-West

Hills Community Plan. A true and correct copy of the Community Plan from the City’s website is attached hereto as **Exhibit 3**. For example, the Project is changing the zoning and is interfering with the nearby residential areas.

**F. Conditional Use Finding No. 3 – Substantial Conformance with General Plan/Community Plan**

The ZA erroneously found that the Project substantially conforms to the purpose, intent, and provisions of the General Plan, including the Community Plan. (LOD at p. 20.) For example, the LOD at p. 21, inaccurately states that: “The limited hours ensure compatibility with the nearby residential uses.” First, the hours are not limited because the hours are from 4:30 AM to 11:00 PM on the weekends and 4:30 AM to 10:00 PM Monday through Thursday. Second, the nearby residences are to the north, west, and south of the project site and the conditions imposed by the ZA do not protect the nearby residences.

The Project, consisting of a Class 2 CUP and a Zone Boundary Adjustment to allow the construction, use, and maintenance of a new Starbucks drive-through coffee shop is a zone change to accommodate the project of approximately 1,479 square feet. This is a zone change and is inconsistent with the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan. For example, the project is changing the zoning and is adversely interfering with the nearby residential areas. Community issues include the need to preserve existing single-family neighborhoods (see **Exh. 3** Community Plan at p. I-3), inadequate transition between commercial and residential uses (see **Exh. 3** Community Plan at p. I-4), and traffic speed and/or volume compromises safety and quality of many residential neighborhoods and some commercial areas (see **Exh. 3** Community Plan at p. I-5).

Opportunities include establishing appropriate transition between commercial and adjoining, primarily residential, uses (see **Exh. 3** Community Plan at p. I-4), and improving physical appearance of commercial districts and reducing scales of commercial development adjacent to residential neighborhoods (see **Exh. 3** Community Plan at p. I-4). The neighborhood character is to preserve and enhance the positive characteristics of existing uses which provide the foundation for community identity, such as scale, height, bulk, setbacks, and appearance. (See **Exh. 3** Community Plan at p. I-9.) The intent of the Community Plan is the promotion of an arrangement of land uses, streets, and services which will encourage and contribute to the economic, social and physical health, safety, welfare, and convenience of the people who live and work in the community. (See **Exh. 3** Community Plan at p. II-2.)

The plan also guides development to create a healthful and pleasant environment. (See **Exh. 3** Community Plan at p. II-2.) Current Plan policy provides for continued preservation of the existing single-family districts and multi-family clusters. (See **Exh. 3** Community Plan at p. III-1.) Goal 1 is to have a safe, secure, and high-quality residential environment for all economic, age, and ethnic segments of the Canoga Park-Woodland Hills-West Hills Community Plan Area. (See **Exh. 3** Community Plan at p. III-2.) Objective 1-1.3 is to protect existing stable single-family and low-density residential neighborhoods from being impacted by the size of commercial development. Objective 1-1.5 is to protect existing stable single family and low-density residential neighborhoods

from encroachment by higher density and other incompatible uses. (See **Exh. 3** Community Plan at p. III-3.) All zone changes, subdivisions, parcel maps, variances, conditional uses, specific plans, and community and neighborhood revitalization programs for residential projects shall be consistent with Community Plan land use designations. (See **Exh. 3** Community Plan at p. III-3.) Goal 15 is to have a sufficient system of well-designed and convenient on-street parking and off-street parking facilities throughout the plan area. (See **Exh. 3** Community Plan at p. III-23.) The Project as approved by the ZA fails to conform to all of these issues, opportunities, intent, and goals of the Community Plan. Thus, the Project Approval must be set aside.

**G. Failure to Comply with California Environmental Quality Act**

The Zoning Administrator's determination (LOD at p. 1) that the Project is exempt from CEQA pursuant to CEQA Guidelines Sections 15301 (Class 1) and 15305 (Class 5) is in error.

The Project is simply not eligible for a categorical exemption from CEQA. Further, the Project is subject to the Significant Effect/Unusual Circumstances Exception of CEQA Guidelines Section 15300.2(c).

**1. Inapplicability of Class 1 (Existing Facilities) and Class 5 (Minor Alterations)**

The Project involves the demolition of the vacant 2,135 square-foot Pizza Hut building and new construction of a 1,479 square-foot drive-through Starbucks coffee shop, a complete change in use and character requiring a Conditional Use Permit and a Zone Boundary Adjustment. This is not a "minor alteration" or "negligible expansion of use" as required by Class 1. The Noise Report, at **Exhibit 1** p. 1, affirmatively states, "...[T]he project is a new construction of a coffee shop and drive-through, as opposed to minor interior/exterior modification or repairs to an existing facility, and introduces new noise sources (such as drive-through traffic, outdoor ordering and pickup interactions, etc.), so does not qualify as a Class 1 project according to CEQA Guidelines Section 15301." Furthermore, the Project seeks a CUP for a drive-through adjacent to residential zoning and four deviations from Commercial Corner Development standards (LOD at p. 2). Granting a highly discretionary CUP, extended operating hours (4:30 AM to 10 PM or 11 PM), and numerous deviations that facilitate a high-intensity vehicular use adjacent to a residential zone is not a "minor alteration in land use limitations" as required by Class 5. The Noise Report, at page 1, references the extended operating hours next to a residential zone, and states that given the potential for significant nighttime noise impacts, the proposed land use alterations cannot be considered "minor" and the Project does not therefore fall into the Class 5 categorical exemption under CEQA Guidelines Section 15305. Further, the Project seeks a Zone Boundary Adjustment to change the zoning on the project site to make the Project fit at the project site. This is further evidence that the Project does not consist of an "minor" alteration of land use limitations.

**2. Significant Effect Due to Unusual Circumstances (CEQA Guidelines §15300.2(c))**

**Unusual Circumstances:** The combination of the Project's specific characteristics, as detailed in the preceding sections, constitutes unusual circumstances with a reasonable possibility of having a significant environmental effect. The site requires a Zone Boundary Adjustment, sits

immediately adjacent to RS-1 single-family homes (buffered only by a 20-foot alley), and is proposing an inherently high-volume vehicular use (drive-through) with operating hours extending until 10:00 or 11:00 PM and starting at 4:30 AM. Therefore, there are unusual circumstances which preclude the usage of a categorical exemption. The Noise Report, at pages 1 to 2, supports the position, and concludes that unusual circumstances preclude the application of a categorical exemption to the project.

**Significant Effect:** Further, the administrative record contains substantial evidence (public testimony, LADOT conditions, etc.) of potential significant effects relating to:

- **Noise:** Constant vehicular noise, car doors, idling, and trash collection noise between 4:30 AM and 10:00 PM or 11:00 PM, impacting the closest adjacent residential properties (LOD at p. 14).
- **Traffic/Safety:** The necessity of mandatory conditions for a "Right Turn Only" egress, prohibition of left turns into the site, and required employee queuing management (Condition 27, 28) demonstrates that the project's design would otherwise result in a significant traffic hazard and disruption.
- **Air Quality:** The inherent queuing and idling of up to 13 cars placed immediately adjacent to residential uses (LOD at p. 13) introduces localized air quality impacts (fumes/toxics) that were not analyzed by the ZA. These negative air quality impacts are compounded when added to the existing queuing of the drive through and other cars going to McDonald's, which is immediately adjacent. Moreover, Appellant has commissioned an expert report from Ray Kapahi of Environmental Permitting Specialists (See **Exhibit 4**) who concluded as follows:
  1. The exhaust from cars in the drive through would exceed the 1-hour NOx stand by a wide a wide margin. The adjacent homes would face exposure to vehicle exhaust 18 hours per day. This risk would continue for 18 hours per day, 365 days per year. For example, residents at the adjacent home located at 23801 Victory Blvd are only 60 feet from the drive-through lane and would be at the highest risk.
  2. Residents would also be exposed to higher concentration of carbon monoxide and fine particulate. The area already violates the air quality standards for PM10 and PM2.5. The additional emissions add to the already poor air quality that the residents must endure.
  3. The NOx emissions would further exacerbate the area that is already suffering from the worst air quality in the country.
  4. Cancer risk from car exhaust would be less than significant (around 1 cancer per million). However, when added to the background risk, the impact is **cumulatively significant**.

The existence of these unusual circumstances and the demonstrated potential for significant impacts remove the Project's eligibility for a categorical exemption from CEQA and require the preparation of an Initial Study to fully assess the environmental effects.

## **II. CONCLUSION**

The Zoning Administrator's determination relies on insufficient and speculative operational conditions to mitigate inherent land use conflicts, noise, and traffic impacts on adjacent RS-1 residential properties. The public testimony provided substantial evidence that the proposed Starbucks drive-through, operating from approximately 4:30 AM until 10:00 PM, Monday through Thursday and until 11:00 PM Friday to Sunday, is too intense and inappropriately located to be compatible with its immediate neighbors.

Appellant reserves the right to supplement these bases for appeal. For the foregoing reasons, the Appeal should be Granted. Thank you for your consideration of this matter.

Sincerely,



Jamie T. Hall

Encls.

**Exhibit 1:** Steve Rogers Acoustics Noise Report

**Exhibit 2:** Table A: 23717 Victory Blvd Project Trip Generation (Fast-Food Pass-By)

**Exhibit 3:** Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan

**Exhibit 4:** Environmental Permitting Specialists Air Quality / GHG Report

# Channel Law Group, LLP

February 2, 2026

Supplemental Justification for Appeal of Case No. ZA-2024-5222-CU2-ZBA, Conditional Use Permit and Zone Boundary Adjustment, and ENV Case No. 2024-5223-CE, 23717 Victory Boulevard  
(Proposed Starbucks Drive-Through)

# **EXHIBIT 1**



Steve Rogers Acoustics

February 1, 2026

Jamie Hall  
Channel Law Group, LLP  
8383 Wilshire Boulevard, Suite 750  
Beverly Hills, CA 90211  
[jamie.hall@channellawgroup.com](mailto:jamie.hall@channellawgroup.com)

Subject: **Starbucks Drive-Through Coffee Shop Project, West Hills  
Review of Noise Impacts**

Dear Jamie:

We have reviewed the Zoning Administrator's (ZA) Letter of Determination dated October 21, 2025 as well as findings provided by the Applicant's representatives – Kaidence Group – and have the following comments:

1. BACKGROUND

The Project is a new drive-through coffee shop to be located on a 23,500 ft<sup>2</sup> lot which fronts onto Victory Boulevard in West Hills, CA. There are adjacent single-family residential properties to the west, including one home that is immediately adjacent (common property line) to the northwest portion of the Project site. The ZA has determined that the project is exempt from the California Environmental Quality Act (CEQA) and has approved a Conditional Use Permit (CUP) and other entitlements, based on findings presented in a Letter of Determination (LOD) dated October 21, 2025. The purpose our review is to confirm the validity of the ZA's findings as they relate to the potential noise impact of the Project on the neighboring homes.

2. PROJECT IS NOT EXEMPT FROM CEQA

a) Project is Not in an Exempt Class

CEQA defines a number of classes of project that are categorically exempt from the requirement for preparation of environmental documents because they do not have a significant effect on the environment. The ZA has determined that the Project is exempt from CEQA because it falls in Class 1 (Existing Facilities per CEQA Guidelines Section 15301) and Class 5 (Minor Alterations in Land Use Limitations per CEQA Guidelines Section 15305).

However, the project is new construction of a coffee shop and drive-through, as opposed to minor interior/exterior modification or repairs to an existing facility, and introduces new noise sources (such as drive-through traffic, outdoor ordering and pickup interactions, etc.), so does not qualify as a Class 1 project according to CEQA Guidelines Section 15301.

Similarly, the proposed alterations in land use limitations include extended operating hours that would allow drive-through traffic adjacent to a residential zone from 4:30 AM to 11:00 PM. Given the potential for significant nighttime (before 7:00 AM and after 10:00 PM) noise impacts, the proposed land use alterations cannot be considered "minor" and the project does not, therefore, fall in Class 5 according to CEQA Guidelines Section 15305.

b) Unusual Circumstances, Probability of Significant Effect

According to Section 15300.2(c) of the CEQA Guidelines:

*"A categorical exemption shall not be used for an activity where there is a reasonable possibility*



*that the activity will have a significant effect on the environment due to unusual circumstances.”*

The Project in this case would include a new drive-through, operating from 4:30 AM to 11:00 PM, very close to residential properties. The unusual circumstances (close proximity to homes) and nature of the activity introduce a reasonable possibility of significant noise impacts – which disqualifies the Project from exemption from CEQA.

### 3. ZA'S LETTER OF DETERMINATION

#### a) Conditional Use Finding #2 is Not Adequately Supported

On page 20 of the LOD, the ZA states that:

*“The applicant will also offer a less than five-foot wide landscaping along the west property line to maintain parking and construct a new 6-foot tall wall, serving as a noise barrier to protect residential uses. The project has been conditioned to shield HVAC equipment to reduce noise and visual impacts.”*

And goes on to conclude that:

*“As conditioned, the project will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare, and safety and the development of the community.”*

However, the ZA provides no analysis other supporting evidence for the claim that a 6-foot high wall and a screen around the rooftop HVAC equipment will be sufficient to eliminate adverse noise impacts in the neighboring homes or to ensure that those residential properties are not degraded by the noise of drive-through operations, which include nighttime operations.

Conditions 17 and 18 (page 4 of the LOD) address the noise impact of outdoor speakers (such as the drive-through speaker box) but not the main sources of noise impact in the Project – namely, the steady flow of vehicles through the drive-through, vehicle doors and music playing in vehicles, interactions at the ordering and pickup points (and with Starbucks staff taking orders outside in the case that the queue exceeds 13 vehicles), etc.

Condition 22 (page 5 of the LOD) requires coordination with vendors to “minimize” driving of delivery trucks in reverse – triggering beeping noises – and to “avoid” driving of trucks in the alley. Condition 22 does not eliminate truck reversing/beeping (which would be impossible in practice) nor does it forbid delivery trucks driving (forward or backwards) in the alleyway. It also places no restrictions on the time that deliveries may occur – presumably opening the possibility of nighttime deliveries as late as 11:00 PM and as early as 4:30 AM. So, as written, Condition 22 does not adequately mitigate adverse noise impacts or ensure that the noise of delivery trucks does not degrade the nearby residential properties.

#### b) Drive-Through Finding #4 is Not Adequately Supported

On page 22 of the LOD, the ZA finds that:

*“The project will also construct a new 6-foot tall wall that will contribute to offsetting any potential noise and disturbances from the drive-through operation. As such, residential uses in the vicinity of a proposed drive-through fast-food establishment will be adequately protected from any significant noise resulting from outdoor speakers, autos, or other sources of noise associated with the lot.”*

However, there is no analysis to show how much a 6-foot wall will reduce noise from cars,



trucks, in-car music, ordering/pickup interactions, etc., or what the net levels of noise received beyond the western property line will homes be. In other words, there is no support for the ZA's claim that the neighboring homes will be "adequately protected" from noise.

#### 4. KAIDENCE GROUP FINDINGS

Kaidence Group has provided several sets of written findings in support of the Project application. These findings include six features of the Project that purport to address noise control, as follows:

- The drive-through is located on the east side of the Project site – away from the residences along the western property line.
- The ordering speaker box is on the eastern property line and its volume will be controlled automatically to minimize sound spillover into the community
- Vehicles (presumably parked or queuing) between the speaker box and homes will act as barriers to noise
- A new 6-foot high wall at the western property line
- The alley between the project site will act as a "buffer" to noise.
- New landscaping will "help mitigate sound".

There is no analysis or other evidence to show that the design of the project will adequately control noise emissions. And, we believe that some of the features touted as noise-mitigation measures will offer very little – or even no – relief to the neighbors in practice. We address each of these features individually as follows:

##### a) Drive-Through Location

Kaidence appears to have focused on the ordering point and speaker box as the primary drive-through noise source. So, by locating the speaker box close to the eastern property line, they reason that the Project design is minimizing noise impact on the neighbors.

However, in reality, the speaker box is not the main source of drive-through noise. It is the vehicles themselves that will like produce the most noise (engines, horns, doors slamming, music being played in vehicles, delivery trucks, etc.) and these vehicles will not be confined to that portion of the drive-through that is closest to the eastern property line. In fact, moving, parked and parking vehicles will be generating noise all over the project site – including close to the western property line and the neighboring homes. So this feature does little to mitigate vehicle/traffic noise impacts on the homes.

##### b) Speaker Box, Volume Control

As discussed in the previous paragraphs, the speaker box is not the main noise concern with the proposed drive-through. It may be true that the automatic volume control prevents loudspeaker noise from being heard beyond the property line (this is also one of the ZA's conditions) but this does nothing to mitigate noise impacts from vehicles/traffic.

##### c) Vehicles as Noise Barriers

Again, this feature seems to assume that the speaker box is the main noise concern so that vehicles situated between the ordering point and the western property line might help block that noise. In fact, these vehicles (whether queuing in the drive-through itself or elsewhere on the site) are a source of noise in their own right – much more significant than the ordering speaker box – and yet are not addressed as such in the Kaidence findings.



d) New 6-foot High Wall

The true value of the proposed 6-foot wall is also questionable – because no calculations are provided in the Kaidence findings to show how much reduction it will provide and what the net levels of noise will be at the nearby homes. The house closest to the project site is slightly elevated and – even with the existing vinyl fence – there are clear lines-of-sight between the windows and the drive-through, speaker box, pickup window, etc.; so the proposed new 6-foot wall will do nothing to reduce noise received at this location.

e) The Alley as a Mitigating Feature

The alleyway to the west is an existing part of the neighborhood rather than a design feature of the Project. And, while the alley does provide a little additional distance between the proposed noise sources on the Project site and the homes that front onto Victory Boulevard, there is no analysis in the Kaidence findings to show how much noise attenuation is achieved by that extra distance. Also, the residential property to the northwest shares a property line with the Project site – there is no alley here.

f) Landscaping

It is simply not true that the proposed landscaping will act as a barrier to noise. It would take a deep (50 to 100-feet), continuous band of densely-planted trees to provide any meaningful reduction in noise. The handful of trees scattered around the site – as shown on the landscaping plan – will provide no noise reduction benefit at all to the neighboring homes.

Yours sincerely,

**Steve Rogers Acoustics, LLC**

Steve Rogers  
Principal

# Channel Law Group, LLP

February 2, 2026

Supplemental Justification for Appeal of Case No. ZA-2024-5222-CU2-ZBA,  
Conditional Use Permit and Zone Boundary Adjustment, and ENV Case No.  
2024-5223-CE, 23717 Victory Boulevard (Proposed Starbucks Drive-Through)

## **EXHIBIT 2**



**Table A: 23717 Victory Blvd Project Trip Generation (Fast-Food Pass-By)**

Land Use	Size	Unit	AM Peak Hour				PM Peak Hour		
			Daily	In	Out	Total	In	Out	Total
<b>Trip Rates <sup>1</sup></b>									
Coffee Shop with Drive-Through		tsf	533.57	43.80	42.08	85.88	19.50	19.49	38.99
Fast Casual Restaurant		tsf	97.14	0.72	0.71	1.43	6.90	5.65	12.55
<b>Project Trip Generation</b>									
Coffee Shop with Drive-Through	1.479	tsf	789	65	62	127	29	29	58
	Pass-By Reductions <sup>2</sup>		(391)	(32)	(30)	(62)	(15)	(14)	(29)
<b>Total</b>			<b>398</b>	<b>33</b>	<b>32</b>	<b>65</b>	<b>14</b>	<b>15</b>	<b>29</b>
<b>Existing Trip Generation</b>									
Fast Casual Restaurant	2.135	tsf	207	2	1	3	15	12	27
<b>Net Trip Generation (Project - Existing)</b>			<b>191</b>	<b>31</b>	<b>31</b>	<b>62</b>	<b>(1)</b>	<b>3</b>	<b>2</b>

<sup>1</sup> Trip rates referenced from the Institute of Transportation Engineers (ITE) *Trip Generation Manual*, 11<sup>th</sup> Edition (2021).

Land Use Code 937 - Coffee/Donut Shop with Drive-Through Window

Land Use Code 933 - Fast Casual Restaurant

<sup>2</sup> Pass-by reductions referenced from the ITE *Trip Generation Handbook*, 3rd Edition (2017).

49.5% Daily, 49% AM, and 50% PM Pass-By assumed for the project (Land Use Code 934 - Fast-Food Restaurant with Drive-Through Window).

tsf = thousand square feet

# Channel Law Group, LLP

February 2, 2026

Supplemental Justification for Appeal of Case No. ZA-2024-5222-CU2-ZBA,  
Conditional Use Permit and Zone Boundary Adjustment, and ENV Case No.  
2024-5223-CE, 23717 Victory Boulevard (Proposed Starbucks Drive-Through)

## **EXHIBIT 3**

**CANOGA PARK-WINNETKA-  
WOODLAND HILLS-WEST HILLS**

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# **Community Plan**

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- III. Land Use Policies and Programs
- IV. Coordination Opportunities for Public Agencies
- V. Urban Design



# CANOGA PARK-WINNETKA- WOODLAND HILLS-WEST HILLS

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# Community Plan

## Chapter I INTRODUCTION

### COMMUNITY BACKGROUND

#### PLAN AREA

The Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan Area is in the southwest San Fernando Valley. The Community Plan Area covers 17,887 acres -- approximately 6 percent of the land in the City of Los Angeles. Planning communities that border this CPA are Chatsworth-Porter Ranch, Reseda-West Van Nuys, Encino-Tarzana, the Cities of Hidden Hills and Calabasas, and portions of Los Angeles and Ventura Counties.

A diverse natural and socioeconomic landscape characterize this Community Plan Area. Dominant on the natural landscape are the Simi Hills of West Hills, the hillsides of the Santa Monica Mountains and the Chalk Hills of Woodland Hills, and the valley plain in Canoga Park and Winnetka. Initially an agricultural cattle oriented community, the area has undergone substantial residential and commercial development over the last fifty years. As agriculture gave way to industry, the aerospace industry transformed the Community Plan Area. Today the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan Area offers a diverse range of housing opportunities and is the economic hub of the San Fernando Valley.

The Community Plan Area consists of four community subareas, each with a distinct identity.

#### **Canoga Park**

Settled early this century, Canoga Park is the heart of the West San Fernando Valley. Located within the boundaries of Roscoe Boulevard to the north, south to Victory Boulevard, Fallbrook Avenue to the West and De Soto Avenue to the east, this area contains a diversity of housing and commercial activity. The traditional main street commercial corridor is being reestablished as a community hub for cultural and social activities.

## **Woodland Hills**

This subarea lies in the southern portion of the Community Plan Area. The boundaries run generally along Victory Avenue from Corbin Street to Topanga Canyon Boulevard, Topanga Canyon Boulevard to US 101, US 101 Freeway west to the City limits, and the Santa Monica Mountains on the south. This subarea contains a variety of predominantly single family homes and is home to Pierce College and Warner Center.

## **West Hills**

This primarily single-family neighborhood is bound by Roscoe Boulevard to the north, Topanga Canyon Boulevard on the east, the Ventura Freeway to the South, and the Simi Hills on the South and Southwest.

## **Winnetka**

One of the earliest subareas to be settled, this community is bound by Roscoe Boulevard on the north, Corbin Avenue on the east, Victory Boulevard on the South, and De Soto Avenue on the West.

## **Specific Plans**

The Ventura/Cahuenga Boulevard Corridor, Warner Center and Mulholland Scenic Parkway Specific Plans address the unique opportunities and challenges associated with commercial and residential development within the Canoga Park-Winnetka-Woodland Hills-West Hills plan area.

### **Ventura/Cahuenga Boulevard Corridor Specific Plan**

The goals of the Ventura/ Cahuenga Boulevard Corridor Specific Plan are to assure an equilibrium between transportation infrastructure and land use development. The Specific Plan provides for an effective local circulation system; promotes attractive and harmonious site design for commercial development; provides compatible and harmonious relationships between commercial and residential areas when adjacent to each other; promotes and encourages development of pedestrian activity; and maintains the distinct character of each of the five Specific Plan communities located within its boundaries.

### **Mulholland Scenic Parkway Specific Plan**

The goals of the Mulholland Scenic Parkway Specific plan are to assure maximum preservation and enhancement of the parkways's outstanding and unique scenic features and resources. The Specific Plan assures that design and placement of buildings and other improvements preserve, complement and/or enhance views; minimizes grading and assures that graded slopes have a natural appearance. Additionally, the plan seeks to preserve the natural appearance compatible with the characteristics of the Santa Monica Mountains; to protect prominent ridges, trees and environmentally sensitive areas; and protect all identified archaeological and paleontological resources.

### **Warner Center Specific Plan**

The Goals of the Warner Center Specific Plan are to coordinate orderly commercial and residential development with transportation improvements. The Specific Plan protects residential neighborhoods from the intrusion of through traffic, establishes a hierarchy of land use intensity which decreases with distance away from the Warner Center Core, encourages mixed-use development within Warner Center in accordance with the city's goal to improve the jobs/housing relationship.

The purpose of the Specific Plan is to make Warner Center a vibrant environment, providing daytime and nighttime activities; preserve existing high technology industrial and research uses, encourage opportunities to stimulate human interaction and pedestrian activity.

The Specific Plan does this through establishment of urban design, landscaping and sign control standards to insure that the high quality of development in Warner Center is maintained; encourage art work in public spaces; and provide child care facilities for the employees of Warner Center businesses.

### **COMMUNITY PARTICIPATION**

The State of California requires citizen participation in the preparation or amendment of community plans. General Plan Government Code Section 65351 reads, "During the preparation or amendment of the general plan the planning agency shall provide opportunities for the involvement of citizens, public agencies, public utility companies, civic, education, and other community groups through public hearings and any other means the city or county deems appropriate."

Drafting of the first community plan involved members of the community who helped to identify and define the needs, desires, resources, and the unique nature of the community. Subsequent changes in the plan have served to broaden the community participation that took place with the formation of the original plan. Community participation helps to update the plan by identifying changes that have taken place since its adoption.

## **COMMUNITY ISSUES AND OPPORTUNITIES**

The following summarizes the most significant planning and land use issues and opportunities identified in the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan Area:

### **RESIDENTIAL**

#### **Issues**

- Need to preserve existing single family neighborhoods.
- Cumulative effects if permitted development exceeds infrastructure capacity.
- Need for more affordable senior housing.

- Compatibility between residential and industrial uses.
- Blighting impact of vacant and obsolete commercial development on adjacent residential neighborhoods.
- Preservation of single-family housing stock in older residential neighborhoods.
- Lack of open space in apartment projects.

#### **Opportunities**

- Support efforts of active homeowners groups working to promote identification, preservation and rehabilitation of local neighborhoods.
- Use land use policies to support ongoing affordable home-ownership and rehabilitation programs in older single family neighborhoods.
- Maintain the diversity of housing opportunities that exists in the CPA.
- Encourage residential and mixed use development along commercial corridors.
- Maintain access and proximity to employment.

## **COMMERCIAL**

#### **Issues**

- Oversupply and poor appearance of strip mall development and obsolete commercial space on major thoroughfares.
- Lack of overall parking and access within commercial strips due to physical constraints such as shallow commercial lot depths.
- Unsightliness of new construction due to the lack of landscaping, architectural character and scale.
- Inadequate transition between commercial and residential uses.

#### **Opportunities**

- Promote the character, economic viability, and quality of existing and new commercial development through design guidelines.
- Establish appropriate transition between commercial (mixed use) and adjoining, primarily residential, uses.
- Create pedestrian/friendly shopping areas by incorporating street trees, benches, convenient parking/access, and maintaining retail frontage at ground level.
- Improve physical appearance of commercial districts and reduce scale of commercial development adjacent to residential neighborhoods.

- Complement any unique existing development/uses to reinforce desirable design characteristics and uses.

## **INDUSTRIAL**

### **Issues**

- No separation exists between industrial uses and residential neighborhoods in many older parts of the Community Plan Area.
- The viability of older industrial districts is challenged by changing site and location requirements of modern industrial uses.
- Contraction of industrial sectors important to the west valley, such as aerospace, has led to a loss of local manufacturing jobs and the need to find new industrial uses.

### **Opportunities**

- Excellent access to regional freeways and rail services.
- Availability of industrial sites within Warner Center to meet the location and site specifications of clean modern industrial users.
- Apply strategies traditionally used in commercial districts to revitalize older industrial districts. Such strategies may include establishment of business improvement districts and design guidelines.
- Encourage recycling of unused industrial sites by uses linked to expanding sectors of the regions economy.
- Increasing presence of desirable (“clean”) industrial uses, thus generating less harmful pollutants and lower noise levels.
- Use of appropriate administrative review for major expansions of existing industrial sites when located near residential uses.
- Availability of sites planned for job producing uses that improve the economic and physical condition of the area.

## **TRANSPORTATION**

### **Issues**

- The Community Area is poorly served by Public Transportation.
- Generalized congestion exists in many parts of the Community Plan Area.
- Traffic speed and/or volume compromises safety and quality of many residential neighborhoods and some commercial areas.
- Ahmanson Ranch development use of Victory Boulevard may adversely impact CPA traffic, particularly on Valley Circle Boulevard.

### **Opportunities**

- C MTA Right-Of-Way offers the opportunity for a variety of public transportation improvements including light rail, busways, and pedestrian/bike trails.
- C Expanded use of the “Smart Bus” or Dash lines in CPA can relieve traffic congestion and provide residential service to commercial centers.
- C Traffic calming measures in residential areas and in pedestrian oriented commercial areas may improve neighborhood quality.

## **RECREATION, PARKS, AND OPEN SPACE**

### **Issues**

- C Addition, expansion and/or improvement of needed local parks throughout the Community should be accelerated, where feasible.
- Continued development of Equestrian, Hiking and Bicycle Trails.

### **Opportunities**

- C Continued efforts to establish State and local park sites within the hillside areas.

## **MAJOR DEVELOPMENT OPPORTUNITY SITES**

Several areas have been identified as major opportunity sites:

### **Gateway To The City**

Since 1972, the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan has included the “Gateway To The City” concept. This concept initially envisioned a commercial development where the U.S. 101 freeway enters the valley. This entrance is framed by the Santa Monica Mountains and the Simi Hills and offers a panoramic vista of the San Fernando Valley.

Today, the Gateway to the Valley is Warner Center. Warner Center is a preeminent center for commerce, cultural, and civic life at the entrance to the San Fernando Valley. Below is a summary of major issues that should be considered with regard to continuation of the Gateway to the City concept:

### **Issues**

- C Scenic vistas should be preserved at entrance of U.S. 101 freeway into the San Fernando Valley.
- C Gateway related development shall be within Warner Center.

### **Opportunities**

- C Enhance entrance to City by preservation of scenic vista.
- C Identify Warner Center as the Commercial Gateway to the San Fernando

Valley and the City and promote commercial districts along Ventura Boulevard, parallel to the U.S 101.

### **Pierce College Agricultural Land**

The Los Angeles Pierce College is an important part of the Community Plan Area's History. Its agricultural program is one of the few remaining connections to the community's agrarian past. A legacy of this program is preservation of a sizable and environmentally important piece of publicly held open space. Potential development of this site could occur due to fiscal pressures and changing priorities of the Los Angeles Community College District.

The following is a summary of major issues that should be considered for any future development of this site.

#### **Issues**

- C Open Space portion of Pierce College is an environmentally sensitive resource.
- C Current use of land is an important educational resource for the Community Plan Area.
- C The Community Area is well served by existing commercial land. No new commercial land is needed for the life of this Community Plan. Adequate commercial land exists in Warner Center and in nearby Community Commercial Centers.

#### **Opportunities**

- C Pierce College represents a rare opportunity to preserve a significant, publicly held Open Space.
- C Master Plan for Pierce College Campus may provide opportunity to integrate future use of this site with needs of the community.

### **MTA Right-Of-Way**

The former Southern Pacific Railway ROW, now owned by MTA, is an important development opportunity for the community. Among potential uses are public transportation in the form of light rail or busways, recreational uses in the form of bike/walking/equestrian trails, or opportunities for industrial development where it runs contiguous to existing industrial areas.

#### **Issues**

- C Future rail development of this property is uncertain. Alternative uses for this property should maintain potential rail ROW.
- C Poor maintenance in absence of active use.

### **Opportunities**

- C Potential to connect bikeways with those to the eastern end of the valley.
- C Possibility of use for busways.
- C Opportunities to provide increased parking or development opportunities for adjacent commercial and industrial uses.

### **Treeland Nursery**

The site of the Treeland Nursery, the northwest corner of Valley Circle and Long Valley Road, is of a size and configuration to accommodate substantial commercial development. Currently land use on the site is divided between General Commercial and Very Low Residential land use. A footnote will be placed on the site requiring any commercial zone change to include a [Q] condition limiting the Floor Area Ratio to 0.5:1, a thirty foot height limit, and uses limited to those first permitted in the C1.5 zone, and requiring a minimum 25 foot setback from any residential property. The limited Floor Area Ratio is consistent with commercial zoning for all properties with Neighborhood or general commercial designations.

Below is a summary of issues and opportunities to be considered in any development proposal for that portion of the site currently designated for General Commercial uses or for any future expansion of the General Commercial designation on the remaining portion of the Treeland property:

### **Issues**

- C The Treeland Nursery Site is in a predominantly residential area. Development must be compatible with residential character of the area.
- C Scenic vistas that characterize the location need to be preserved.
- C Existing access off Long Valley Road provides unacceptable primary access for any major commercial development of the site. Secondary access shall be considered only after traffic study completed by the Los Angeles Department of Transportation as part of any discretionary review for the subject property.
- C The location is well served by Community Commercial activity at the El Camino Shopping Center and along Ventura Boulevard.

### **Opportunities**

- C Ensure environmentally sensitive design and compatibility with adjacent residential areas through development standards. These standards shall include:
  - a. Limit Floor Area Ratio to .5:1.
  - b. Require adequate buffers between commercial activity and residential neighbors.

- c. Attractive landscaping throughout the site.
  - d. A maximum height limit of 30 feet for all structures developed on the property.
- Ⓒ Commercial uses on this property shall be limited to those first permitted in the C1.5 zone.
  - Ⓒ Approval of any commercial zone on the subject property shall include an equestrian trail easement to the trail backbone of the Santa Monica Mountains Conservancy Trail System.

### **Canoga Park Industrial Corridor**

This industrial corridor extends from Vanowen Street to Roscoe Boulevard and rests between Canoga Boulevard and Deering Avenue. Initially developed by rail oriented industries, the corridor runs parallel to the former Southern Pacific Railway, this industrial corridor is a traditional source of employment in the Community Plan Area. While still predominantly industrial, this corridor is in a period of transition and opportunities for recycling exist. Policies and programs to promote clean industrial uses such as light assembly, research, and subcontracting specialties related to the motion picture industry should be promoted.

#### **Issues**

- Ⓒ Proximity to residential properties
- Ⓒ Poor maintenance, unsightly appearance and dilapidated condition of structures.
- Ⓒ Proliferation of low-end uses such as auto repair establishments.

#### **Opportunities**

- Ⓒ Access and proximity to employment for local residents.
- Ⓒ Existing internal circulation system.
- Ⓒ Potential for expansion on former rail easement.
- Ⓒ Use of traditional commercial revitalization tools such as Business Improvement Districts and Design Guidelines to improve district aesthetics and attract clean manufacturing uses.

## **NEIGHBORHOOD CHARACTER**

Preserve and enhance the positive characteristics of existing uses which provide the foundation for community identity, such as scale, height, bulk, setbacks and appearance.

### **Issues**

- C Scale, density and character of multiple dwelling housing adjacent to single-family homes.
- C Scale, density and character of strip mall and neighborhood commercial development adjacent to residential areas.
- C Impact on street parking from new high density apartments.
- C The need to preserve and rehabilitate historic areas with a sensitivity to the character of the established neighborhood.

### **Opportunities**

- C Development of design guidelines for commercial and industrial areas adjacent to residential neighborhoods.
- C Appropriately scaled commercial development in neighborhood commercial centers.
- C Inclusion of mixed use development in commercial areas adjacent to transit station stops.

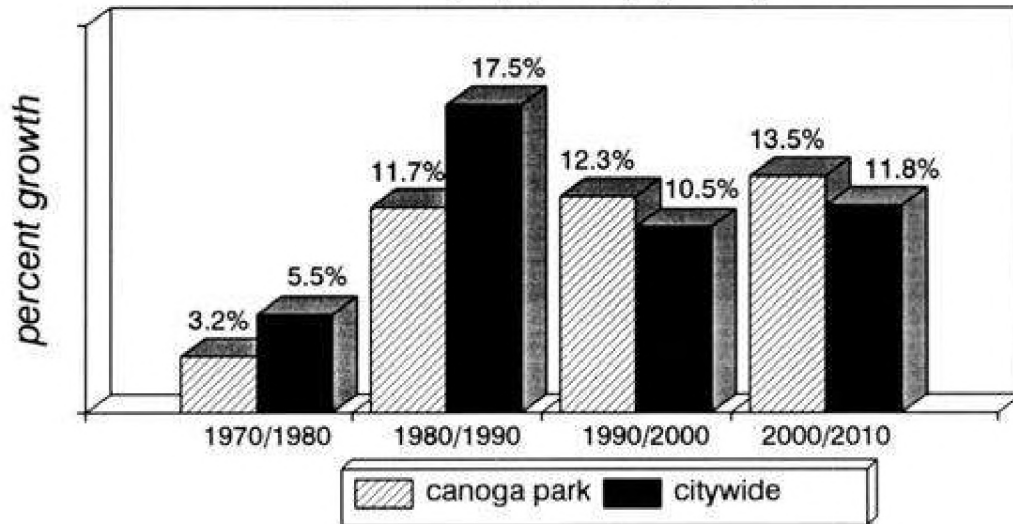
## **COMMUNITY PROFILE**

The Community Profile provides an overview of population, housing, and socio/demo-graphics for the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan Area and compares it to the rest of the City. The following tables contain the statistical data for previous census dates and rates of growth.

# CANOGA PARK/WEST HILLS/WINNETKA/WOODLAND HILLS COMMUNITY PROFILE

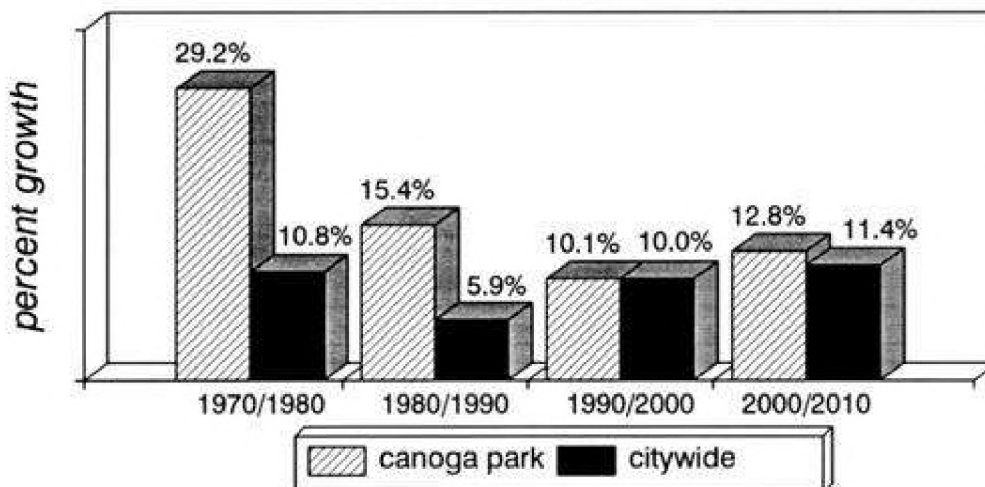
	estimated data (from U.S. Census)			projections (from SCAG) *	
	1970	1980	1990	2000	2010
<b>total population</b>					
canoga park	130694	134844	150560	169109	191892
citywide	2811801	2966850	3485398	3852993	4306564
<b>growth rate</b>					
canoga park		1970 to 1980	1980 to 1990	1990 to 2000	2000 to 2010
citywide		3.2%	11.7%	12.3%	13.5%
		5.5%	17.5%	10.5%	11.8%

**population growth rate comparison**  
(includes group quarters population) \*\*



	1970	1980	1990	2000	2010
	<b>total households</b>				
canoga park	37311	48206	55643	61260	69079
citywide	1024873	1135491	1203052	1323882	1474514
<b>growth rate</b>					
canoga park		1970 to 1980	1980 to 1990	1990 to 2000	2000 to 2010
citywide		29.2%	15.4%	10.1%	12.8%
		10.8%	5.9%	10.0%	11.4%

**household growth rate comparison**  
(occupied dwelling units only)



\* Southern California Association of Governments; a regional council of county and municipal governments that includes Imperial, Los Angeles, Orange, Riverside, San Bernardino, and Ventura Counties.  
 \*\* Population in group quarters includes institutionalized individuals, students in dormitories, and persons in emergency shelters, migrant worker housing, halfway houses, nursing homes, military quarters, etc.

# CANOGA PARK – COMMUNITY PROFILE

estimated data (from U.S. Census)

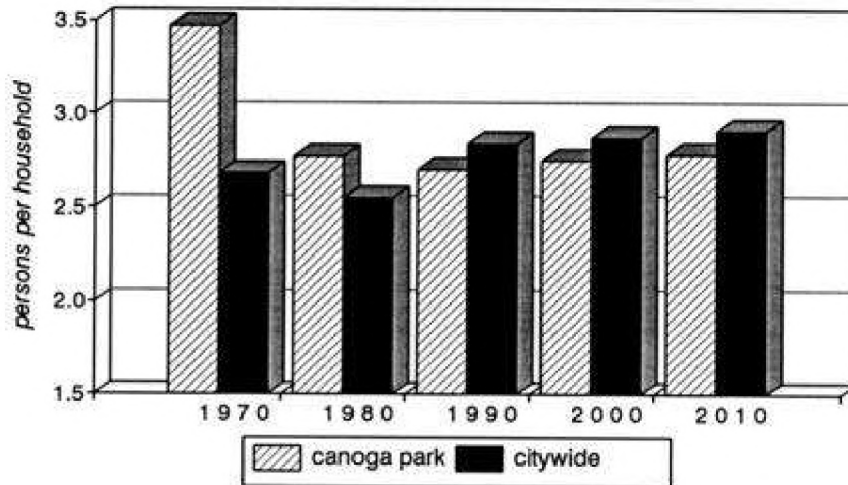
projections (from SCAG)

## household size

(persons per dwelling unit) \*

	1970	1980	1990	2000	2010
canoga park	3.47	2.77	2.70	2.75	2.78
citywide	2.68	2.55	2.84	2.87	2.91

## household size comparison

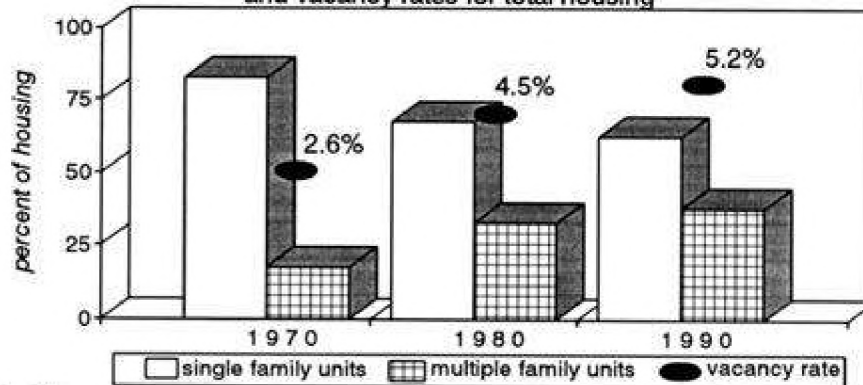


## housing splits / vacancy factors \*\*

(canoga park only)

	1970	1980	1990
single family dwellings	82%	68%	62%
multiple family dwellings	18%	32%	38%
vacancy rate (total housing)	2.6%	4.5%	5.2%

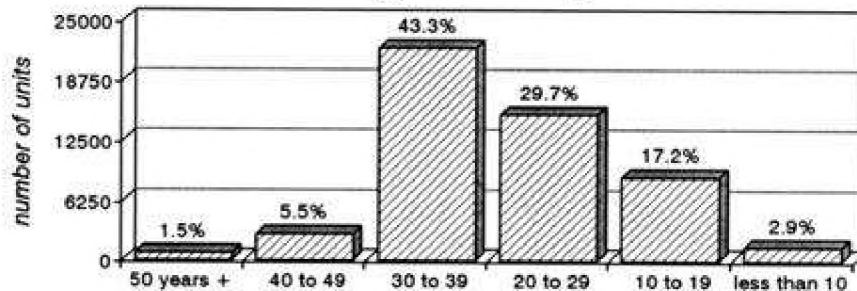
## single family dwelling units, multiple family dwelling units, and vacancy rates for total housing



## age of housing as of 1994 \*\*\*

total dwellings in canoga park (includes vacant and occupied units).

## age of housing



\* Count of all persons in occupied dwellings. Does not include group quarters population.

\*\* Housing splits are defined by the presence of a common wall between two or more dwelling units. Typical multiple family units include condominiums and apartments. Typical single family units include detached structures.

\*\*\* Source of this information is the Los Angeles County Assessor. Data derived from the Assessors LUPAMS (Land Use Planning and Management Subsystem) file. File date is mid 1994.

## CANOGA PARK – COMMUNITY PROFILE HOUSING and OCCUPANCY FACTORS

### 1990 census data;

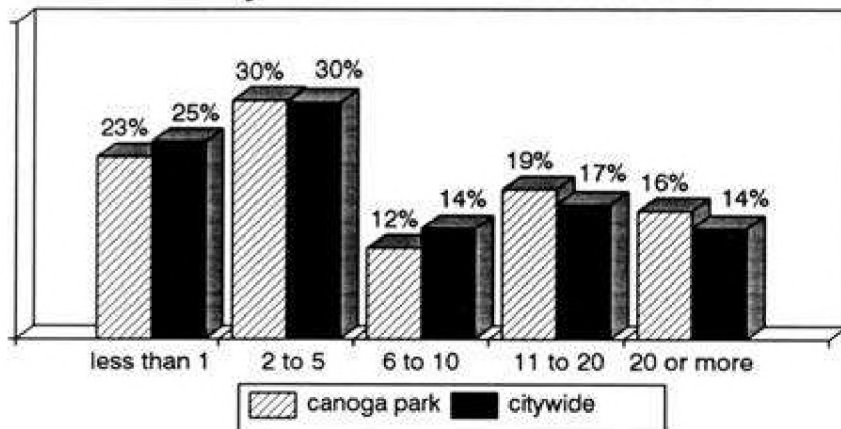
<u>cost of housing (as a percent of income) *</u>	20% or less	20% to 29%	30% or more
owner occupied housing units	43.6%	21.6%	34.8%
renter occupied housing units	23.3%	24.0%	52.7%

<u>cost of housing (owner occupied units)</u> (value estimated by owner)	under \$100,000	\$100,000 to \$200,000	\$200,000 to \$300,000	\$300,000 to \$500,000	\$500,000 or more
canoga park	1.4%	11.7%	41.8%	34.7%	10.4%
citywide	8.4%	28.5%	25.9%	21.4%	15.8%

<u>cost of housing (renter occupied units)</u> (monthly cost estimated by resident)	under \$300	\$300 to \$500	\$500 to \$750	\$750 to \$1,000	\$1,000 or more
canoga park	3.3%	11.6%	41.2%	24.2%	19.7%
citywide	10.9%	29.9%	38.3%	13.1%	7.9%

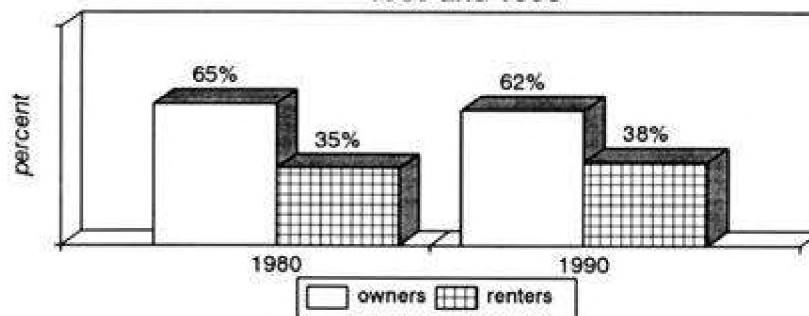
<u>stability indicator (percent) **</u> (length of time in the community)	less than 1 year	2 to 5 years	6 to 10 years	11 to 20 years	20 years or more
canoga park	23.1%	30.2%	11.5%	18.9%	16.3%
citywide	25.1%	30.3%	13.7%	16.9%	14.1%

### years at same address



### residential tenure (ownership status)

### owners/renters 1980 and 1990



**NOTE:** All information included on this sheet calculated on basis of householders response to census questionnaire.  
 \* Sums to 100% by type of housing. This is a distributed calculation of all householders who responded to census questions about cost of housing. Approximately 90% of all householders responded.  
 \*\* Describes length of time living at the same location. Owners and renters combined.

## CANOGA PARK – COMMUNITY PROFILE SOCIO/DEMOGRAPHICS

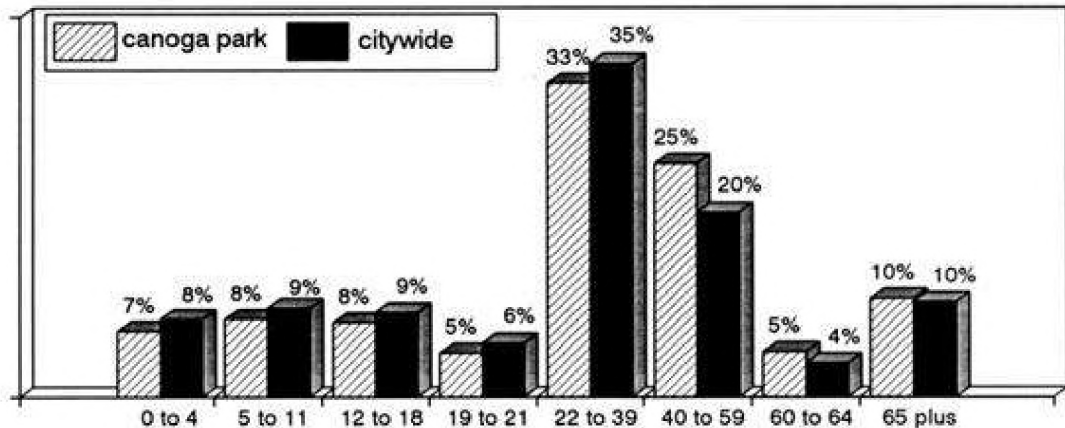
### 1990 census data;

<u>employment (percent) *</u>	
females employed	43.8%
males employed	56.2%
employment participation rate (citywide rate)	72.7%
	67.3%

<u>household income (1989) **</u>	
average	\$59,746
(citywide)	\$45,701
poverty (percent)	6.9%
(citywide)	18.9%

<u>education (percent) ***</u>	high school	beyond high school	college graduate
canoga park	83.9%	62.8%	30.1%
citywide	67.0%	47.8%	23.0%

### age of the general population

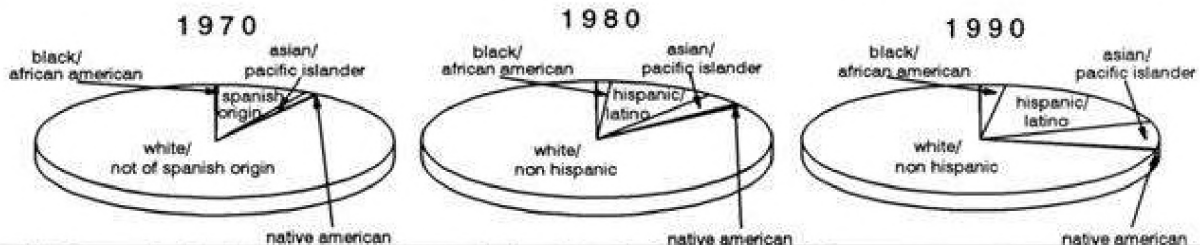


<u>language and citizenship (percent) ****</u>	foreign language spoken at home			foreign born
	spanish	asian	other language	
canoga park	14.0%	5.5%	10.1%	29.8%
citywide	24.3%	6.0%	5.6%	44.9%

<u>means of transportation to work (percent) *****</u>	drive alone	vanpool/ carpool	public transit	other means
	canoga park	77.2%	12.7%	2.3%
citywide	65.2%	15.4%	10.5%	10.5%

<u>head of household *****</u>	live alone	married with children	married no children	single parent	single non family
	canoga park	24.8%	27.0%	32.2%	7.4%
citywide	31.0%	24.3%	22.0%	12.7%	10.0%

<u>race/ethnicity (percent) *****</u>	1970	1980	1990
asian/pacific islander	1.1%	3.5%	8.2%
black/african american	0.1%	1.3%	2.3%
hispanic/latino	8.2%	9.9%	17.8%
native american	0.2%	0.5%	0.3%
white-non hispanic	90.3%	84.8%	71.4%



**NOTE:** All information included on this sheet calculated on basis of householders response to census questionnaire.

\* Civilian persons 16 years or older. Employment participation measures only persons eligible to work; therefore, students, retirees, housewives, military personnel, etc. are not included in this calculation.

\*\* See the note above. Poverty is calculated on the basis of all persons surveyed (98% of citywide population). Only persons 25 years or older are included in this calculation.

\*\*\* Persons 5 years or older (except for foreign born which excludes anyone under the age of 18).

\*\*\*\* Total workers 16 years of age or older. Includes military personnel.

\*\*\*\*\* Adult person acknowledged as representing the household in response to census questionnaire. Household may consist of any number of persons or families.

\*\*\*\*\* Census definition of hispanic/latino persons changed after 1970. Previously described as "spanish origin".

## **Chapter II**

# **FUNCTION OF THE COMMUNITY PLAN**

Chapter 2 of the Plan Text contains the statutory requirements for the Community Plan outlining the mandatory elements that must be addressed. The Chapter contains the explanations of the Role, Purpose, and Organization of the Community Plan. Chapter 2 shows the relationship to other General Plan elements and provides for Plan Monitoring and Consistency.

### **STATUTORY REQUIREMENTS**

California State Law (Government Code Section 65300) requires that each city prepare and adopt a comprehensive, long-term general plan for its development. It must contain seven mandatory elements including land use, circulation, housing, conservation, open space, noise, and safety. California State law requires that the Land Use Element be prepared as part of the city's General Plan, and that it correlate with the Circulation Element. In the City of Los Angeles thirty-five community plans comprise the City's Land Use Element.

The Land Use Element has the broadest scope of the State required General Plan elements, since it regulates how land is to be utilized. It correlates to many of the issues and policies contained in all other plan elements.

Government Code Section 65302(a) requires a land use element to designate the proposed general distribution, general location, and extent of uses of the land for housing, business, industry, open space (including agriculture, natural resources, recreation and enjoyment of scenic beauty), education, public buildings and grounds, solid waste disposal facilities, and other categories of public and private uses of land. The Land Use Element shall include a statement of the standards of population density and building intensity recommended for the various communities and other territory covered by the plan.

The Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan is a part of the General Plan of the City of Los Angeles. It consists of this text and the accompanying map. The Community Plan text states the goals, objectives, policies, and programs. The Community Plan Map, footnotes and legend outline the arrangement and intensities of land uses, the street system, and the locations and characteristics of public service facilities.

### **ROLE OF THE COMMUNITY PLAN**

The General Plan is the fundamental policy document of the City of Los Angeles. It defines the framework by which the City's physical and economic resources are to be managed and used over time. The Plan guides decisions regarding land use, building design and character, open space, housing

conservation and development, provision of supporting infrastructure and public and human services, protection of environmental resources, and protection of residents from natural and man-caused hazards.

The intent of the Community plan is promotion of an arrangement of land uses, streets, and services which will encourage and contribute to the economic, social and physical health, safety, welfare, and convenience of the people who live and work in the community. The plan also guides development to create a healthful and pleasant environment. Goals, objectives, policies, and programs are created to meet the existing and future needs and desires of the community through the year 2010. The general plan clarifies and articulates the City's intentions with respect to the rights and expectations of the public, property owners, prospective investors and business interests. Through the Community Plan, the City can inform these groups of its goals, policies, and development standards, thereby communicating what is expected of the City government and private sector to meet its objectives.

The Community Plan ensures that sufficient land is designated to provide for the housing, commercial, employment, education, recreational, cultural, social, and aesthetic needs of the residents of the plan area. The Plan identifies and provides for the maintenance of any significant environmental resources within the Plan Area. The Plan also seeks to enhance community identity and recognizes unique neighborhoods within the Plan area.

## **PURPOSE OF THE COMMUNITY PLAN**

The last comprehensive update of the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan was completed in 1984 and later revised through the General Plan Zoning Consistency Program required by AB283 in 1987, and through on-going Periodic Plan Review and other Plan amendments. In the past 20 years, the community has shown a growth rate lower than the overall rate for the city. During the 1970's, community population increased by 4,150 residents, an increase of 3.2%. Since 1980, the community's population has grown by 15,716 residents-- an increase of 11.7%.

This update of the Community Plan reflects current conditions and the prevailing visions and objectives of the area's residents, property and business owners.

## **ORGANIZATION AND CONTENT OF THE COMMUNITY PLAN**

This Plan sets forth goals, objectives, policies, and programs for the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan Area. Broader issues, goals, objectives, and policies are provided by the Citywide General Plan Framework.

The Plan is organized and formatted to simplify periodic updates. The State recommends that the entire plan be comprehensively reviewed every five years to reflect new conditions, local attitudes, and technological advances.

The principal method for the implementation of the Land Use Map is the Zoning Ordinance. The City's Zoning Map must be updated to remain consistent with the adopted Land Use Map. Together, the Zoning Ordinance and the Zoning Map will identify specific types of land use, intensity of use and development standards applicable to specific areas and parcels of land within the community.

## RELATIONSHIP TO OTHER GENERAL PLAN ELEMENTS

The City of Los Angeles has the responsibility to maintain and implement the City's General Plan. Since State Law requires that the General Plan have internal consistency, the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan must be consistent with other Elements and components of the General Plan.

The Citywide General Plan Framework is the umbrella concept of the General Plan that will provide the overall guiding vision for Los Angeles into the 21st century. It is based on a directed growth strategy that targets residential and commercial growth along boulevards and corridors and clustered development around community focal points and high activity centers. The General Plan Framework is a special element of the General Plan which looks to the future as required by law and replaces the Centers concept, which was adopted by the City Council in 1974, as the City's long-range development strategy.

The Citywide General Plan Framework Element neither overrides nor supersedes the Community Plans. It guides the city's long range growth and development policy, establishing citywide standards, goals, policies, and objectives for citywide elements and community plans. The Framework is flexible, suggesting a range for uses within its land use definitions. Precise determinations are made in the Community Plans.

The General Plan Framework provides the following projections for year 2010 population, housing and employment levels for the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan Area:

<i>Population (2010) projection:</i>	<i>191,892</i>
<i>Employment (2010) projection:</i>	<i>142,400</i>
<i>Housing (2010) projection:</i>	<i>87,187</i>

The above population, employment and housing numbers are provided as reference during the Community Plan update. These figures are estimates derived from regional data that are then disaggregated to the City and community levels. Population, jobs and housing could grow more quickly, or slowly, than anticipated depending on economic trends.

Regional forecasts do not always reflect the adopted community plan land use capacity or buildout estimated from planned land use. Plan capacity or build out is also an estimate based on specific assumptions regarding development density and household size. Actual plan build out may be more or less than estimated. It should be noted that community plan capacity does not include housing in commercial districts nor the current residential

vacancy rate. The Community Plan includes appropriate policies and implementation measures generated from the mitigation measures listed in the environmental clearance.

In addition to the seven State-mandated elements, the City's General plan includes a General Plan Framework Element, a Public Facilities and Historical Preservation/Cultural Resources element, and an Air Quality Element. All provisions and requirements of the General Plan elements apply to the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan.

Neighborhood plans involve the preparation of special plans that blend both policy and implementation functions for unique neighborhoods within a community. Besides these neighborhood plans, overlay zones also combine policy and implementation functions to address issues peculiar to a specific neighborhood.

The Community Plan includes appropriate policies and implementation measures generated from the mitigation measures listed in the environmental clearance. In Many instances these measures encompass the policies contained in the General Plan Framework.

## **PLAN CONSISTENCY**

Each plan land use category shows the corresponding zones permitted by the Plan. However, development may be further restricted by the Plan text, footnotes, adopted Specific Plans or other specific limitations on discretionary approvals. The Plan recognizes that the residential densities, commercial intensities and industrial intensities depicted on the Plan Map are theoretical and will not occur due to plan and zone regulations, economic conditions, and design limitations.

For each plan category, the Plan permits all identified corresponding zones, and those zones that are more restrictive, as referenced in Section 12.23 of the Los Angeles Municipal Code (LAMC). Any subsequent action that modifies the Plan or any monitoring review that results in changes to the Plan must make new Plan consistency findings at the time of the decision.

City actions on most discretionary projects require a finding that the action be consistent or in conformance with the General Plan. In addition to the required general finding, decision-makers acting on certain projects in the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan Area shall refer to each of the applicable additional findings that the Plan identifies as programs, policies or objectives in Chapter III of the Plans which are underlined for ease of reference. To further substantiate the consistency findings, decision makers may cite other programs, policies, or objectives which would be furthered by the proposed project. In addition, Chapter V of the Plan requires a decision maker to make a finding of conformance with applicable design standards for discretionary projects.

## **PLAN MONITORING**

The Plan has a land use capacity greater than the projected development likely to occur during the Plan period. During the life of the Plan, growth will be monitored and reported in the City's Annual Report on Growth and Infrastructure which will be submitted to the City Planning Commission, Mayor, and City Council. In the fifth year following Plan adoption (and every

five years thereafter), the Director shall report to the Commission on the relationship between population, employment, housing growth and plan capacities. If growth has occurred faster than projected, a revised environmental analysis will be prepared and appropriate changes recommended to the Community Plan and zoning. These Plan and zoning changes and any related moratorium or interim control ordinances, shall be submitted to the Planning Commission, Mayor, and City Council as specified in the Los Angeles Municipal Code.

## **Chapter III**

# **LAND USE POLICIES AND PROGRAMS**

Chapter 3 of the Plan Text Contains Land Use Planning Goals, Objectives, Policies, and Programs. Organized by land use category, this section is divided into residential, commercial, and industrial land use and public and institutional service system categories. The Planning Department is responsible for the goals, objectives, policies, initiation and direct implementation of the programs contained in Chapter 3.

### **RESIDENTIAL**

The quality of life and stability of neighborhoods throughout Canoga Park-Winnetka-Woodland Hills-West Hills depends on providing infrastructure resources (i.e.: police, fire, water, sewerage, parks, traffic circulation, etc.) commensurate with the needs of its population. If population growth occurs faster than projected and without needed infrastructure improvements to keep pace with that growth, the consequences for livability within the Community Plan Area could be problematic.

Accordingly, the proposed Plan has three fundamental premises. First, is limiting single family residential densities in various neighborhoods to the prevailing density of development in these neighborhoods. Second, is the monitoring of population growth and infrastructure improvements through the City's Annual Report on Growth and Infrastructure with a report to the City

Planning Commission every five years on the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan following Plan adoption. Third, if this monitoring finds that population in the Plan area is occurring faster than projected; and that infrastructure resource capacities are threatened, particularly critical resources such as water and sewerage; and that there is not a clear commitment to at least begin the necessary improvements within twelve months; then building controls should be put into effect, for all or portions of the Canoga Park-Winnetka-Woodland Hills - West Hills, until land use designations for the Community Plan and corresponding zoning are revised to limit development.

Existing residential land use patterns vary greatly according to local conditions in the neighborhoods and communities which comprise the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan. Topography, population characteristics, housing markets, location of employment centers, and transportation infrastructure have great influence on the type, location and density of development throughout the community.

The Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan Area offers a diversity of housing opportunities and strong neighborhoods. Predominantly single family in nature, housing opportunities exist for a wide range of incomes. Historically, the majority of the area has been planned for residential purposes. Therefore, current plan policy provides for continued preservation of the existing residential neighborhoods throughout the area, retaining existing single family districts and multi-family clusters. The Plan designates residential land use densities as indicated in the

following table. The Table depicts the reasonable expected population and dwelling unit count for the year 2010, using the mid-point of the range for the dwelling units per net acre category. The midpoint represents a reasonable factor to use, as new development within each land use category is not likely to occur at one of the other extremes of the range but rather throughout the entire range.

## PLAN POPULATION AND DWELLING UNIT CAPACITY

Residential Land Use Category	Dwelling Units Per Net Acre Midpoint	Number of Dwelling Units	Net Acres	Persons Per Dwelling Unit (2010)	Reasonable Exp. Population
<b>Minimum</b>	0.5 (0 - 1)	499.68	999.36	3.03	1,514.04
<b>Very Low</b>	2.5 (1+ to 4)	13,618.00	3,438.96	3.03	26,050.14
<b>Low</b>	6.5 (4+ to 9)	47,175.22	5,414.97	3.01	105,943.88
<b>Low Medium I</b>	13.5	2,160.66	160.45	2.36	5,111.90
<b>Low Medium II</b>	23.5	4,550.42	156.70	2.36	8,690.30
<b>Medium</b>	42 (20+ to 55)	25,269.32	464.08	2.35	45,804.99
<b>High Medium</b>	82 (55+ to 109)	4,398.14	40.39	2.36	7,815.69
<b>TOTALS</b>		<b>72,946.00</b>	<b>10,674.91</b>		<b>200,930.95</b>

**GOAL 1**

***A SAFE, SECURE, AND HIGH QUALITY RESIDENTIAL ENVIRONMENT FOR ALL ECONOMIC, AGE, AND ETHNIC SEGMENTS OF THE CANOGA-PARK-WINNETKA-WOODLAND HILLS-WEST HILLS COMMUNITY PLAN AREA.***

**Objective 1-1**

Achieve and maintain a housing supply sufficient to meet the diverse economic needs of current and projected population to the year 2010.

**Policies**

1-1.1 Maintain an adequate supply and distribution of multi-family housing opportunities in the Community Plan Area.

***Program:*** The Plan Map identifies specific areas where multi-family residential development is permitted.

1-1.2 Protect existing single family residential neighborhoods from new, out-of scale development.

***Program:*** Recent changes in the Zoning Code set height limits for new single family residential development.

1-1.3 Protect existing stable single family and low density residential

neighborhoods from being impacted by the size of commercial development.

**Program:** Commercial development in areas designated Neighborhood or General Commercial adjacent to single family residential shall be limited to an FAR of .5 to 1 and buffered by building setbacks and landscaping.

- 1-1.4 Protect the quality of the residential environment through attention to the physical appearance of communities.

**Program:** Design Guidelines and Standards for residential development are included in the Urban Design Chapter of the Community Plan.

- 1-1.5 Protect existing stable single family and low density residential neighborhoods from encroachment by higher density residential and other incompatible uses.

**Program:** The Plan Map identifies lands where only single family residential development is permitted; it protects these areas from encroachment by designating, where appropriate, transitional residential densities which serve as buffers (Transitional Height Ordinance); and reflects plan amendments and corresponding zone changes which are directed at minimizing incompatible uses.

- 1-1.6 Promote neighborhood preservation, particularly in existing single family neighborhoods, as well as in areas with existing multi-family residences.

**Program:** The Community Plan establishes residential land use categories and makes an appropriate designation for each neighborhood in the Community Plan Area. All zone changes, subdivisions, parcel maps, variances, conditional uses, specific plans, community and neighborhood revitalization programs for residential projects shall be consistent with Community Plan land use designations.

**Program:** The Homeowner's Encouragement Loan Program (HELP), administered by the City's Housing Preservation and Production Department, provides rehabilitation loans to owners of small residential buildings (one to four units) to correct code violation.

**Program:** The Residential Rehabilitation Loan Program, administered by the Community Redevelopment Agency (CRA), makes funds available for the rehabilitation of lower-income multi-family rental housing. The program is partially funded by the U.S. Department of Housing and Urban Development (HUD) and requires matching funds from a private lender with CRA as a last resort.

**Program:** The Targeted Neighborhood Initiative leverages resources of Department of Housing and the CRA to provide micro-loans and

grants to support housing rehabilitation and home ownership in specific residential neighborhoods.

**Objective 1-2**

Reduce automobile trips in residential areas by locating new housing in areas offering proximity to goods, services, and facilities.

**Policies**

1-2.1 Locate higher residential densities near commercial centers and major bus routes where public service facilities, utilities and topography will accommodate this development.

**Program:** Maintain and continue the implementation of the City's Land Use/Transportation Policy.

1-2.2 Encourage multiple residential development in commercial zones.

**Program:** The Plan provides the potential for a floor area ratio bonus by providing for mixed use corridors in specific commercial areas.

**Objective 1-3**

Preserve and enhance the character and integrity of existing single and multi-family neighborhoods.

**Policies**

1-3.1 Seek a high degree of compatibility and landscaping for new infill development to protect the character and scale of existing residential neighborhoods.

**Program:** The Plan includes Design Guidelines which establish design standards for residential development to implement this policy.

1-3.2 Approval of proposals to change residential density in any neighborhood shall be based, in part, on consideration of factors such as neighborhood character and identity, compatibility of land uses, impact on livability, adequacy of services and public facilities, and traffic impacts.

**Program:** The decision maker should adopt a finding which addresses these factors as part of any decision relating to changes in planned residential densities.

1-3.3 Preserve existing views in hillside areas.

**Program:** Maintain and continue implementation of the adopted Citywide Hillside Ordinance and the Mulholland Scenic Parkway Specific Plan which contribute to preservation of views.

**Objective 1-4**

Provide a diversity of housing opportunities capable of accommodating all persons regardless of income, age or ethnic background.

**Policies**

- 1-4.1 Promote greater individual choice in type, quality, price and location of housing.

**Program:** The plan promotes greater individual choice through its allocation of lands for a variety of residential densities.

- 1-4.2 Promote mixed use housing projects in pedestrian oriented areas.

**Program:** Maintain and continue the implementation of the Land Use/Transportation Policy.

**Program:** The plan provides a bonus in floor area for mixed use projects in the areas identified in this policy.

- 1-4.3 Ensure new housing opportunities minimize displacement of the residents.

**Program:** The decision-maker shall adopt a finding that addresses any potential displacement of residents as part of any decision relating to new housing construction.

- 1-4.4 Increase home ownership options by providing opportunities for development of townhouses, condominiums, and similar types of housing.

**Program:** The Plan cannot require that condominium units be built instead of rental units; however, the Plan encourages such type of development by designating specific areas for Low Medium residential land use categories.

## **Objective 1-5**

To limit the intensity and density of residential development in hillside areas.

### **Policies**

- 1-5.1 Limit development according to the adequacy of the existing and assured street circulation system within the Plan Area and surrounding areas.

**Program:** Continue the implementation of the Citywide Hillside Ordinance and the Mulholland Scenic Parkway Specific Plan.

- 1-5.2 Ensure the availability of adequate sewers, drainage facilities, fire protection services and facilities and other public utilities to support development within hillside areas.

**Program:** The decision-maker shall adopt a finding which addresses the availability of these services and utilities as part of any decision relating to hillside residential development.

- 1-5.3 Consider the steepness of the topography and suitability of the geology in any proposal for development within the Plan area.

**Program:** The Plan retains hillside areas in restrictive plan designations and zones due to topography. Continue the implementation of the Subdivision Map Act on individual project applications.

1-5.4 Require that any proposed development be designed to enhance and be compatible with adjacent development.

**Program:** Continue the implementation of the Citywide Hillside Ordinance and the Mulholland Scenic Parkway Specific Plan.

## COMMERCIAL

The Canoga Park-Winnetka-Woodland Hills - West Hills Community Plan Area is the commercial hub of the West Valley. Commercial development in this Community Plan Area ranges from Corporate Headquarters in Warner Center, major shopping Malls such as Topanga Plaza, The Promenade, and Fallbrook Mall, pedestrian oriented community centers found along Ventura Boulevard and in Canoga Park, and a proliferation of auto-oriented strip development on major thoroughfares throughout the Community Plan Area.

The appearance, physical layout, and proliferation of strip mall development are major issues in the Community Plan Area. The allocation of land in the CPA designated for commercial use is sufficient to meet the needs of the community. However, poor or obsolete design and inadequate maintenance characterize a significant share of existing commercial development. Such substandard commercial development compromises the vitality of neighborhood and community commercial centers and has a blighting effect on adjacent residential neighborhoods. To address these issues, this plan provides for the use of design guidelines, Community design overlay districts, and policies that encourage recycling of obsolete commercial space over designation of new commercial lands.

The Land Use Diagram Map shows the general boundaries of Centers, Districts, and Mixed Use Boulevards in the Community Plan a (see Appendix A).

## GOAL 2

***AN ECONOMICALLY VITAL COMMERCIAL SECTOR OFFERING A DIVERSITY OF GOODS AND SERVICES TO MEET THE NEEDS OF THE COMMUNITY PLAN AREA. THIS MEANS THAT COMMERCIAL LAND USE POLICIES MUST SUPPORT MAXIMUM EFFICIENCY AND ACCESSIBILITY OF COMMERCIAL DEVELOPMENT WHILE PRESERVING THE HISTORIC COMMERCIAL AND CULTURAL CHARACTER OF THE DISTRICT.***

### Objective 2-1

Conserve and strengthen viable commercial development and encourage recycling of obsolete commercial development.

#### **Policies**

2-1.1 Locate new commercial development in areas currently designated for such development.

**Program:** The plan provides well defined boundaries for commercial areas, any extended growth outside those boundaries requires a plan amendment.

**Program:** Some commercial areas fall within the boundaries of CRA Redevelopment Project Area. Within these areas, obsolete commercial space may be recycled through use of redevelopment powers.

## Objective 2-2

Enhance the appearance of commercial districts

### Policies

2-2.1 Require that any proposed development be designed to enhance and be compatible with adjacent development.

**Program:** Continue the implementation of the Ventura/ Cahuenga Boulevard Corridor Specific Plan, and implement the applicable design standards identified in the Design Guidelines Chapter of the Community Plan.

2-2.2 Require screening of open storage and auto repair uses, and prohibit storage of automobile parts and other noxious commercial related products in front of commercial development, exposed to the street.

**Program:** The Community Plan and Specific Plans include Design guidelines which implement this policy.

2-2.3 Preserve community character, scale and architectural diversity.

**Program:** The Plan establishes height limits, amends Plan designations and recommends corresponding zone changes to implement this policy.

**Program:** Chapter V of this Plan, Urban Design includes design guidelines for individual commercial projects.

2-2.4 Improve safety and aesthetics of parking areas in commercial areas.

**Program:** Implement design standard for parking areas established in the Ventura/Cahuenga Boulevard Corridor Specific Plan and within the Design Standards Chapter of this plan.

2-2.5 Landscaped corridors should be created and enhanced through the planting of street trees along segments with no building setbacks and through median plantings.

**Program:** The Design Guidelines in this Plan and the Ventura/Cahuenga Boulevard Corridor Specific Plan include sections which establishes guidelines for community design and landscaping. These guidelines are intended to serve as reference to other City Departments and public agencies and any private entities who participate in projects which involve improvements to public spaces and right-of-way, including streetscape and landscaping.

## Objective 2-3

Use Pedestrian Oriented Districts and Mixed Use Boulevards to provide alternatives to automobile oriented commercial activity.

### Policies

2-3.1 Preserve existing Pedestrian Oriented Districts.

**Program:** Continue implementation of the Ventura/Cahuenga Boulevard Specific Plan which designates specific areas as POD districts.

2-3.2 New development in Pedestrian Oriented Districts should add to and enhance existing pedestrian street activity.

**Program:** New commercial activity within these districts should be consistent with uses specified within Specific Plan regulations or, for areas outside Specific Plan boundaries, POD implementing ordinances.

2-3.3 Require that the first floor street frontage of structures, including mixed use projects and parking structures located in pedestrian oriented districts, incorporate commercial uses.

**Program:** Continue implementation of the Ventura/Cahuenga Boulevard Corridor Specific Plan Design Guidelines which address this policy for areas within the Specific Plan boundaries. Additionally, where appropriate establish Pedestrian Oriented Districts outside of the Specific Plan boundaries.

## Objective 2-4

Reinforce the identity of distinct commercial districts through the use of design guidelines and development standards.

### Policies

2-4.1 Ensure that commercial infill projects achieve harmony with the best of existing development.

**Program:** Implementation of Design Guidelines and the Ventura/Cahuenga Boulevard Corridor Specific Plan.

2-4.2 Require that mixed use projects and development in pedestrian oriented districts be designed and developed to achieve a high level of quality, distinctive character, and compatibility with existing uses.

**Program:** The Plan includes a Design Guidelines provision which will implement this policy for commercial projects located within pedestrian oriented districts.

2-4.3 Implement development standards that promote commercial development at a scale commensurate with their classification as Neighborhood, General, Community, or Regional Centers and that is compatible with adjacent, primarily residential uses.

**Program:** All commercial zones on properties designated

Neighborhood Commercial and General Commercial shall include the permanent [Q] conditions limiting Floor Area Ratio to 0.5:1. Mixed use projects located on Mixed Use Boulevards as designated by the Citywide General Plan Framework Land Use Diagram, may develop to an FAR of 1.5:1.

**Program:** All commercial zones for properties located on Owensmouth Avenue between Sherman Way and Vanowen Street shall include a permanent [Q] condition limiting development to a Floor Area Ratio of 1:1.

## **INDUSTRIAL**

Industrial uses in the Community Plan Area are found adjacent to the former Southern Pacific Rail Right-Of-Way which runs parallel to Canoga Boulevard, and in Warner Center. Industrial restructuring in recent years has led to a deterioration of older industrial space along Canoga Boulevard, and pressures to reuse modern facilities for retail and other non-industrial uses.

### **GOAL 3**

***MAINTAIN A VITAL INDUSTRIAL BASE IN THE COMMUNITY PLAN AREA BY PROVIDING FOR AN ADEQUATE SUPPLY OF INDUSTRIAL LAND AND THROUGH POLICIES THAT REDUCE CONFLICTS BETWEEN INDUSTRIAL AND RESIDENTIAL LAND USES.***

#### **Objective 3-1**

Provide sufficient land for expansion of low intensity industrial uses.

##### **Policies**

3-1.1 Designate lands for the continuation of existing entertainment industry uses and development of new production, post production, research and development uses which provide employment opportunities.

**Program:** The Plan Map identifies lands, which have industrial designations to accommodate the variety of uses noted above.

3-1.2 Require that any proposed development be designed to enhance and be compatible with adjacent development.

**Program:** Design Guidelines include provisions for industrial projects which are adjacent to or in the vicinity of residential uses.

#### **Objective 3-2**

Maintain viability of Canoga Boulevard Industrial Corridor and increase compatibility with adjacent residential properties through use of traditional commercial district revitalization strategies.

##### **Policies**

3-2.1 Improve the physical appearance of older industrial areas.

**Program:** Chapter V. - Urban Design includes design guidelines for individual industrial projects.

**Program:** The plan map designates boundaries for a Community Design Overlay District for a portion of the Canoga Boulevard

## Industrial Corridor.

### Objective 3-3

To assure mitigation of potential negative impacts generated by manufacturing uses when they are located in close proximity to residential neighborhoods, the Plan proposes design guidelines for new manufacturing uses when so located.

#### Policies

3-3.1 Encourage new industrial uses adjacent to residential neighborhoods to mitigate their impact on the residential neighborhoods to the extent feasible.

**Program:** New development of manufacturing uses located adjacent to residential neighborhoods shall comply with the Industrial/Residential design guidelines found in the Urban Design Chapter (Chapter V) of this plan.

**Program:** Restrict new industrial uses located adjacent to a residential neighborhood to uses first permitted in the CM zone.

## PUBLIC AND INSTITUTIONAL LAND USE

Public facilities such as fire stations, libraries, parks, schools, and police stations shown on the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan are to be developed in substantial conformance with the standards of need, site area, design, and general location identified in the Service Systems Element and the Safety Element of the General Plan. Such development shall be sequenced and timed to provide an efficient and adequate balance between land use and public services.

Development to full residential, commercial, and industrial densities and intensities proposed by the Plan is predicated on substantial compliance with the standards contained in the Public Facilities and Service Element of the General Plan. Such development should be sequenced to provide a workable, efficient and adequate balance between land use and service facilities.

A continuing need exists to modernize public facilities, improve services, and accommodate changes in the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan. However, the amenities and environmental quality of the community must be adequately protected. Cost and equitable distribution are major issues in the provisions of public facilities. It is essential that priorities be established and new and different sources of revenue be found. Furthermore, public and private development must be coordinated to avoid expensive duplication and to assure a balance among needs, services and costs.

This plan seeks to use the location, characteristics, and timing of public facility and utility development as a tool in achieving planned land use patterns. The intent is to achieve economy and efficiency in the provision of services and facilities consistent with standards for environmental quality.

## RECREATIONAL AND PARK FACILITIES

In the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan area public parks and the majority of recreational areas are managed by the City of Los Angeles Recreation and Parks Department. Parks fall into one of three categories: regional, community, or neighborhood.

Ten Neighborhood Parks, six Community Parks, and two Regional Parks serve the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan Area. Additionally, one private golf course is in the Community Plan Area. The plan area, with its diverse topography, limits the placement of park sites south of Ventura Boulevard. Thus those neighborhood parks located south of Ventura Boulevard offer limited recreational facilities for hillside homeowners. The community parks serve a much wider interest range due to the lack of sites in the hillside areas of the plan area. While the existing parks satisfy the needs of the current residents, the community is still deficient in the number of neighborhood parks.

### GOAL 4

***ADEQUATE RECREATION AND PARK FACILITIES WHICH MEET THE NEEDS OF THE RESIDENTS IN THE PLAN AREA.***

#### Objective 4-1

To conserve, maintain and better use existing recreation and park facilities.

#### **Policies**

4-1.1 Preserve the existing recreational facilities and park space.

***Program:*** By maintaining the Open Space zone and land use designation, existing parks and recreation facilities are protected.

### OPEN SPACE

Open space provides the community with important physical benefits and environmental protection. There are two classifications for Open Space, publicly owned and privately owned open space. In addition to parkland, this Community Plan Area possesses important Open Space resources, public and private, owned by entities other than the City of Los Angeles.

Open Space is broadly defined as land which is essentially free of structures and buildings and/or is natural in character and functions in one or more of the following ways:

1. Recreational and educational opportunities.
2. Scenic, cultural and historic values.
3. Public health and safety.
4. Preservation and creation of community peak travel identity.
5. Rights-of-Way for utilities and transportation facilities.
6. Preservation of natural resources or ecologically important areas.

7. Preservation of physical resources including ridge protection.

**GOAL 5**

***A COMMUNITY WITH SUFFICIENT OPEN SPACE IN BALANCE WITH NEW DEVELOPMENT TO SERVE THE RECREATIONAL, ENVIRONMENTAL AND HEALTH NEEDS OF THE COMMUNITY.***

**Objective 5-1**

To preserve existing open space resources and develop new open space resources.

**Policies**

- 5-1.1 Encourage the retention of passive and visual open space which provides a balance to the urban development of the Plan Area.

***Program:*** The Plan Map designates areas for open space, thus protecting them from encroachment from more intense uses.

- 5-1.2 Accommodate active park lands and other open space uses.

***Program:*** The Plan Map designates lands for open space uses including and the slopes adjacent to the 101 freeway.

- 5-1.3 Require development in major opportunity sites to provide public open space.

***Program:*** The Plan includes this as a guiding principle in the section which addresses the future development of major opportunity sites.

**Objective 5-2**

Provide/Insure access to the regional parks of the Santa Monica Mountains.

**Policies**

- 5-2.1 Insure that public access to the open space areas of the Santa Monica Mountains have adequate parking and trail heads.

***Program:*** The City shall continue implementation of the Open Space and Conservation Element of the General Plan.

**SCHOOLS**

In the Canoga Park-Winnetka-Woodland Hills-West Hills, public schools are administered by the Los Angeles Unified School District (LAUSD). The number of LAUSD facilities in the Plan area are twenty-two elementary schools, five middle schools, and three high schools.

**GOAL 6**

***PUBLIC SCHOOLS THAT PROVIDE A QUALITY EDUCATION FOR ALL OF THE CITY'S CHILDREN, INCLUDING THOSE WITH SPECIAL NEEDS, AND ADEQUATE SCHOOL FACILITIES TO SERVE EVERY NEIGHBORHOOD IN THE CITY.***

**Objective 6-1**

Work constructively with LAUSD to promote the siting and construction of adequate school facilities phased with growth.

**Policies**

6.1.1 Explore creative alternatives for providing new school sites in the City, where appropriate.

**Program:** Develop plans to address issues of siting and joint use of facilities including strategies for expansion in transit-rich locations.

**Program:** Use the City's "Annual Growth Report" to monitor locations for growth and potential new school sites.

**Objective 6-2**

Maximize the use of local schools for community use and local open space and parks for school use.

**Policies**

6-2.1 Encourage the siting of community facilities (libraries, parks, schools and auditoriums) together.

**Program:** Formulate/update plans to address issues relating to siting and the joint use of facilities. Identify strategies for the expansion of school facilities including:

1. Siting of schools and other community facilities (libraries, parks, and auditoriums) within a transit station, center, or mixed-use area so they can complement each other and make the most efficient use of the land provided for these services.
2. Locating middle schools and high schools where possible, close to transit stations and key centers and mixed-use districts, so students can use the transit system to get to and from school.
3. Encouraging private redevelopment of existing school sites in the immediate vicinity of transit stations and centers so that the existing site (a low intensity use) would be replaced by a high intensity mixed-use development that would incorporate school facilities.

**LIBRARIES**

The Canoga Park - Winnetka - Woodland Hills - West Hills Plan area is serviced by three public library branches.

**GOAL 7**

***ENSURE ADEQUATE LIBRARY FACILITIES AND SERVICES ARE PROVIDED TO THE AREA'S RESIDENTS.***

**Objective 7-1**

To encourage the City's Library Department to provide adequate library service which responds to the needs of the community.

**Policies**

7-1.1 Encourage flexibility in siting libraries in mixed-use projects, shopping malls, pedestrian-oriented areas, transit stations, office buildings, and similarly accessible facilities.

**Program:** Through the inclusion of this policy in the Plan text, the Plan supports these identified locations as desirable sites for new libraries and recommends that this policy be considered when the Library Department and decision-makers review and approve site for new libraries.

**POLICE PROTECTION**

Police protection services are provided by the Los Angeles Police Department. The police station serving the Plan Area is the West Valley station located within the Reseda-West Van Nuys Community Plan Area.

**GOAL 8**

***A COMMUNITY WITH ADEQUATE POLICE FACILITIES AND SERVICES TO PROTECT THE COMMUNITY'S RESIDENTS FROM CRIMINAL ACTIVITY, REDUCE THE INCIDENCE OF CRIME AND PROVIDE OTHER NECESSARY LAW ENFORCEMENT SERVICES.***

**Objective 8-1**

To provide adequate police facilities and personnel to correspond with population and service demands.

**Policies**

8-1.1 Coordinate with the Police Department as part of the review of significant development projects and General Plan Amendments affecting land use to determine the impact on service demands.

**Program:** A decision maker should include a finding which considers the impact on police service demands of the project or land use plan change. This consultation with the Police Department is currently in effect for plan amendments which must be reviewed by the General Plan Advisory Board which includes representation from the Police Department.

**FIRE PROTECTION**

The Fire protection and Prevention Plan of the City of Los Angeles provides an official guide to City Departments, other government agencies, developers and interested citizens for the construction, maintenance and operation of fire facilities. It is intended to promote fire prevention by maximizing fire safety education and minimizing loss of life through fire prevention programs. Pursuant to their plan it may be necessary to expand or relocate existing facilities as land patterns change.

**GOAL 9**

***PROTECT THE COMMUNITY THROUGH A COMPREHENSIVE FIRE AND LIFE SAFETY PROGRAM.***

**Objective 9-1**

Ensure that fire facilities and protective services are sufficient for the existing and future population and land uses.

**Policies**

9-1.1 Coordinate with the Fire Department as part of the review of significant development projects and General Plan Amendments affecting land use to determine the impact on service demands.

**Program:** Require a decision maker to include a finding as to the impact on fire service demands for all plan amendments within five years of adoption.

This coordination with the Fire Department is currently in effect for projects which are subject to the subdivision process and for plan amendments which must be reviewed by the General Plan Advisory Board which includes representation from the Fire Department.

## TRANSPORTATION

### TRANSPORTATION IMPROVEMENT AND MITIGATION PROGRAM (TIMP)

Transportation Improvement and Mitigation Program [TIMP]

The Transportation Improvement and Mitigation Program (TIMP) is a set of specific transportation recommendations to meet the circulation needs of the community to the year 2010. The TIMP was prepared for the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan through an analysis of land use impacts on the transportation system.

Specific circulation improvements include a series of Public Transit Improvements which include bus service improvements, Metrolink service improvements and the creation of a community transit center. The TIMP contains Transportation System Management (TSM) recommendations including roadway improvements and redesignations, expansion of the Automated Traffic Surveillance and Control (ATSAC) system, peak hour parking restrictions, and creation of neighborhood traffic control plans. The TIMP also recommends a set of Transportation Demand Management (TDM) programs. TDM program recommendations include creating bikeways, forming transportation management associations, a trip reduction ordinance, and continued participation by the City in regional transportation management programs.

Below is a series of programs and implementation steps. Each implementation measure taken from the TIMP is identified in brackets [] as follows: [TIMP]

### PUBLIC TRANSPORTATION

Opportunities exist within Canoga Park-Winnetka-Woodland Hills-West Hills to increase the use of public transportation.

While it is anticipated that the private automobile will remain the primary mode of transportation within the time frame of the Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan, bus service and the community "DASH" or paratransit will be the primary public transportation modes through the year 2010.

The City of Los Angeles Department of Transportation (LADOT) operates "Cityride", a city-wide demand-responsive paratransit program for senior

citizens aged 65 or older and persons with mobility impairments. Cityride registrants may obtain a low-cost book of transit scrip each quarter which can be exchanged for MTA monthly bus passes and discounts on taxi, private lift-van and Cityride lift-van Dial-A-Ride services.

**GOAL 10**

***DEVELOP A PUBLIC TRANSIT SYSTEM THAT IMPROVES MOBILITY WITH CONVENIENT ALTERNATIVES TO AUTOMOBILE TRAVEL.***

**Objective 10-1**

To encourage improved local/shuttle bus service through the Community Plan Area and encourage park-and-ride facilities to interface with freeways, high occupancy vehicle (HOV) facilities, and rail facilities.

**Policies**

10-1.1 Coordinate with the Metropolitan Transportation Authority (MTA) to improve local bus service to and within the Community Plan Area.

**Program:** Transit improvements [TIMP]. Implement Transit Priority Treatment (e.g., signal coordination, transit signal priority, queue jumpers, bottleneck intersection improvements, and signing and striping modifications) along the following streets;

- C Canoga Avenue (Victory Boulevard to Oxnard Street)
- C Oxnard Street (Topanga Canyon Boulevard to Canoga Avenue)
- C Roscoe Boulevard (east of Topanga Canyon Boulevard)
- C Topanga Canyon Boulevard (Ventura Boulevard to Oxnard Street and north of Victory Boulevard)
- C Ventura Boulevard (east of Topanga Canyon Boulevard)
- C Victory/Vanowen corridor (east of Topanga Canyon Boulevard)

**Program:** Transit Improvements [TIMP]. Implement and/or support MTA's; implementation of recommendations contained in the *Study of Restructuring Public Transit Service in the San Fernando Valley*. Specifically:

- C Establish Warner Center Transit Center
- C Develop timed hub and spoke network focused on Warner Center Transit Center.
- C Establish new shuttle routes including a high frequency Canoga corridor shuttle, a West Valley shuttle, and a Calabasas shuttle.

**Program:** Transit Improvements [TIMP] Implement Warner Center Specific Plan Transit Improvements as planned. Particularly Canoga Boulevard north/south commuter shuttle, Long-Distance

Commuter Express Bus System, Warner Center internal DASH Shuttle System, and the Local Circulator Bus System.

**Program:** Transit Improvements [TIMP]. Implement Ventura Boulevard/Cahuenga Boulevard Corridor Specific Plan Transit Improvements.

- 10-1.2 Encourage the provision of safe, attractive and clearly identifiable transit stops with user friendly design amenities.

**Program:** The Plan includes an Urban Design chapter that outlines design guidelines for transit stops. Identification of transit stops and user friendly amenities are also being provided through implementation of the Ventura/Cahuenga Specific Plan. This Specific Plan establishes a Pedestrian Oriented District along Ventura Boulevard in Woodland Hills.

## **TRANSPORTATION DEMAND MANAGEMENT (TDM)**

To the extent feasible and appropriate in light of the Mobility Plan's and the Community Plans' policies promoting multi-modal transportation and safety, it is the City's objective that the traffic level of service (LOS) on the street system in the community not exceed LOS E. Studies indicate that most of Canoga Park - Winnetka - Woodland Hills - West Hills major street intersections currently meet this standard. However, the level of trips generated by future development in the plan area and surrounding communities requires implementation of a Transportation Demand Management (TDM) Program and other improvements to enhance safety and mobility. TDM is a program designed to reduce trips. People are given incentives to use TDM measures such as public transit, ridesharing, modified work schedules, van pools, telecommuting, and non-motorized transportation modes such as the bicycle.

In addition to the specific policies and programs listed below, The Transportation Demand Management (TDM) Program for the Community Plan Area includes the following actions by the City.

1. Transportation Management Association Formation/Coordination.

The City should encourage the formation of Transportation Management Associations (TMA's) in order to assist employers in creating and managing trip reduction programs.

2. Participation in Regional Transportation Management Programs.

The city will continue to participate and coordinate with local and regional TDM programs that are in the process of being implemented, by other agencies, and adjacent jurisdictions.

3. TDM Ordinance. the Citywide Ordinance on TDM and trip reduction measures (L.A.M.C. 12.26 J) will continue to be implemented for the Canoga Park - Winnetka - Woodland Hills - West Hills area. This ordinance calls for several measures to be taken in developments to achieve trip reduction targets.

4. Monitoring. The City of Los Angeles Department of Transportation (LADOT) is responsible for monitoring the current Citywide TDM Ordinance.
5. The City should continue to implement bikeways development as specified in the Mobility Plan (2035) as part of an overall transportation demand management.

**GOAL 11**

***ENCOURAGE ALTERNATIVE MODES OF TRANSPORTATION TO THE USE OF SINGLE OCCUPANT VEHICLES (SOV) IN ORDER TO REDUCE VEHICULAR TRIPS.***

**Objective 11-1**

To pursue transportation demand management strategies, that can maximize vehicle occupancy, minimize average trip length, and reduce the number of vehicle trips.

**Policies**

- 11-1.1 Encourage non-residential development to provide employee incentives for utilizing alternatives to the automobile (i.e., carpools, vanpools, buses, flex time, bicycles, and walking, etc.).

***Program:*** The Citywide Ordinance on TDM and trip reduction measures will continue to be implemented for the Canoga Park - Winnetka - Woodland Hills - West Hills area. This Ordinance calls for several measures to be taken by non-residential developments to achieve necessary trip reduction targets. [TIMP]

- 11-1.2 Require that proposals for major new non-residential development projects include submission of a TDM Plan to the City.

***Program:*** The decision-maker shall include this requirement in approval of projects.

**TRANSPORTATION SYSTEM MANAGEMENT (TSM)**

Transportation System Management (TSM) strategies are intended to improve the existing transportation system through a program of minor, low cost, physical improvements. TSM incorporates features such as synchronization of traffic signals, limiting on-street parking during peak travel times, conversion of parallel arterials into one-way couplets, reversible lane operations, and intersection improvements.

**GOAL 12**

***A WELL MAINTAINED, SAFE, EFFICIENT FREEWAY AND STREET NETWORK.***

**Objective 12-1**

Reduction of vehicular trip delays in the Community Plan Area through coordination of the street traffic signal system with the Caltrans freeway traffic management system.

**Policies**

- 12-1.1 Implement Smart Corridor technologies in the Ventura Freeway corridor, including parallel arterials to the north and south [TIMP].  
**Program:** Implement when funding is available.

**Objective 12-2**

That the Canoga Park - Winnetka - Woodland Hills - West Hills signalized intersections are integrated with the City's ATSAC system by the year 2010.

**Policies**

- 12-2.1 Install ATSAC equipment at an accelerated rate with expanded funding.  
**Program:** Accelerated installation of ATSAC equipment when funding becomes available [TIMP]. ATSAC, a computerized system that directs traffic control operations based on the data collected at each signalized intersection, is recommended to be installed by the year 2010 at the arterial signalized intersections.
- 12-2.2 Accelerate controller replacement to upgrade and improve signal efficiency.  
**Program:** Implement when funding is becomes available.

**GOAL 13**

***TO THE EXTENT FEASIBLE AND CONSISTENT WITH THE MOBILITY PLAN 2035'S AND COMMUNITY PLANS' POLICIES PROMOTING MULTI-MODAL TRANSPORTATION AND SAFETY, A SYSTEM OF FREEWAYS AND STREETS THAT PROVIDES ADEQUATE CIRCULATION TO SUPPORT EXISTING, APPROVED, AND PLANNED LAND USES AND MAINTAINS A DESIRED LEVEL OF SERVICE AT INTERSECTIONS.***

**Objective 13-1**

To the extent feasible and consistent with the Mobility Plan 2035's and the Community Plans' policies promoting multi-modal transportation and safety, comply with Citywide performance standards for acceptable levels of service (LOS) and insure that necessary road access and street improvements are provided to accommodate traffic generated by new development.

**Policies**

- 13-1.1 To the extent feasible and consistent with the Mobility Plan 2035's and the Community Plans' policies promoting multi-modal transportation (e.g., walking, bicycling, driving, and taking public transit) and safety, maintain a satisfactory LOS for streets that should not exceed LOS "D" for Boulevards, Avenues, and Collector Streets. If existing levels of service are LOS "E" or LOS "F" on a portion of an arterial or collector street, then the level of service for future growth should be maintained at LOS "E", where feasible and consistent with the Mobility Plan.

**Program:** To the extent feasible and consistent with the policies of the Mobility Plan 2035, implement the Warner Center Specific Plan Phase I channelization and striping improvements on Burbank Boulevard from Topanga Canyon Boulevard to Ventura Freeway and Oxnard Street form Topanga Canyon Boulevard to Canoga Avenue.

**Program:** Implement the following Warner Center Specific Plan Phase I peak hour parking restrictions or prohibitions, to the extent feasible and consistent with the policies and programs of the Mobility Plan 2035, to provide 6 through lanes of traffic, at least during peak traffic periods [TIMP]:

1. De Soto Avenue - Vanowen Street to Victory Boulevard;
2. Topanga Canyon Boulevard - Victory Boulevard to Burbank Boulevard;
3. Topanga Canyon Boulevard - Sherman Way to Strathern Street;
4. Victory Boulevard - Fallbrook Avenue to Topanga Canyon Boulevard;
5. Winnetka Avenue from south of Victory Boulevard to Roscoe Boulevard.
6. Sherman Way from Fallbrook Avenue to Victory Boulevard; restrict parking, at least during peak periods, to provide 6 through lanes. The need for the proposed peak parking restrictions on Sherman Way will be reevaluated at the time of the intended Plan review in the fifth year following Plan adoption, pending evaluation of the successfulness of the ongoing Canoga Park business district pedestrian and streetscape enhancements. The proposed restrictions will not be implemented prior to such review.

**Program:** In a manner feasible and consistent with the policies of the Mobility Plan, implement intersection improvements (including right-of-way acquisition, intersection flaring, and signal improvements) recommended in the Ventura/Canoga Boulevard Corridor Specific Plan, Phase I and II [TIMP].

**Program:** To the extent feasible and consistent with the policies of the Mobility Plan (2035), implement Warner Center Specific Plan Phase I intersection improvements as growth and traffic warrants [TIMP].

**Program:** Improve to their designated standard specifications, substandard segments of arterials expected to experience heavy traffic congestion by the year 2010, to the extent feasible and consistent with the Mobility Plan. The following streets should be included in the City's Capital Improvement Program.

1. Burbank Avenue from Fallbrook Avenue to Farralone Avenue: Improve to provide 4 through lanes.
2. Victory Boulevard from Topham Street to Corbin Street: improve and implement peak parking restrictions to provide 6 peak through lanes.

**Program:** Implement the following Warner Center Specific Plan Phase I street improvements [TIMP], in a manner feasible and consistent with the policies and programs of the Mobility Plan.:

1. Canoga Avenue from Vanowen Street to Victory Boulevard: upgrade and widen from Avenue to Boulevard standards.
2. Canoga Avenue at Ventura Freeway: widen to provide 6 through traffic lanes under US 101 Bridge.
3. De Soto Avenue at Los Angeles River: widen to provide 6 traffic lanes on the Los Angeles River bridge.
4. De Soto Avenue at Ventura Freeway: widen to provide 3rd northbound through traffic lane under US 101 Bridge.
5. Oxnard Street from Topanga Canyon Boulevard to De Soto Avenue: upgrade and widen from Avenue to Boulevard standards.
6. Ventura Freeway westbound on-ramp from Burbank Boulevard: widen to provide 2 traffic lanes through the ramp meter.

**Program:** Implement the following street widenings, to the extent feasible and consistent with the policies and programs of the Mobility Plan. [TIMP]

1. Fallbrook Avenue from Ventura Boulevard to Avenue San Luis: reconstruct and widen to provide left turn lane, 2 through northbound lanes, and 1 through southbound lane.
2. Mulholland Drive from west of San Feliciano Drive to east of Mulholland Highway: reconstruct and widen to provide 4 through lanes.
3. Mulholland Drive from east of Mulholland Highway to Topanga Canyon Boulevard: reconstruct and widen to provide 4 through lanes.
4. Roscoe Boulevard from Jason Avenue to Valley Circle Boulevard: Improve to provide 4 through lanes.

13-1.2 Discourage non-residential traffic flow for streets designed to serve residential areas only by the use of traffic control measures.

**Program:** The use of Residential Neighborhood Protection Plans to relieve congestion on collector streets that are expected to experience traffic congestion by the year 2010[TIMP].

**Program:** Implement Warner Center Specific Plan Neighborhood Protection Plans for designated neighborhoods [TIMP].

## Objective 13-2

To insure that the location, intensity and timing of infrastructure development maintain the City's streets standards.

**Policies**

13-2.1 No increase in density and intensity shall be effectuated by zone change, variance, conditional use, parcel map, or subdivision unless it is determined that the transportation system can accommodate the increased traffic generated by the project.

**Program:** The decision-maker shall adopt a finding which addresses this factor as part of any decision.

**Program:** Require that new development projects incorporate TSM and/or TDM programs with Citywide Land Use Transportation Policy.

13-2.2 Driveway access points onto, arterial, and collector streets should be limited in number and be located to insure a smooth, and safe flow of vehicles and bicycles.

**Program:** Require that new development projects incorporate such considerations.

**NON-MOTORIZED  
TRANSPORTION**

The plan provides for various modes of non-motorized transportation/circulation such as walking and bicycle riding. The Mobility Plan (2035) identifies a backbone bikeway system and support routes through Canoga Park - Winnetka - Woodland Hills - West Hills. The Community Plan establishes policies and standards to facilitate the development of a bicycle route system which is intended to complement other transportation modes.

The Citywide Major Equestrian and Hiking Trails Plan identifies proposed equestrian trails in the community.

**GOAL 14**

***A SYSTEM OF SAFE, EFFICIENT AND ATTRACTIVE BICYCLE, PEDESTRIAN AND EQUESTRIAN FACILITIES.***

**Objective 14-1**

To promote an adequate system of safe bikeways for commuter, school and recreational use.

**Policies**

14-1.1 Assure that local bicycle facilities are identified and linked with facilities of neighboring areas of the City.

**Program:** The Community Plan endorses full implementation of the Mobility Plan, which designates bikeways for the following ;

1. Class I bike path along the Los Angeles River.

2. Class II bike lanes along Bell Canyon Boulevard, Mulholland Drive (east of Mulholland Highway), <Mobility Plan designates for Class IV bike lane>, Victory Boulevard (Valley Circle Boulevard to Fallbrook Avenue), Victory Boulevard (Valley Circle Boulevard to Fallbrook Avenue), Victory Boulevard (east of De Soto Avenue), and Winnetka Avenue (north of Ventura Boulevard).
3. Commuter (peak period) bike lanes on De Soto Avenue (north of Victory Boulevard), Roscoe Boulevard (east of Topanga Canyon Boulevard) and Victory Boulevard (Fallbrook Avenue to De Soto Avenue).

14-1.2 Encourage the provision of showers, changing rooms and bicycle storage at new and existing non-residential developments and public places.

**Program:** Los Angeles municipal Code Sections 12.21 A 16 and 91.0705 provide for bicycle parking requirements and employee facilities for showers and lockers.

**PARKING**

The Plan supports the City's continuing efforts to develop City owned (off-street) parking facilities in Canoga Park - Winnetka - Woodland Hills - West Hills. City- owned parking lots should be located in or near commercial areas.

**GOAL 15**

***A SUFFICIENT SYSTEM OF WELL DESIGNED AND CONVENIENT ON-STREET PARKING AND OFF-STREET PARKING FACILITIES THROUGHOUT THE PLAN AREA.***

**Objective 15-1**

To provide parking in appropriate locations in accord with Citywide standards and community needs.

**Policies**

15-1.1 Consolidate parking where appropriate, to minimize the number of ingress and egress points onto Arterials.

**Program:** The Plan contains an Urban Design Chapter which outlines guidelines for parking areas.

15-1.2 Consider new Citywide parking standards for areas around transit stations, designated centers and pedestrian oriented areas.

**Program:** Implement peripheral parking lot/ structure program as recommended in the Ventura/Cahuenga Boulevard Corridor Specific Plan.

15-1.3 New parking lots and new parking garages shall be developed in accordance with design standards.

**Program:** The Plan contains an Urban Design Chapter which outlines guidelines for parking facilities.

## HISTORIC AND CULTURAL RESOURCES

Much of the history of the Canoga Park-Winnetka-Woodland Hills-West Hills reflects economic transformation of the area from Native American Settlements, then the development of farming communities, and later rapid suburbanization and development of a strong aerospace sector. The Historical and cultural resources of the Community Plan Area reflect this rapid change. Important landmarks reflecting the Community's history include the Workman House at Shadow Ranch Park, the Ledere Residence, and Leonis Adobe.

Today the Community Plan Area is culturally diverse and contains important cultural resources. These resources include El Centro De Amistad, Valley Cultural Center, Warner Plaza, the New Madrid Performing Arts Center, numerous art galleries, and performing groups.

### GOAL 16

***PRESERVATION AND RESTORATION OF CULTURAL RESOURCES, NEIGHBORHOODS, AND LANDMARKS WHICH HAVE HISTORICAL AND/OR CULTURAL SIGNIFICANCE.***

#### Objective 16-1

To ensure that the community's historically significant resources are protected, preserved, and/or enhanced.

#### Policies

16-1.1 Assist private owners of existing and future historic resources to maintain and/or enhance their properties in a manner that will preserve the integrity of such resources in the best possible condition.

***Program:*** Adherence to the City's historic properties preservation ordinances and Cultural Heritage Board requirements for preservation and design. Implementation of design standards.

***Program:*** Utilize City historic properties restoration programs which provide funding for renovating and/or reusing historic structures.

**CANOGA PARK - WINNETKA - WOODLAND HILLS - WEST HILLS**

**SUMMARY OF LAND USE**

CATEGORY	LAND USE	CORRESPONDING ZONES	NET ACRES	% AREA	TOTAL NET ACRES	TOTAL % AREA
<b>RESIDENTIAL</b>						
<b>Single Family</b>					<b>9,860</b>	<b>55.1</b>
	Minimum	OS, A1, A2, RE40	1,012	5.7		
	Very Low	RE20, RA, RE15, RE11	3,424	19.1		
	Low	RE9, RS, R1, RU, RD6, RD5	5,424	30.3		
<b>Multiple</b>					<b>826</b>	<b>4.6</b>
	Low Medium I	R2, RD3, RD4, RZ3, RZ4, RU, RW1	162	0.9		
	Low Medium II	RD1.5, RD2, RW2, RZ2.5	156	0.9		
	Medium	R3	469	2.6		
	High Medium	R4	39	0.2		
<b>COMMERCIAL</b>						
	Neighborhood	C1, C1.5, C2, C4	167	0.9		
	Limited	CR, C1, C1.5, P	52	0.3		
	General	CR, C1.5, C2, C4	186	1.0		
	Community	CR, C2, C4	347	2.0		
	Regional	CR, C1.5, C2, C4, R3, R4, R5	220	1.2		
<b>INDUSTRIAL</b>						
	Limited	CM, MR1, M1	292	1.6		
	Light	MR2, M2	385	2.2		
<b>PARKING</b>						
	Parking	P, PB	0	0.0		
<b>OPEN SPACE/PUBLIC FACILITIES</b>						
	Open Space	OS, A1	1,404	7.8		
	Public Facilities	PF	713	4.0		
<b>STREETS</b>						
	Private Streets	-	21	0.1	<b>3,442</b>	<b>19.3</b>
	Public Streets	-	3,421	19.2		
<b>TOTAL</b>					<b>17,894</b>	<b>100.0</b>

## Chapter IV

# COORDINATION OPPORTUNITIES FOR PUBLIC AGENCIES

Chapter 4 identifies actions which are recommended to be promoted by the City through the appropriate city departments and through other agencies including Federal, State, and private sector entities to further the goals of the Plan. These are objectives or goals that the Planning Department does not have control over, but which involve issues that should be identified in the community plan and which help to reinforce the intent of the goals and objectives found in Chapter 3.

### RECREATION AND PARK FACILITIES AND OPEN SPACE

1. The City Department of Recreation and Parks should work with the Los Angeles Unified School District to develop a program for shared use of school sites for both educational and recreation and park opportunities.
2. Encourage continuing efforts by County, State, and Federal agencies to acquire vacant land for publicly-owned open space.
3. Ensure that parks are adequately illuminated and secured for safe use at night, as appropriate.
4. Coordinate with the Department of Recreation and Parks and the Police Department to insure adequate police patrols and "defensible space", where feasible, in the design of recreation and park facilities.
5. Promote the supervision of park activities and enforcement of codes restricting illegal activity.
6. Improve the utilization and development of recreational facilities at existing parks, as needed, and as funds become available.
7. Coordinate with City Departments, neighboring cities, and County, State, and Federal agencies to utilize existing public lands such as flood control channels, utility easements, and Department of Water and Power properties for such recreational uses as hiking, biking and horseback riding, where possible.
8. Plan and design the expansion of existing facilities and the acquisition of new sites to minimize the displacement of housing and the relocation of the residents.
9. Target the provisions of park and recreation facilities in areas with the greatest deficiencies.
10. Pursue resources to clean up land that could be used for public recreation safely.

**SCHOOLS** Consider large vacant parcels as a first alternative to accommodate the demand for new schools, prior to the displacement of existing uses.

- LIBRARIES**
1. Seek additional resources to maintain and expand library services to satisfy service demands to the Year 2010.
  2. Develop a Citywide policy for locating non-English language permanent collections.
  3. Support the efforts of the Library Department and the Canoga Park - Winnetka - Woodland Hills - West Hills community to increase the service levels of the libraries so they are appropriate for the Canoga Park - Winnetka - Woodland Hills - West Hills population.

**POLICE PROTECTION** Support and encourage community-based crime prevention efforts such as Neighborhood Watch, through regular intersection and coordination with existing community-based policing, foot and bicycle patrols, watch programs, assistance in the formation of new neighborhoods watch groups, and regular communication with neighborhoods and civic organizations.

**FIRE PROTECTION** Provide that adequate fire service personnel are maintained by periodically evaluating population growth, level-of-service (response TAMP and staffing) and fire hazards in the City.

**HISTORIC PRESERVATION** Assist private owners of historic resources to maintain and/or enhance their properties in a manner that will conserve the integrity of such resources in the best possible condition.

- HOUSING**
1. Locate senior citizen housing projects in neighborhoods within reasonable walking distance of health and community facilities, services and public transportation.
  2. Maintain and preserve the character and integrity of existing neighborhoods and encourage participation in self-help preventive maintenance to promote neighborhood conservation, beautification and rehabilitation.
  3. Improve the coordination of public services to support neighborhood conservation activities.
  4. Ensure that the location of low and moderate income housing is equitably distributed throughout the Plan area predicated on a fair share basis in relationship to all other planning areas.
  5. Encourage new and alternative housing concepts, as well as alternative materials and methods of construction, which are found to be compatible with City codes.

6. Allow for the assembly and trade of public land in order to encourage new housing in appropriate locations within the Plan area.
7. Ensure that the development of transitional housing and emergency shelters is appropriately located.
8. Encourage the development of housing types intended to meet the special needs of senior citizens and the physically challenged.

**UTILITIES**

Install utilities underground through assessment districts or other funding, when feasible.

**EMPLOYMENT**

1. Encourage businesses to participate in job training programs for local residents.
2. Develop employment opportunities for a wide range of jobs, skills, and wages.

**PUBLIC TRANSPORTATION**

1. Coordinate with the Metropolitan Transit Authority to improve local bus service to and within the Canoga Park - Winnetka - Woodland Hills - West Hills Community Plan Area.
2. Encourage the expansion of programs wherever feasible, aimed at enhancing the mobility of senior citizens, disabled persons, and the transit-dependent population.
3. Develop an intermodal mass transportation plan to link future rail service.

**NON-MOTORIZED TRANSPORTATION**

Encourage funding and construction of bicycle facilities connecting residential neighborhoods to schools, open space areas, employment centers and transit stations.

**NATURAL DISASTERS**

Natural disasters such as the 1971 Sylmar-San Fernando and the 1994 Northridge earthquakes, floods, and fires have and will continue to impact the Canoga Park - Winnetka - Woodland Hills - West Hills community. City government, other governmental agencies, the private sector, disaster relief agencies, and the citizens of Canoga Park - Winnetka - Woodland Hills - West Hills should be encouraged to work together to minimize the impacts of a disaster in terms of land development practices, providing essential services, preventing transportations and communication blockages and to ensure that recovery will proceed as expeditiously as possible.

**EARTHQUAKE PREPAREDNESS**

The 1994 Northridge earthquake devastated portions of the Canoga Park - Winnetka - Woodland Hills - West Hills area. The magnitude 6.8 (Richter Scale) earthquake caused extensive and widespread property damage to residences, businesses, nonprofit organizations, public facilities, and infrastructure including freeways, water lines, power lines, and natural gas

lines. Recovery and rebuilding efforts have already begun following the North ridge earthquake and will continue over the next several years.

## **Chapter V URBAN DESIGN**

The Canoga Park-Winnetka-Woodland Hills-West Hills Community Plan Area is made up of a number of neighborhoods with distinctive characteristics. It is the purpose of this Chapter to lay out broad, general policies for individual multiple residential and commercial projects, and community design elements. This Chapter is divided into two sections. The Design Policies section is directed at individual projects. The Community Design and Landscaping Guidelines section is directed at a community's use of streetscape improvements and landscaping in public spaces and rights-of-way.

The Design Policies in this Chapter establish the minimum level of design that shall be observed in multiple residential and commercial projects within the entire Plan Area. They also address design issues for parking and landscaping.

The Administration of the general policies found in this Chapter shall be by establishment of a Community Design Overlay District (CDO), for specific portions of the Canoga Park-Winnetka-Woodland Hills-West Hills Community, per the Supplemental Use District Section of the Zoning Code (Section 13.00)

### **GOALS AND PURPOSES**

These design policies and standards are to ensure that residential, commercial and industrial projects and public spaces and rights-of-way incorporate specific elements of good design. The intent is to promote a stable and pleasant environment.

The Design Policies for commercial corridors emphasize the visual continuity of streetscapes and creation of an environment that encourages pedestrian and economic activity. The Design Policies for multiple-family residential areas emphasize architectural design to enhance quality-of-life, living conditions and neighborhood pride of the residents.

### **DESIGN POLICIES FOR INDIVIDUAL PROJECTS**

#### **COMMERCIAL**

##### **Site Planning**

Structures shall be oriented toward the main commercial street where a parcel is located and shall avoid pedestrian/ vehicular conflicts by:

1. Locating surface parking to the rear of structures.

2. Minimizing the number of driveways providing sole access to the rear of commercial lots.
3. Maximizing retail and commercial service uses along frontages of commercial developments.
4. Providing front pedestrian entrances for businesses fronting on main commercial streets.
5. Providing landscaping strips between driveways and walkways accessing the rear of properties.
6. Providing speed bumps for driveways paralleling walkways for more than 50 feet.
7. Requiring site plans which include ancillary structures, service areas, pedestrian walkways, vehicular path, loading areas, drop off and landscaped areas.
8. Providing where feasible, the under grounding of new utility service.

#### **Height and Building Design**

The mass, proportion and scale of all new buildings and remodels shall be at a pedestrian scale. The design of all proposed projects shall be articulated to provide variation and visual interest, and enhance the streetscape by providing continuity and avoiding opportunities for graffiti.

Building materials shall be employed to provide relief to bland untreated portions of exterior building facades. The purpose of these provisions is to ensure that a project avoids large sterile expanses of building walls, is designed in harmony with the surrounding neighborhood and creates a stable environment with a pleasant and desirable character.

1. No structure shall exceed two stories in height within 15 feet and 30 feet of front and rear property lines, respectively.
2. Maximize the area devoted to transparent building elements, for front facades and facades facing rear parking.
3. Require use of articulation, recesses, surface perforations, porticoes to break up long, flat building facades.
4. Use accenting, complementary building materials for building facades.
5. Maximize application of architectural features or articulation of building facades.
6. Designate architecturally untreated facades for signage.
7. Screen of mechanical and electrical equipment from public view.
8. Screen all rooftop equipment and building appurtenances from public

view.

9. Require the enclosure of trash areas for all projects.

### **Parking Structures**

Parking structures shall be integrated with the design of the buildings they serve through:

1. Design of parking structure exteriors to match the style, materials and color of the main building.
2. Use landscaping to screen parking structures not architecturally integrated with the main building.
3. Buffer adjacent residential uses from parking structures with landscaping and decorative walls.

### **Surface Parking Landscaping**

1. Devote 2% of total surface area of surface parking lots to landscaping.
2. Provide landscaped buffers along public streets or adjoining residential uses.

### **Light and Glare**

1. Install on-site lighting along all pedestrian walkways and vehicular access ways.
2. Shield and direct on-site lighting onto driveways and walkways. Direct on-site lighting away from adjacent residential uses.

### **Multiple Residential Site Planning**

All multiple residential projects of five or more units shall be designed around a landscaped focal point or courtyard to serve as an amenity for residents.

1. Provide a pedestrian entrance at the front of each project.
2. Require useable open space for outdoor activities, especially for children.

### **Design**

The design of all buildings shall be of a quality and character that improves community appearance by avoiding excessive variety and monotonous repetition. This policy shall be accomplished through:

1. Requiring the use of articulations recesses surface perforations and porticoes to break up long, flat building facades.
2. Using complementary building facades.

3. Incorporating varying designs to provide definitions for each floor.
4. Integrating building fixtures, awnings, security gates, etc. into design of the building
5. Screening all rooftop equipment and building appurtenances from adjacent properties.
6. Require decorative, masonry walls to enclose trash.

**Parking Structures**

Parking structures shall be integrated with the design of the building they serve through:

1. Designing parking structure exterior to match the style, materials and color of the main building.
2. Using decorative walls and landscaping to buffer residential uses from parking structures.

**INDUSTRIAL/  
RESIDENTIAL  
INTERFACE AREAS**

**Design Guidelines**

In order to mitigate potential negative impacts generated by manufacturing uses when they are located adjacent to residentially zoned or developed neighborhoods, new development of manufacturing uses shall incorporate the following design guidelines:

**Loading Areas**

New development of industrial uses located across a local or collector street from a residentially zoned or developed lot shall design their loading area in such a manner that the loading area is visually shielded from the line of sight of adjacent residential uses by a 3 ½ to 8 foot solid decorative masonry wall, depending on whether the wall is located in a front, side, or rear yard.

**Walls/Landscaping**

1. Where vehicle parking, loading, or open storage for new industrial development is located within 50 feet of a public street which separates the industrial and residential zones or uses, a minimum 3 ½ foot high solid decorative masonry wall shall be provided in a front yard, or a minimum 5 foot 9 inch to 8 foot solid decorative masonry wall in a side or rear yard. That a minimum of a 5 foot landscaped setback buffer with an installed automatic sprinkler system shall be located in front of said wall, along the street frontage.
2. New industrial development located directly across a local or collector street from a residentially zoned or developed neighborhood shall provide a minimum 5 foot landscaped setback along any portion of the frontage, not required for driveways, facing the residential use. Said landscaping shall contain a minimum of one 24 inch box tree (with a minimum trunk diameter of two inches, a height of eight feet at the time of planting, and

with an installed automatic sprinkler system) for every 30 feet of street frontage.

3. On any other interior property line which separates an industrial use from an abutting residential zone or use, a minimum 5 foot 9 inch to 8 foot solid decorative masonry wall shall be provided.

### **Architectural Guidelines**

1. New industrial development located directly across a local or collector street, or with a lot line adjoining a residentially zoned or developed area, shall have all exterior lighting shielded and directed onto the site and no floodlighting shall be located so as to be seen directly by adjacent residential areas. This condition shall not preclude the installation of low-level security lighting.
2. New industrial development on local or collector streets fronting onto residentially zoned or developed areas shall be designed with articulated facades (for example, facades that have architectural details, wall breaks, or other architectural features which provide at least 5 feet of relief to a minimum depth of 8 inches every 20 feet of the length of the building wall) facing the residential development.
3. New industrial development adjacent to residentially zoned or developed areas shall be designed with no window openings facing residential properties if the structure is within 10 feet of the side or rear property lines.
4. On new industrial development adjacent to residentially zoned or developed areas, all exhaust fans and exterior or rooftop mechanical equipment shall be screened with such screening material incorporated in the design of the project. Such equipment shall be set back as far as possible from residential property lines and sound proofed.

## **COMMUNITY DESIGN AND LANDSCAPING GUIDELINES**

In addition to identifying Design Policies for individual projects, a community's identity can be enhanced through improvements to the streetscape and landscaping in public spaces and rights-of-way. It is the intent of this section to establish a set of guidelines that will serve to improve the environment both aesthetically and physically. These guidelines should be implemented as public improvements occur in the CPA that affect public spaces and rights-of-way. These guidelines should be referred to and implemented to the extent feasible through such projects and should be a guide to other City departments as they develop, update and implement their respective plans.

The presence or absence of street trees is an important ingredient in the aesthetic quality of an area. Consistent use of appropriate street trees provides shade during hot summer months, emphasizes sidewalk activity by separating vehicle and pedestrian traffic, and creates an area wide identity which distinguishes the communities within Canoga Park - Winnetka - Woodland Hills - West Hills from each other.

The following improvements are recommended:

**ENTRYWAY  
IMPROVEMENTS**

Provide improvements along principal streets and at major identified intersections and edges which clearly distinguish these locations as major streetscapes and entries. Such improvements may include elements such as signage, landscaping, vertical pylons and/or other distinctive treatments.

**STREETSCAPE**

1. Provide for a coordinated streetscape design at identified entries to the Plan Area, the Community Center and the Regional Center that includes street lighting, street furniture, and sidewalk/crosswalk improvements in the public right-of-way.
2. Establish a comprehensive streetscape and landscape improvement program for identified corridors and districts for the selection and installation of, but not limited to, the following:
  - a. street trees
  - b. street lighting
  - c. streetscape elements (sidewalk/crosswalk paving, street furniture)
  - d. public signage

**STREET TREES**

1. Select species which:
  - a. Enhance the pedestrian character, and convey a distinctive high quality visual image.
  - b. Are drought and smog tolerant, and fire-resistant, and complement existing street trees.
2. Establish a hierarchy for street trees which shall include:

- a. Major Accent Trees.

These trees should be located at entry locations, intersections, and activity centers.

- b. Street Trees.

Select specific species to be the common tree for street frontages. A single flowering species may be selected for all residential neighborhoods and commercial districts or different species selected to distinguish one neighborhood, district, or street from another. In residential neighborhoods, the trees should be full, to provide shade and color. In commercial districts, the trees should provide shade, but be more transparent to promote views of store fronts and signs.

- c. Ornamental or Special Plantings.

At special areas along street frontages, such as linkages to pedestrian walkways and plazas and outdoor dining areas, ornamental trees providing shade and color should be utilized to emphasize and focus attention on those places.

Provide for the installation of street trees along public sidewalks defining the types and spacing in accordance with a Street Tree Master Plan.

## **STREET FURNITURE**

Install street furniture that encourages pedestrian activity or physical and visual access to buildings and which is aesthetically pleasing, functional and comfortable. Street furniture may include such elements as bus and pedestrian benches, bus shelters, kiosks, trash receptacles, newspaper racks, bicycle racks, public telephones, landscaped planters, drinking fountains, and bollards. Priority should be given to pedestrian-oriented areas.

## **STREET LIGHTING**

Establish street lighting standards for commercial districts which provide elements of design and compatibility with street furniture and building facades.

1. Install new street lights in commercial districts which are attractively designed, and compatible with facades and other street furniture, to provide adequate visibility, security, and a festive night time environment.
2. Establish a consistent street lighting type utilizing a light standard that is compatible with the overall street furniture and graphic/signage program,
3. Any new street lighting or pedestrian lighting system built in the public right-of-way must be designed to currently adopted City standards. Equipment must be tested and approved by the Bureau of Street Lighting.
4. New lighting systems will be designed to minimize glare and "light trespass".
5. No new or replacement lighting systems require due process. Street lighting is installed through the formation of special assessment districts. Where any increase in special assessment is anticipated, public hearings are required.
6. Ornamental or historic poles can not be removed without the prior approval of the City's Cultural Affairs Commission.

## **SIDEWALKS/PAVING**

Develop sidewalk "pull-outs" at intersections, where they do not adversely impact traffic flow or safety or disrupt bus service, by extending the sidewalk to the depth of a parking stall to accommodate landscaping and street furniture and reduce the crosswalk width.

## **SIGNAGE**

Establish a consistent design for all public signage, including fixture type, lettering, colors, symbols, and logos designed for specific areas or pathways.

1. Provide for distinctive signage which identifies principal entries to unique neighborhoods, historic structures, and public buildings and parks.
2. Ensure that public signage complements and does not detract from adjacent commercial and residential uses.
3. Provide for signage which uniquely identifies principal commercial areas.

## **PUBLIC OPEN SPACE AND PLAZAS**

Establish public open space standards that will guide the design of new public plazas and open spaces. These standards should include the following:

1. Consideration of the siting of open space to maximize pedestrian accessibility and circulation.
2. Solar exposure or protection.
3. Adjacent to pedestrian routes and other open spaces.
4. Appropriate plant and hardscape materials.

**RICHARD RIORDAN**, Mayor  
James Kenneth Hahn, City Attorney  
Rick Tuttle, Controller

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## **DEPARTMENT OF CITY PLANNING**

Con Howe, Director of Planning  
Franklin Eberhard, Deputy Director  
Gordon B. Hamilton, Deputy Director  
Robert H. Sutton, Deputy Director

## **COMMUNITY PLAN UPDATE**

### **COMMUNITY PLANNING**

Jack Sedwick, Principal City Planner  
Frank Fielding, Senior City Planner  
Deuk Perrin, City Planner  
Jim Holmes, City Planning Associate

### **GEOGRAPHIC INFORMATION SYSTEMS**

Paul Burns, GIS Supervisor I  
Carmen Mirafior, GIS Supervisor I  
Daniel Garcia, GIS Specialist  
Cecelia Hernandez, GIS Specialist

### **PUBLICATION**

Gary Booher, City Planner  
Jae H. Kim, City Planning Associate  
Hilda Garcia, Principal Clerk  
Edna Roxas-Zafra, Clerk Typist

DEPARTMENT OF  
CITY PLANNING  
200 N. SPRING STREET, ROOM 525  
LOS ANGELES, CA 90012-4801  
CITY PLANNING COMMISSION

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PRESIDENT  
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VICE-PRESIDENT  
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BRADLEY MINDLIN  
THOMAS E. SCHIFF

GABRIELE WILLIAMS  
COMMISSION EXECUTIVE ASSISTANT  
(213) 978-1300

CITY OF LOS ANGELES  
CALIFORNIA



JAMES K. HAHN  
MAYOR

EXECUTIVE OFFICES

CON HOWE  
DIRECTOR  
(213) 978-1271  
FRANKLIN P. EBERHARD  
DEPUTY DIRECTOR  
(213) 978-1273  
GORDON B. HAMILTON  
DEPUTY DIRECTOR  
(213) 978-1272  
ROBERT H. SUTTON  
DEPUTY DIRECTOR  
(213) 978-1274  
FAX: (213) 978-1275  
INFORMATION  
(213) 978-1270  
[www.lacity.org/PLN](http://www.lacity.org/PLN)

April 21, 2005

All Interested Parties:

**RAS INTERPRETATION TO COMMUNITY PLAN FOOTNOTES  
DIRECTOR'S INTERPRETATION**

Attached is a copy of the Department of City Planning's interpretation of Ordinance 174,999, effective January 15, 2003, which established the RAS Zones. This published interpretation becomes final and effective 20-days from the date of this communication unless an appeal to the City Planning Commission is filed within this time period. Appeals shall be filed in duplicate on forms provided at any of the following public offices of the Department of City Planning, along with the required filing fee:

Planning Department – Public Counter  
201 North Figueroa Street, 3<sup>rd</sup> Floor  
Los Angeles, CA 90012  
Phone: (213) 482-7077

San Fernando Valley Office  
6262 Van Nuys Boulevard  
Van Nuys, CA 91401  
Phone: (818) 374-5050

If you have any questions regarding this case, please contact Jane Blumenfeld at (213) 978-1372 of myself at (213) 978-1274.

Sincerely,

CON HOWE  
Director of Planning

ROBERT H. SUTTON  
Deputy Director

CH/RHS:hkt

Attachment

cc: Council Planning Deputies  
Ray Chan, Building and Safety Department  
David Kabashima, Department of City Planning  
Jane Blumenfeld, Department of City Planning



April 21, 2005

**RAS RELATIONSHIP TO COMMUNITY PLAN FOOTNOTES  
DIRECTOR'S INTERPRETATION**

All Interested Parties:

SUBJECT:

Inquiries have been made regarding potential conflicts between Footnotes on the Community Plans and the RAS 3 and RAS 4 (hereafter referred to as RAS) Zones.

BACKGROUND:

The Residential/Accessory Services Zones (RAS) allow a greater floor area than commercial zones and greater height than otherwise allowed in height district 1VL.

“An example is:

Where a traditional C2-1VL with a Commercial plan designation is limited to a 1.5:1 FAR and a 45 height limit, the RAS 3-1VL and RAS 4-1VL shall not exceed a 3:1 FAR and 50 feet in height in accordance with the LAMC 12.10.5, 12.11.5 and 12.21.1.”

The Community Plans as recommend by the City Planning Commission and adopted by City Council are a general guide to development for the community and city as a whole. Rarely do the Community Plans specify special planning rights or restrictions for particular parcels.

Some community plan maps contain footnotes regarding height and floor area. Footnotes appear on the map legend next to the commercial land use categories or in some cases on specific properties or areas. The footnotes that are attached to the commercial land use categories generally relate in a broad-brushed manner to all areas of the plan designated for that particular use. Typically such footnotes are not site specific, and as such, do not relate to specific locations, blocks, or parcels within the community plan area.

“An example of such a footnote which appears in most Community Plans reads:

Footnote 1: ‘Height District 1VL’

This means all properties within the commercial land use category that have this footnote are limited to an FAR of 1.5:1 with a 45-foot height limit.”

DISCUSSION:

When the City Council adopted the RAS Zones in 2002, their purpose was to promote mixed use development in the city's commercial zones, particularly in the commercial corridors which provide the greatest access to transit. In their adoption of the RAS Zones, the City Council recognized that

the additional floor area and height allowed by the RAS zones are necessary to make such primarily residential projects viable. However to protect the integrity of the Community Plans, the Council limited the residential density permitted in the RAS 3 and RAS 4 Zones to correspond to the residential densities permitted in the R3 and R4 Zones, respectively. Thus, they permitted RAS 3 and RAS 4 Zones in Plans that permit R4 and higher zoning but only permitted the RAS 3 Zone (and not RAS 4) in Plans that previously had R3 as the highest zoning category.

In one particular plan, the Plan Footnote on a Neighborhood Commercial area states:

“Floor Area Ratio 1:1.”

In this specific situation it cannot be the intent of Council to allow a 3:1 FAR since they knowingly restricted the property to a 1:1 FAR.

#### INTERPRETATION:

It is hereby interpreted that the RAS Zones can exceed a Community Plan Footnote when that footnote is general in nature and generally refers to all parcels under that plan category. Where there is a specific footnote that refers to (a) specific parcel(s) that is more restrictive, the RAS Zone would not be permitted without a corresponding Plan Amendment.

# Channel Law Group, LLP

February 2, 2026

Supplemental Justification for Appeal of Case No. ZA-2024-5222-CU2-ZBA,  
Conditional Use Permit and Zone Boundary Adjustment, and ENV Case No.  
2024-5223-CE, 23717 Victory Boulevard (Proposed Starbucks Drive-Through)

## **EXHIBIT 4**



## TECHNICAL MEMORANDUM

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**To:** Jamie T. Hall  
Channel Law Group, LLP

**Date:** February 2, 2026

**From:** Ray Kapahi *RK*  
Tel: 916.806.8333  
E-Mail: [ray.kapahi@gmail.com](mailto:ray.kapahi@gmail.com)

**Subject:** Summary of Air Quality Impacts Associated with Proposed Starbucks Drive-Thru  
23717 Victory Blvd., West Valley (Los Angeles County)

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### INTRODUCTION AND SUMMARY

Environmental Permitting Specialists (EPS) has reviewed the various documents associated with the above noted project. Specifically, EPS has reviewed the following documents and offers the following comments:

- Letter of Determination City of Los Angeles (October 21, 2025)  
Comment  
There is no discussion of air quality impacts or impacts to public health at homes located just 75 feet West of the drive-up window. Car exhaust from multiple vehicles idling 12 hours/day, 365 days annually would directly impact these homes.
- Site Plans (undated)
- Application Filed 8/15/2024
- Kaidance Group Findings Response (undated)  
Comment  
Incorrectly asserts project would not adversely affect public health, welfare or safety at adjacent properties. No analysis is provided to support this conclusion.

## PROJECT IMPACTS TO AIR QUALITY WILL BE SIGNIFICANT

EPS has completed an air modeling and health risk analysis. The results are summarized below.

Vehicle exhaust from idling vehicles would lead to substantial amount of NO<sub>x</sub>, PM<sub>10</sub> and CO emissions. EPS analysis determined that 1-hour NO<sub>2</sub> impacts would exceed the federal ambient air quality standards by a substantial margin. It is estimated that NO<sub>x</sub> concentrations at the adjacent homes would exceed 450 micrograms per cubic meter. This is more than twice the federal air quality standards designed to protect public health. The adjacent homes that would be impacted are located only 75 feet West of the proposed project. The federal air quality standard is 188 ug/m<sup>3</sup>. See table of air quality standards shown below

Table 1

Ambient Air Quality Standards						
Pollutant	Averaging Time	California Standards <sup>1</sup>		National Standards <sup>2</sup>		
		Concentration <sup>3</sup>	Method <sup>4</sup>	Primary <sup>3,5</sup>	Secondary <sup>3,6</sup>	Method <sup>7</sup>
Ozone (O <sub>3</sub> ) <sup>8</sup>	1 Hour	0.09 ppm (180 µg/m <sup>3</sup> )	Ultraviolet Photometry	—	Same as Primary Standard	Ultraviolet Photometry
	8 Hour	0.070 ppm (137 µg/m <sup>3</sup> )		0.070 ppm (137 µg/m <sup>3</sup> )		
Respirable Particulate Matter (PM <sub>10</sub> ) <sup>9</sup>	24 Hour	50 µg/m <sup>3</sup>	Gravimetric or Beta Attenuation	150 µg/m <sup>3</sup>	Same as Primary Standard	Inertial Separation and Gravimetric Analysis
	Annual Arithmetic Mean	20 µg/m <sup>3</sup>		—		
Fine Particulate Matter (PM <sub>2.5</sub> ) <sup>9</sup>	24 Hour	—	—	35 µg/m <sup>3</sup>	Same as Primary Standard	Inertial Separation and Gravimetric Analysis
	Annual Arithmetic Mean	12 µg/m <sup>3</sup>	Gravimetric or Beta Attenuation	9.0 µg/m <sup>3</sup>	15.0 µg/m <sup>3</sup>	
Carbon Monoxide (CO)	1 Hour	20 ppm (23 mg/m <sup>3</sup> )	Non-Dispersive Infrared Photometry (NDIR)	35 ppm (40 mg/m <sup>3</sup> )	—	Non-Dispersive Infrared Photometry (NDIR)
	8 Hour	9.0 ppm (10 mg/m <sup>3</sup> )		9 ppm (10 mg/m <sup>3</sup> )	—	
	8 Hour (Lake Tahoe)	6 ppm (7 mg/m <sup>3</sup> )		—	—	
Nitrogen Dioxide (NO <sub>2</sub> ) <sup>10</sup>	1 Hour	0.18 ppm (339 µg/m <sup>3</sup> )	Gas Phase Chemiluminescence	100 ppb (188 µg/m <sup>3</sup> )	—	Gas Phase Chemiluminescence
	Annual Arithmetic Mean	0.030 ppm (57 µg/m <sup>3</sup> )		53 ppb (100 µg/m <sup>3</sup> )	Same as Primary Standard	

Both project level and cumulative (project + background) would be significant. This is especially troubling as Los Angeles County has been classified as “Extreme” non-attainment for the 8-hour ozone standard. NO<sub>x</sub> is one main precursor to regional ozone formation. High concentrations of ozone lead to increased levels of asthma and other respiratory diseases. The very young and the elderly are most at risk from these air pollutants.

## MODELING METHODOLOGY (NO<sub>x</sub> EMISSIONS)

NO<sub>x</sub> emissions for the construction phase were modeled using Environmental Protection Agency (EPA) and South Coast Air Quality Management District (SCAQMD) recommended AERMOD dispersion model (Version 24142). The air dispersion model is used to translate an emission rate (e.g., lb/hr) into a concentration (e.g., parts per million or micrograms per cubic meter [ug/m<sup>3</sup>]).

Other modeling inputs are described below on the next page.

### Model Set-Up

The following options were used:

- Use of Calm Wind Processing
- Use of Missing Data Processing
- Averaging Times: 1 hour
- Use of Terrain Adjustment

### Modeling Grid and Coordinate System

A rectangular (x-y) Cartesian coordinate system was used. A region 1,225 meters x 1,225 meters (0.76 mile x 0.76 mile) was used. The modeling region divided into 25 meter square cells for a total of 2,500 individual receptors in the vicinity of the project area.

### Meteorological Data

Five years of hourly meteorological data (2011 to 2015) were used in the analysis. The surface data (wind speed, wind direction, temperature, etc.) were recorded at Van Nuys airport. SCAQMD provides model ready meteorological files at their web site<sup>1</sup>.

### Operational (Long-Term) Emissions

Maximum daily emissions of NO<sub>x</sub> were estimated assuming 13 vehicles are idling, 18.5 hours per day, 365 days per year.

## FINDINGS

### Impacts to Air Quality and Significant

The results of this analysis indicate that NO<sub>x</sub> emissions during the construction phase would result in ambient concentrations that would violate California's 1-hour ambient air quality standard for NO<sub>x</sub> emissions by a wide margin.

The maximum 1-hour concentration and the adjacent homes is estimated to equal 350 ug/m<sup>3</sup>. When combined with the background concentration (estimated to equal 90 ug/m<sup>3</sup>) would result in cumulative NO<sub>x</sub> impacts in excess of 440 ug/m<sup>3</sup>.

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<sup>1</sup> Available at: <http://www.aqmd.gov/home/air-quality/meteorological-data/data-for-aermod>

Exposure to nitrogen dioxide harms the lungs causing irritation, coughing and worsening of asthma. Children, the elderly and those with respiratory issues are most at risk. In addition, NO<sub>2</sub> forms harmful secondary air pollutants such as ozone (smog) and fine particulate that worsen respiratory and cardiovascular problems.

### **Impacts to Public Health are Cumulatively Significant**

Some of the major toxic air pollutants released from vehicle idling are styrene, 1,3 butadiene, toluene, benzene and xylenes. Collectively, exposure will lead to excess cancer risk of 1 in a million. While this level is below thresholds of significance, when added to the background risk, it is significant. The cumulative cancer risk is estimated to exceed 50 cancers per million. Detailed calculations and modeling files are provided in a separate Technical Memorandum.

## **CONCLUSIONS**

1. Significant impacts to air quality due to continuous emissions of NO<sub>x</sub> and other pollutants 365 days per year
2. Impacts to public health are cumulatively significant
3. None of the documents reviewed acknowledged the potential of health impacts to nearby residents located only 75 feet from the drive-thru lane.